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Requested By
JEFF CODEGA PLANNING & DESIGN INC
Washoe County Recorder
Kathryn L. Burke - Recorder

APN: (Not required
Per NRS 111.312.1)

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City of Sparks
P.O. Box 857
Sparks, Nevada 89432-0857
ATTN: Community Development Dep't

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NOTICE OF ADOPTION OF DEVELOPMENT PLAN
(NRS Chapter 278A)

Name of Development: Sparks Crossing

Name of Plan: Sparks Crossing Final Development Standards Handbook.

NOTICE IS HEREBY GIVEN that on June 13, 2005, the City Council of the City of Sparks, Nevada, finally adopted the development plan for the above named development, whose legal description is attached as Exhibit A.

Pursuant to NRS 278A.570 (2) after this plan is recorded, all zoning and subdivision regulations applicable to the property described in Exhibit A cease to apply and are replaced with the plan attached as Exhibit B hereto.

Dated this 8th day of July, 2005

City of Sparks, Nevada



Linda Z. Zillich FOR
Deborah J. Dolan LINDA Z. ZILRICH
City Clerk (DEPUTY)

State of Nevada)
) Acknowledgement in representative capacity
County of Washoe) (NRS 240.1665)

This instrument was acknowledged before me on July 8, 2005
By ~~DEBORINE J. DOLAN~~ as the ~~City Clerk~~ of Sparks, Nevada
Lenda Z. Ulrich Deputy City Clerk



Michelle L. Martin
Notary Public

APN's #510-082-01, 02, 03 and 06

LEGAL DESCRIPTION

All that certain property situate in the Southeast One Quarter (SE1/4) of Section 21, Township 20 North, Range 20 East, M.D.M., City of Sparks, County of Washoe, State of Nevada, being Parcel 1 of Parcel Map 3567, Document #2377039 filed in the Official Records of Washoe County on September 1, 1999, a portion of Parcel 1 and a portion of Parcel 2 of Parcel Map 3327, Document #2205419 filed in Official Records of Washoe County on April 30, 1998 and a portion of Parcel A, Parcel Map 3328, Document #2205450 filed in Official Records of Washoe County on April 30, 1998 more particularly described as:

Beginning at the Southwest corner of said Parcel 1 of said Parcel Map 3567;
Thence N 1°05'37" E, a distance of 1409.59 feet;
Thence N 19°12'54"E, a distance of 89.47 feet;
Thence along a non-tangent curve, concave southeasterly, having a central angle of 56°51'41", a radius of 98.50 feet, and an arc length of 97.88 feet from a radial bearing of S 39°30'21" E;
Thence S 72°34'07" E, a distance of 131.30 feet;
Thence along a tangent curve to the left, having a central angle of 10°29'20", a radius of 721.50 feet and an arc length of 132.08 feet;
Thence along a compound curve having a central angle of 6°07'19", a radius of 333.00 feet and an arc length of 35.58 feet;
Thence along a compound curve having a central angle of 13°41'36", a radius of 884.00 feet and an arc length of 211.27 feet;
Thence along a compound curve to the northerly most corner of Parcel A of said Parcel Map 3328, having a central angle of 39°50'09", a radius of 712.00 feet and an arc length of 495.03 feet;
Thence S 56°50'22" E, a distance of 166.32 feet;
Thence S 77°00'59" E, a distance of 375.60 feet;
Thence S 54°45'21" E, a distance of 59.73 feet;
Thence S 24°15'13" E, a distance of 110.31 feet;
Thence S 6°12'15" E, a distance of 84.58 feet;
Thence S 36°57'33" E, a distance of 112.09 feet;
Thence 12°03'05" E, a distance of 116.62 feet;
Thence S 3°14'00" W, a distance of 71.14 feet;
Thence S 7°37'51" E, a distance of 55.99 feet;
Thence N 89°59'46" W, a distance of 32.22 feet;
Thence along a tangent curve to the left, having a central angle of 37°45'52", a radius of 300.00 feet and an arc length of 197.73 feet;
Thence N 37°45'37" W, a distance of 55.01 feet;
Thence N 23°39'19" W, a distance of 61.07 feet;
Thence S 65°00'07" W, a distance of 70.02 feet;
Thence S 19°37'07" E, a distance of 1.77 feet;

Exhibit

A

Thence S 64°43'30" W, a distance of 64.02 feet;
Thence S 22°42'46" E, a distance of 23.66 feet;
Thence S 64°35'35" W, a distance of 64.27 feet;
Thence S 28°52'36" E, a distance of 13.43 feet;
Thence S 64°23'03" W, a distance of 64.03 feet;
Thence N 26°50'12" W, a distance of 14.42 feet;
Thence S 65°18'22" W, a distance of 64.01 feet;
Thence S 24°42'51" E, a distance of 15.65 feet;
Thence S 64°23'03" W, a distance of 64.03 feet;
Thence N 23°46'31" W, a distance of 6.06 feet;
Thence S 66°49'58" W, a distance of 60.11 feet to a point on the East line of Parcel 1 of said Parcel Map 3327;
Thence S 24°24'10" E, a distance of 142.32 feet;
Thence S 66°58'49" W, a distance of 661.03 feet to a point on the East line of Parcel 1 of said Parcel Map 3567;
Thence S 28°35'21" E, a distance of 40.63 feet;
Thence S 29°11'43" E, a distance of 71.16 feet;
Thence S 23°41'18" E, a distance of 66.10 feet;
Thence S 16°05'31" E, a distance of 65.83 feet;
Thence S 15°01'08" E, a distance of 67.60 feet;
Thence S 15°19'31" E, a distance of 67.28 feet;
Thence S 15°44'05" E, a distance of 12.77 feet;
Thence S 74°39'40" W, a distance of 245.72 feet;
Thence along a tangent curve to the right, having a central angle of 15°37'38", a radius of 225.00 feet and an arc length of 61.37 feet;
Thence N 89°42'42" W, a distance of 83.63 feet;
Thence N 88°39'36" W, a distance of 297.51 feet to the point of beginning.

Basis of bearing of this description is Tract Map 4319 recorded March 9, 2004 as Document #3003682, Official Records of Washoe County, Nevada.

Containing 40.42 Acres, more or less.

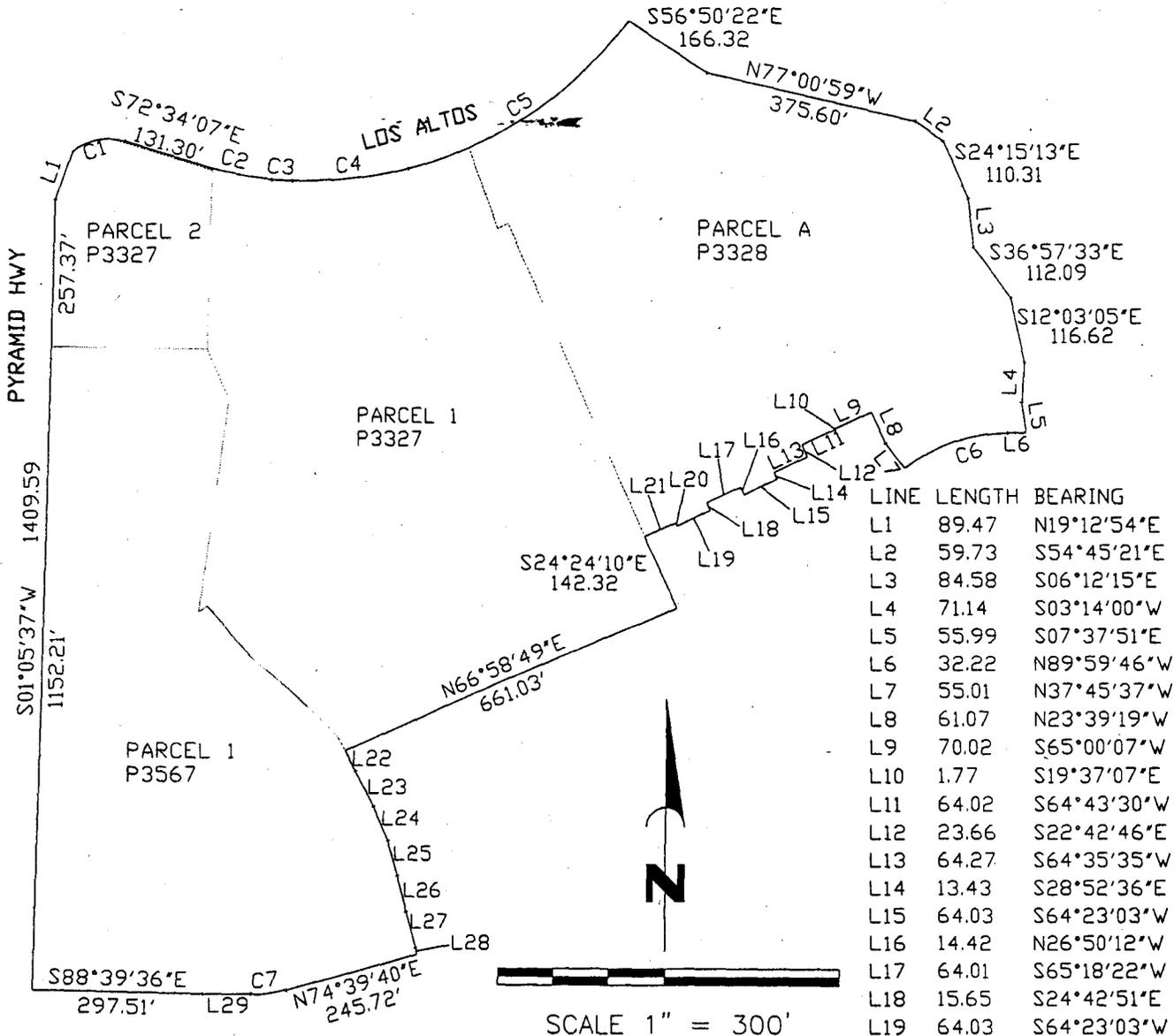
Dan Asikainen, P.L.S. #3318
Jeff Codega Planning/Design
433 West Plumb Lane
Reno, NV 89509



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SPARKS CROSSING

AIG BAKER



LINE	LENGTH	BEARING
L1	89.47	N19°12'54"E
L2	59.73	S54°45'21"E
L3	84.58	S06°12'15"E
L4	71.14	S03°14'00"W
L5	55.99	S07°37'51"E
L6	32.22	N89°59'46"W
L7	55.01	N37°45'37"W
L8	61.07	N23°39'19"W
L9	70.02	S65°00'07"W
L10	1.77	S19°37'07"E
L11	64.02	S64°43'30"W
L12	23.66	S22°42'46"E
L13	64.27	S64°35'35"W
L14	13.43	S28°52'36"E
L15	64.03	S64°23'03"W
L16	14.42	N26°50'12"W
L17	64.01	S65°18'22"W
L18	15.65	S24°42'51"E
L19	64.03	S64°23'03"W
L20	6.06	N23°46'31"W
L21	60.11	S66°49'58"W
L22	40.63	S28°35'21"E
L23	71.16	S29°11'43"E
L24	66.10	S23°41'18"E
L25	65.83	S16°05'31"E
L26	67.60	S15°01'08"E
L27	67.28	S15°19'31"E
L28	12.77	S15°44'05"E
L29	83.63	N89°42'42"W

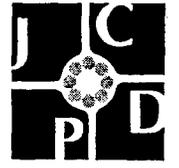
CURVE	RADIUS	LENGTH	CHORD	CHORD BEARING	DELTA
C1	98.50	97.88	93.85	S78°55'48"W	56°51'41"
C2	721.50	132.08	131.90	S77°48'47"E	10°29'20"
C3	333.00	35.58	35.56	S86°07'06"E	6°07'19"
C4	884.00	211.27	210.77	N83°58'26"E	13°41'36"
C5	712.00	495.03	485.12	N57°12'34"E	39°50'09"
C6	300.00	197.73	194.17	S71°07'18"W	37°45'52"
C7	225.00	61.37	61.18	N82°28'29"E	15°37'38"

RECEIVED-CITY WORKS

JUN 15 2005

COMMUNITY DEVELOPMENT BUILDING

EXHIBIT A



JEFF CODEGA
PLANNING/DESIGN, INC.

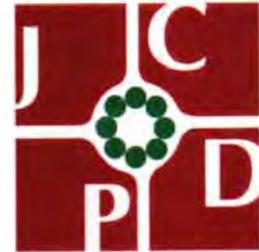
engineers/landscape architects
planners/land surveyors
construction services
433 West Plumb Lane

Date: 11-3-2004
Job #: 2424.0003

SPARKS CROSSING

FINAL DEVELOPMENT STANDARDS HANDBOOK

Prepared by:



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Prepared for:

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Exhibit

B

Adopted: June 13, 2005

Table of Contents

Chapter 1 - Introduction

Statement of Purpose and Plan..... 1-1
Project Location 1-1
Project Concept/Description 1-2
 Project Goals..... 1-2
 Site Analysis 1-2

Chapter 2 – Development Standards

Definitions 2-1
Standards Not Addressed 2-2
Land Use 2-2
 Allowed Uses 2-2
 Uses Requiring a Special Use Permit 2-3
Site Planning 2-3
 Perimeter Landscaping..... 2-3
 Parking 2-3
 Parking Lot Lighting..... 2-6
Landscaping 2-6
 General Requirement..... 2-6
 Streetscape 2-8
 Entry Median Plantings 2-10
 Plaza Areas..... 2-10
 Building Frontage Walkways and Planters 2-10
 Irrigation System..... 2-11
 Flowering Trees 2-11
 Primary Entry Aisle (Los Altos Parkway) 2-13
 Parking Lots..... 2-14
 South Property Boundary 2-15
 West Property Boundary 2-16
 Lawn Areas..... 2-17
 Trash Enclosures..... 2-18
 Maintenance 2-19

Site Grading 2-19
 Standards..... 2-19
Subsequent Review Requirements 2-20
Project Phasing..... 2-20
Administrative Approval for Minor Revisions 2-21
Design Flexibility..... 2-21
Omissions 2-21

Chapter 3 – Architectural Development Standards

Introduction 3-1
Design Review and Approval Process 3-1
 Improvements Requiring Review 3-1
 Plan Submittal Requirements 3-1
 Regulations..... 3-2
Site Design Standards 3-3
 General 3-3
 Visual Orientation 3-3
Pedestrian Circulation 3-5
 Walkways and Sidewalks..... 3-5
 Pedestrian Crossings 3-5
 Typical Grade Transitions 3-5
Public Spaces 3-8
 Los Altos Parkway Entry..... 3-8
 Galleria Parkway Entry 3-8
 Plaza Areas..... 3-8
 Center Plaza 3-8
 West Plaza 3-8
Site Lighting 3-12
 Building Mounted Sidewalk Lighting 3-12
 Building Mounted Area Lighting 3-12
 Pole Mounted Sidewalk Lighting 3-13
 Pole Mounted Area Lighting 3-13
Site Amenities 3-14
Building Design Standards 3-18
 Introduction..... 3-18
 General Architectural Standards 3-19
 Materials 3-22
 Drive-Through Facilities 3-23

Service Areas 3-24

Building Types..... 3-25

 Major Anchor Buildings..... 3-25

 Mini Anchor Buildings..... 3-26

 Shop Buildings 3-26

 Pad Buildings 3-27

Construction, Operation, and Maintenance 3-28

 General 3-28

 Temporary Uses and Structures..... 3-29

 Construction Yards 3-29

Chapter 4 – Sign Standards

General Requirements 4-1

 Definitions and Submittal Requirements 4-1

 Prohibited Exterior Signs..... 4-2

Allowable Signs..... 4-3

 Excepted Signage..... 4-3

 Allowable Signage Areas and Locations..... 4-4

Freestanding and Monument Sign Standards..... 4-5

 Freestanding Signage 4-5

 Monument Signage..... 4-7

Chapter 5 – Infrastructure Phasing

Purpose 5-1

Major Street Network – Principal Access 5-1

Timing of Major Roadway Improvements 5-3

Sanitary Sewer 5-5

Storm Water Management 5-7

Public Water System..... 5-9

Utilities 5-9

Orr Ditch Relocation 5-9

Chapter 6 – Planned Development Policy Analysis

Planned Development Findings 6-1

List of Figures:

Figure 1-1 - Location Map 1-1
Figure 1-2 – Site Analysis..... 1-3
Figure 1-3 – Conceptual Site Plan 1-4
Figure 2-1 – Parking Geometry 2-5
Figure 2-2 – Preliminary Landscape Plan 2-7
Figure 2-3 – Los Altos Parkway Streetscape..... 2-8
Figure 2-4 – Galleria Parkway Streetscape 2-9
Figure 2-5 – Accent Plantings 2-12
Figure 2-6 – Primary Entry Aisle Planting Concept 2-13
Figure 2-7 – Parking Lot Planting Concept 2-14
Figure 2-8 – South Property Line Planting Concept 2-15
Figure 2-9 – West Property Line Planting Concept 2-16
Figure 2-10 – Lawn Areas 2-17
Figure 2-11 – Trash Enclosure Screening Concept..... 2-18
Figure 3-1 – Reference Site Plan 3-4
Figure 3-2 – Plan of Example Hardscape and Planters at Anchor Building..... 3-6
Figure 3-3 – Plan of Typical Walkway Crossing 3-7
Figure 3-4 – Plan of Los Altos Parkway Entry 3-9
Figure 3-5 – Plan of Center Plaza..... 3-10
Figure 3-6 – Plan of West Plaza 3-11
Figure 3-7 – Building Mounted Sidewalk Lighting..... 3-12
Figure 3-8 – Building Mounted Area Lighting..... 3-12
Figure 3-9 – Pole Mounted Sidewalk Lighting..... 3-13
Figure 3-10 – Pole Mounted Area Lighting..... 3-14
Figure 3-11 – Outdoor Public Seating 3-14
Figure 3-12 – Trash Receptacles 3-15
Figure 3-13 – Bicycle Racks..... 3-16
Figure 3-14 – Bollard..... 3-17
Figure 3-15 – Tree Grate..... 3-17
Figure 3-16 – Colored Elevation Key Map..... 3-30
Figure 3-17 – Colored Elevations 3-31
Figure 3-18 – Colored Elevations 3-32
Figure 3-19 – Colored Elevations 3-33
Figure 3-20 – Colored Elevations 3-34
Figure 3-21 – Colored Elevations 3-35
Figure 3-22 – Colored Elevations 3-36

Figure 3-23 – Colored Elevations 3-37

Figure 3-24 – Colored Elevations 3-38

Figure 3-25 – Colored Elevations 3-39

Figure 3-26 – Colored Elevations 3-40

Figure 3-27 – Colored Elevations 3-41

Figure 3-28 – Colored Elevations 3-42

Figure 3-29 – Colored Elevations 3-43

Figure 3-30 – Colored Elevations 3-44

Figure 3-31 – Colored Elevations 3-45

Figure 3-32 – Colored Elevations 3-46

Figure 3-33 – Colored Elevations 3-47

Figure 3-34 – Colored Elevations 3-48

Figure 3-35 – Colored Elevations 3-49

Figure 3-36 – Colored Elevations 3-50

Figure 3-37 – Colored Elevations 3-51

Figure 3-38 – Colored Elevations 3-52

Figure 3-39 – Colored Elevations 3-53

Figure 3-40 – Colored Elevations 3-54

Figure 3-41 – Colored Elevations 3-55

Figure 3-42 – Colored Elevations 3-56

Figure 3-43 – Colored Elevations 3-57

Figure 3-44 – Colored Elevations 3-58

Figure 3-45 – Colored Elevations 3-59

Figure 3-46 – Colored Elevations 3-60

Figure 3-47 – Colored Elevations 3-61

Figure 3-48 – Colored Elevations 3-62

Figure 3-49 – Colored Elevations 3-63

Figure 3-50 – Colored Elevations 3-64

Figure 3-51 – Colored Elevations 3-65

Figure 3-52 – Colored Elevations 3-66

Figure 3-53 – Colored Elevations 3-67

Figure 3-54 – View from Los Altos Parkway 3-68

Figure 3-55 – View from Los Altos Parkway 3-69

Figure 3-56 – View to Entry from Los Altos 3-70

Figure 3-57 – View from Los Altos Entry 3-71

Figure 3-58 – View from Galleria Parkway 3-72

Figure 3-59 – View from Galleria Parkway 3-73

Figure 3-60 – View from Los Altos Entrance..... 3-74

Figure 3-61 – View Toward Center Plaza..... 3-75

Figure 3-62 – View of Center Plaza..... 3-76
Figure 3-63 – View of Anchor Buildings 3-77
Figure 3-64 – View of Anchor Buildings 3-78
Figure 3-65 – View of West Plaza 3-79
Figure 3-66 – View of Shop Building 3-80
Figure 3-67 – View of Shop Buildings..... 3-81
Figure 3-68 – View to Entry from Galleria Parkway..... 3-82
Figure 3-69 – View from Galleria Parkway 3-83
Figure 3-70 – View from Galleria Parkway 3-84
Figure 3-71 – View from Shoppers Way..... 3-85
Figure 3-72 – View to Center Plaza from West..... 3-86
Figure 3-73 – View to Shops C & D from Entry 3-87
Figure 3-74 – View from Pyramid Highway 3-88
Figure 3-75 – View from Pyramid Highway 3-89
Figure 3-76 – View of Pylon Sign from Los Altos Parkway 3-90
Figure 4-1 – Project Identification Sign 4-6
Figure 4-2 – Monument Sign..... 4-7
Figure 5-1 – Access Plan..... 5-2
Figure 5-2 – Galleria Parkway Improvements..... 5-4
Figure 5-3 – Sanitary Sewer Route 5-6
Figure 5-4 – Off-Site Storm Drain Route..... 5-8

List of Tables:

Table 2-1 – Perimeter Landscaping..... 2-3
Table 2-2 – Parking Requirements 2-4
Table 2-3 – Parking Geometry 2-4

Appendices:

- Sparks Crossing Building Color Palette (Exhibit “K”)
- Site Plan Enlargement (Exhibit “L”)
- Sign Guidelines
- Parking Lot Lighting Concept Cut-Sheet

Chapter 1 - Introduction

Statement of Plan and Purpose

The purpose of the Sparks Crossing New Urban District (NUD) is to provide for a high quality commercial development for area residents while creating its own distinct character through quality architecture, careful site planning, and streetscape/landscape improvements.

Project Location

Sparks Crossing consists of 41.5± acres and is located on the south side of Los Altos Parkway, just east of Pyramid Highway (State Route 445). Primary access to the project will occur from Los Altos Parkway, with additional access from Pyramid Highway (Shoppers Way) - shared with the adjacent Sparks Galleria project and Galleria Parkway. Figure 1-1 (below) depicts the location of Sparks Crossing. Additional detail on individual access points is provided in Chapter 5.

Subject Property

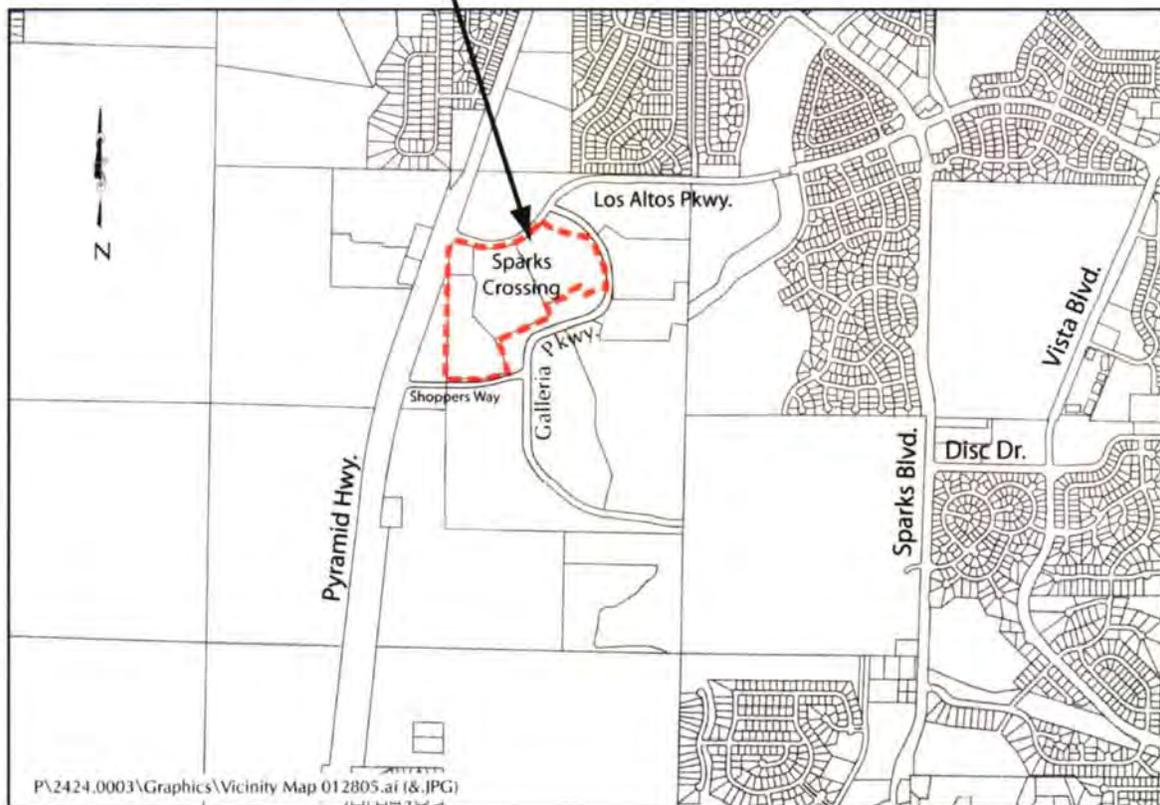


Figure 1-1 - Location Map

Project Concept/Description

Sparks Crossing is envisioned to include a mix of retail uses that will provide needed commercial services to area residents.

Project Goals:

In order to implement the high quality center envisioned for the project, specific goals have been established for Sparks Crossing. The standards set forth in this handbook will serve to implement these goals and vision of the project.

- Goal 1: To provide a mix of needed commercial uses to area residents.
- Goal 2: To promote economic diversification and create employment opportunities within the Spanish Springs Valley.
- Goal 3: To promote a high quality project through architectural design, site configuration (including functional and visual diversity), and landscape/streetscape improvements.
- Goal 4: To ensure proper site development techniques which result in careful grading, appropriate treatment of slopes, etc.

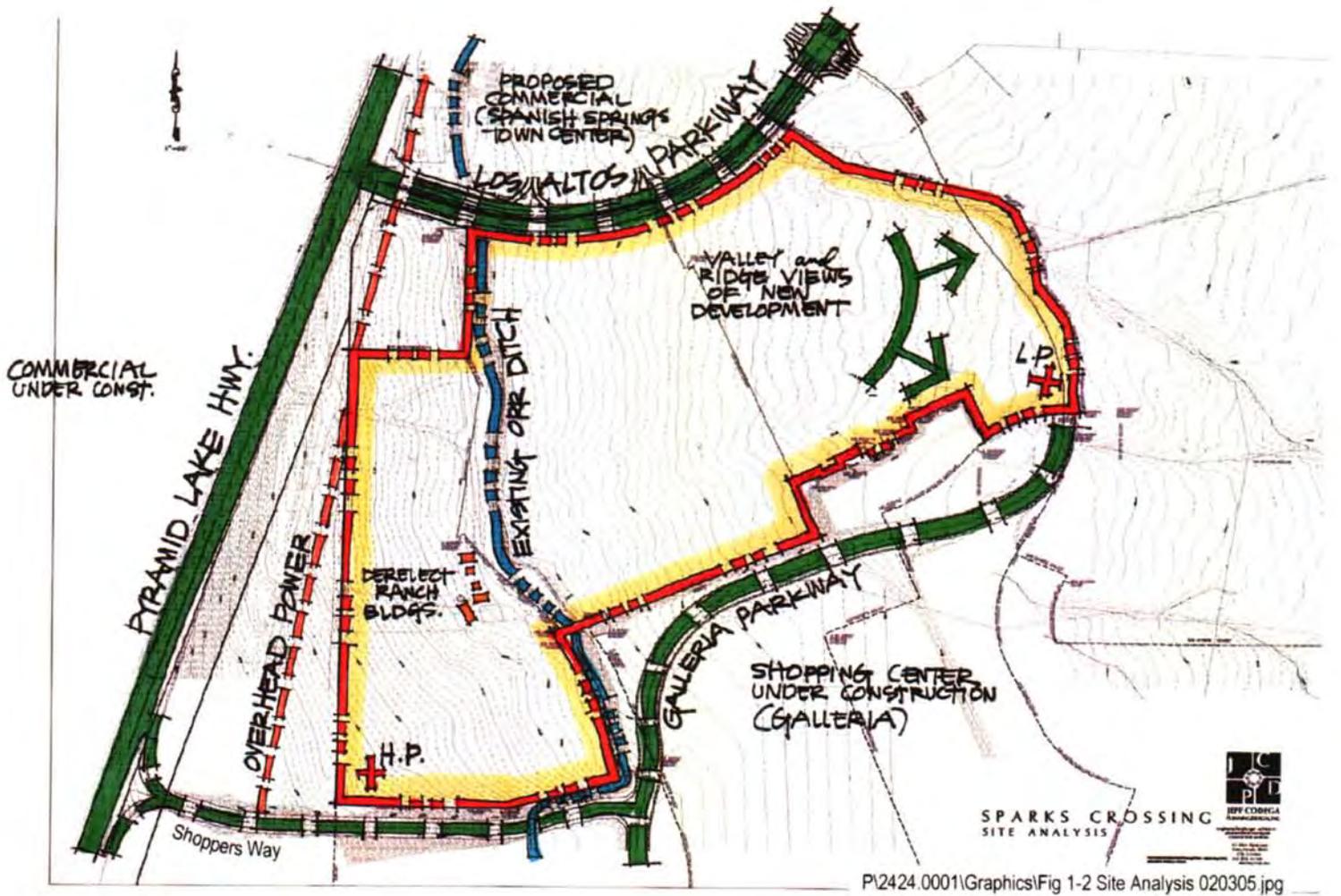
The overall project theme of Sparks Crossing will complement the adjacent commercial uses. However, Sparks Crossing will evoke its own unique character through the standards outlined in this handbook.

Site Analysis:

Consistent with City of Sparks Planned Development (PD) requirements, a comprehensive site analysis has been completed for the Sparks Crossing site. The purpose of the site analysis is to determine prominent natural land features, access points, views, areas of steep slope, natural drainage, etc.

Figure 1-2 (following page) depicts the Sparks Crossing Site Analysis. The project site plan (depicted in Figure 1-3, page 1-4) was developed based on the site analysis and the design of neighboring properties/commercial centers.

As Figure 1-2 depicts, the Orr Ditch traverses the Sparks Crossing site. Consistent with adjacent projects (Sparks Galleria and Spanish Springs Town Center) the Orr Ditch will be piped through the Sparks Crossing site upon development of the site.



Legend
H.P. = High Point
L.P. = Low Point

Figure 1-2 – Site Analysis

SPARKS CROSSING



Note: Refer to Exhibit "L" located in the appendices for enlarged copies of the site plan.

Figure 1-3 – Conceptual Site Plan

Chapter 2 – Development Standards

The Sparks Crossing New Urban District (NUD) emphasizes focused development through specific design controls. The standards included in this handbook are meaningful and specific, providing assurances that the final design of Sparks Crossing is certain. Although the handbook provides for options, there is little flexibility in terms of architectural quality, required landscaping, streetscape treatments, etc.

The Development Standards are developed to promote compatibility with adjacent uses, properly relate to the surrounding environment, provide proportional sizes/arrangements of buildings, ensure adequate parking, create aesthetic appeal, and provide pedestrian circulation/amenities.

Definitions

Words and phrases used herein shall have the meaning as set forth in this handbook:

1. **Administrator:** - “Administrator” shall mean the City Manager or his lawfully designated representative.
2. **Architect:** - “Architect” shall mean design individual or firm contracted by the Master Developer to design the buildings to be constructed by the Developer, tenant, or user to design their building or tenant space, registered to practice architecture in the State of Nevada.
3. **City:** - “City” shall mean The City of Sparks, Nevada.
4. **Code:** - “Code” shall refer to the City of Sparks Municipal Code.
5. **Design Manual:** - “Design Manual” shall refer to The City of Sparks Design Standards Manual. Concepts and requirements for non-residential development contained therein are to be utilized and followed. References to other Municipal Code sections within this Manual, and other ordinances as may be applicable, shall be considered as if entirely stated herein.
6. **Design Standards:** - “Design Standards” shall refer to the contents of this handbook and any subsequent revisions approved by the City of Sparks.
7. **Master Developer:** - “Master Developer” shall mean AIG Baker Sparks, L.L.C., its subsidiaries or subsequent owners, of all or part of this shopping center, collectively referred to as “Master Developer.”

8. **Sign Code:** - "Sign Code" shall refer to The City of Sparks Municipal Code, Chapter 20.56, Signs. References to other Municipal Code sections within this chapter, and other ordinances as may be applicable, shall be considered as if entirely stated herein.

9. **Conformance:** Any building/structure erected, placed, established, relocated, created or maintained in the Sparks Crossing development shall conform to all standards, procedures, exemptions and all other requirements of The City of Sparks Municipal Code and the Design Standards.

Standards Not Addressed

Any development standards not specifically addressed in this handbook, shall be subject to the requirements set forth in the City of Sparks Municipal Code and/or the City of Sparks Design Standards Manual.

Land Use

Allowed Uses¹:

- Retail and Wholesale Stores (no outside storage is permitted)
- Personal Service Shops/Facilities (such as hair/nail salon, day spa, shoe repair, dry cleaning, alterations, etc.)
- Entertainment Facilities (such as video arcades, bowling alleys, ice skating, movie theatres, family fun centers, etc.)
- Restaurants (with or without alcohol service)
- Bars/Sports Bars
- Automobile Service Stations (maximum of 12 nozzles) or Facilities contained within a building (i.e. "quick" lube, tire sales, car washes, etc.). Outside storage is prohibited.
- Drive-Thru Facilities (subject to provisions of SMC Section 20.41.120)
- Child Care Center
- Copying/Mail Establishments
- Health Clubs (3,000 square feet or less)
- Financial Institutions
- Offices (including professional and medical/dental offices/clinics)
- Veterinary Clinics/Offices (no outside kennels)
- Technical Training (i.e. computers, cooking, and trades)

¹ All new construction shall be subject to the approval of a Site Plan Review by the City of Sparks Community Development Department to ensure compliance with the standards outlined in this handbook.

Uses Requiring a Special Use Permit:

- Major Public Utility/Communication Structures (above ground and used for distribution, storage, and/or transmission)
- Uses Operating in Excess of 17 Hours per day
- Health Clubs (over 3,000 square feet)

Site Planning

Perimeter Landscaping:

The standards included with Sparks Crossing are intended to complement adjacent commercial developments, as well as promote a uniform landscaped streetscape along Los Altos Parkway and Galleria Parkway. The standards outlined below will provide flexibility in locating buildings and ensure a varied appearance.

Table 2-1 (below) provides a summary of the perimeter landscape required within Sparks Crossing.

Table 2-1 – Perimeter Landscaping

Location	Requirement
Adjacent to Los Altos Parkway	25 feet – Entire 25’ width shall be landscaped ^{1 + 2}
Adjacent to Galleria Parkway	20 feet - Entire 20’ width shall be landscaped ^{1 + 2}
West Property Line	10 feet – Entire 10’ width shall be landscaped ²⁺³
South Property Line	20 feet – Entire 20’ width shall be landscaped ²⁺³

- 1 Sidewalks and signs shall be permitted within landscaped area.
- 2 Where retaining walls are included within the perimeter landscaping, they will be counted as part of the required landscape setback area.
- 3 Where terracing is used (more than one retaining wall), 6 feet of landscaping bench shall be provided.

Building setbacks from internal property lines may be zero. This will apply when parcels are created internally that create zero setbacks for attached buildings.

Parking:

1. Parking within Sparks Crossing shall be provided per the requirements outlined in Section 20.49 of the Sparks Municipal Code. Minimum parking requirements shall conform to the standards outlined in Table 2-2 (following page). Parking geometry standards are outlined in Table 2-3 (following page).

Table 2-2 – Parking Requirements

Use ¹	Parking Requirement
General Retail and Service	One space per 250 square feet of net leasable floor area.
Personal Service (barber shop, beauty salon, copy center, etc.)	One space per 150 square feet of net leasable floor area.
Office/Financial Institution	One space per 200 square feet of net leasable floor area.
Bar/Sports Bar/Restaurant	One space per 100 square feet of floor area, including service areas.
Theater	One space for every 3 seats.
Child Care Facility	One space per employee, plus one space for every 8 children.
Automobile Service Stations	One space for every 100 square feet of work area.
Retail Sales of Building Materials, Furniture, Major Appliances, etc.	One space per 500 square feet of net leasable floor area.
Health Club and/or Recreation Facility	One space per 200 square feet of building area.

¹ Any use not defined in Table 2-2 or the Sparks Municipal Code, Section 20.49.020 shall be subject to interpretation by the Administrator.

Note: The overall parking ratio for all uses within Sparks Crossing shall be no less than 1 space per 222 square feet of total building area (on average).

Table 2-3 (below) provides the parking geometry standards for Sparks Crossing. Figure 2-1 (following page) provides a visual depiction of the standard outlined in Table 2-3.

Table 2-3 – Parking Geometry

Angle (degrees)	A	D	L	N	P	Q	V	W	X	Y
0	12 ¹	9.0	24	21.0	30.0 ¹	---	24.0	9	---	---
15	12 ¹	13.4	18	25.4	38.8 ¹	4.7	34.8	9	17.4	2.3
30	12 ¹	16.9	18	28.9	45.6 ¹	9.0	18.0	9	15.6	4.5
45	12 ¹	19.1	18	31.1	50.2 ¹	12.8	12.7	9	12.7	6.4
60	16 ¹	20.1	18	36.1	56.2 ¹	15.6	10.4	9	9.0	7.8
75	24	19.7	18	43.7	63.4	17.4	9.3	9	4.6	8.7
90	24	18	18	42.0	60.0	18.0	9.0	9	---	9.0

¹ Minimum aisle width for two-way traffic is 20 feet.

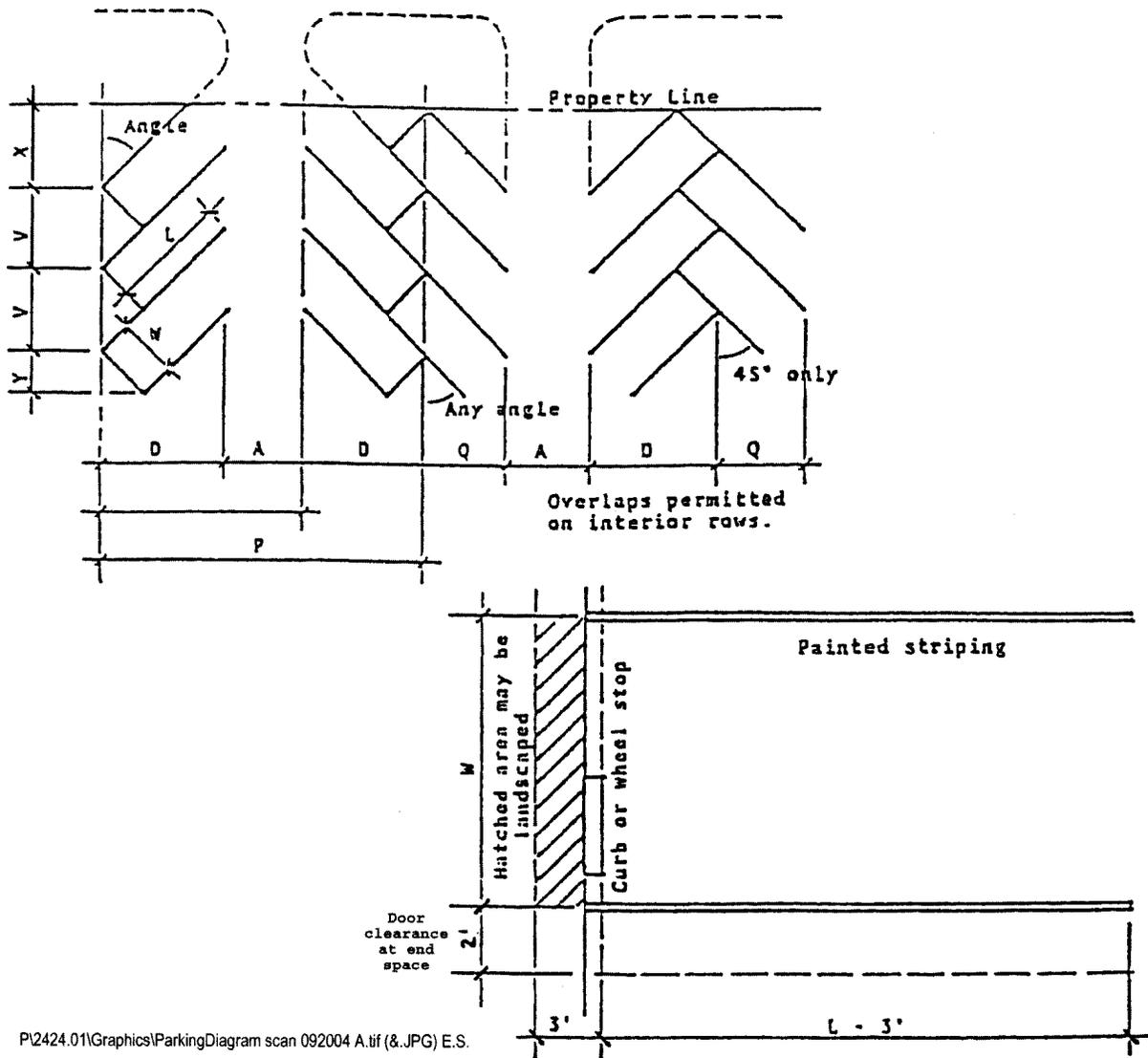


Figure 2-1 – Parking Geometry

2. One accessible parking space shall be provided for every 25 required spaces for the first 100 spaces; one for every 50 or fraction of 50 for the next 100 spaces; one for every 100 or fraction of 100 for the next 300 spaces; in excess of 500 spaces shall provide 2 percent of the total number of spaces as accessible.
3. One out of every 8 accessible spaces shall be van accessible.

4. Accessible spaces shall be a minimum of 8 feet wide and have an adjacent 5 foot wide aisle. Van accessible spaces shall be a minimum of 8 feet wide and have an adjacent 8 foot wide aisle.
5. A reciprocal parking agreement shall be provided to the City of Sparks encompassing all uses within the project boundaries.
6. As Site Plan Reviews or Special Use Permits are brought forward, the applicant/tenant shall be required to provide the City of Sparks with a “running total” of required/provided parking, in order to ensure compliance with the Standards.
7. Based on final tenant mix and actual parking demands, an increase and/or decrease in provided parking may be permitted within Sparks Crossing if documented through a parking study.
8. Parking lot design (i.e. driveways, access, paving, etc.) shall conform to the design requirements of the Sparks Municipal Code, Chapter 20.49, Section 20.49.010.

Parking Lot Lighting:

1. Parking lot lighting is required for public safety; such lighting facilities shall be located, with hoods provided and adjusted, so as to preclude the direct glare of the lights from shining directly onto adjoining property or streets. A “cut-sheet” detailing acceptable fixture specifications is included in the appendix of this handbook.
2. The height of parking lot lighting shall not exceed 30 feet from grade of parking lot (27-foot poles mounted on 3-foot concrete base).

Landscaping:

General Requirement:

1. A minimum of 20% of the gross site area (8.36± acres) shall be landscaped. Decorative hardscape including stamped/colored concrete, interlocking pavers, etc. shall count towards total landscape area, up to 5% of the 20% requirement.
2. All ground surfaces within landscape areas shall include 4± inch rounded river rock or bark mulch, laid to a depth of 4 inches, placed over a pre-emergent herbicide and weed barrier film. This does not exclude the use of accent boulders and rocks.
3. Landscaping within Sparks Crossing shall comply with the SMC, Chapter 20.32, as well as the City of Sparks Design Standards Manual for standards not specifically addressed.

SPARKS CROSSING

Figure 2-2 (below) depicts the preliminary landscape plan developed for Sparks Crossing, reflecting the standards presented within this section.

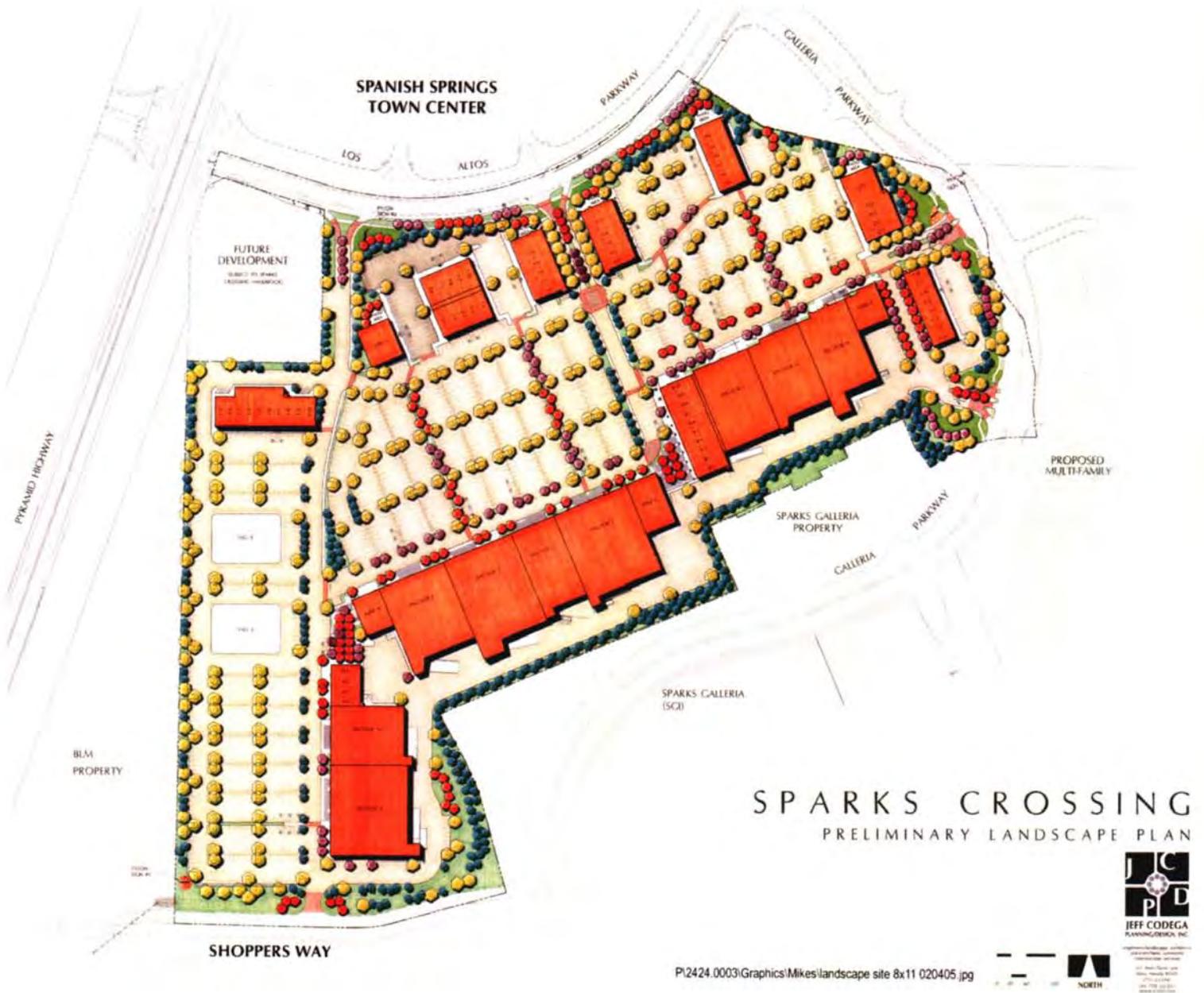


Figure 2-2 – Preliminary Landscape Plan

Streetscape:

1. Los Altos Parkway:

A 25-foot landscape buffer shall be provided along Los Altos Parkway. No buildings or parking areas shall encroach into this area.

A 5' foot "straight" concrete sidewalk shall be constructed 11 feet from back of curb of Los Altos Parkway and include the granting of a public access easement to the City of Sparks.

Trees shall be planted behind the sidewalk at a rate of one tree per 15 lineal feet of Los Altos site frontage. Plantings shall include a mix of evergreen ($\pm 40\%$) and deciduous trees ($\pm 60\%$) clustered in order to create visual interest and frame project architecture, entries, etc.

Deciduous trees shall be a minimum 2-inch caliper at time of planting, with evergreen trees at a minimum of 8-feet at time of planting. A minimum of 6 shrubs (5 gallon minimum) shall be planted for every required tree within the Los Altos streetscape behind the sidewalk. Shrub planting for front of sidewalk to back of curb shall be one shrub (5 gallon minimum) per 39 square feet.

Use of lawn, berming, and landscape boulders may be incorporated into the Los Altos Parkway streetscape design.

The standards described above shall pertain to all Los Altos entries, including secondary accesses. See Figure 2-3 (below) for plan view of the Los Altos streetscape treatment.

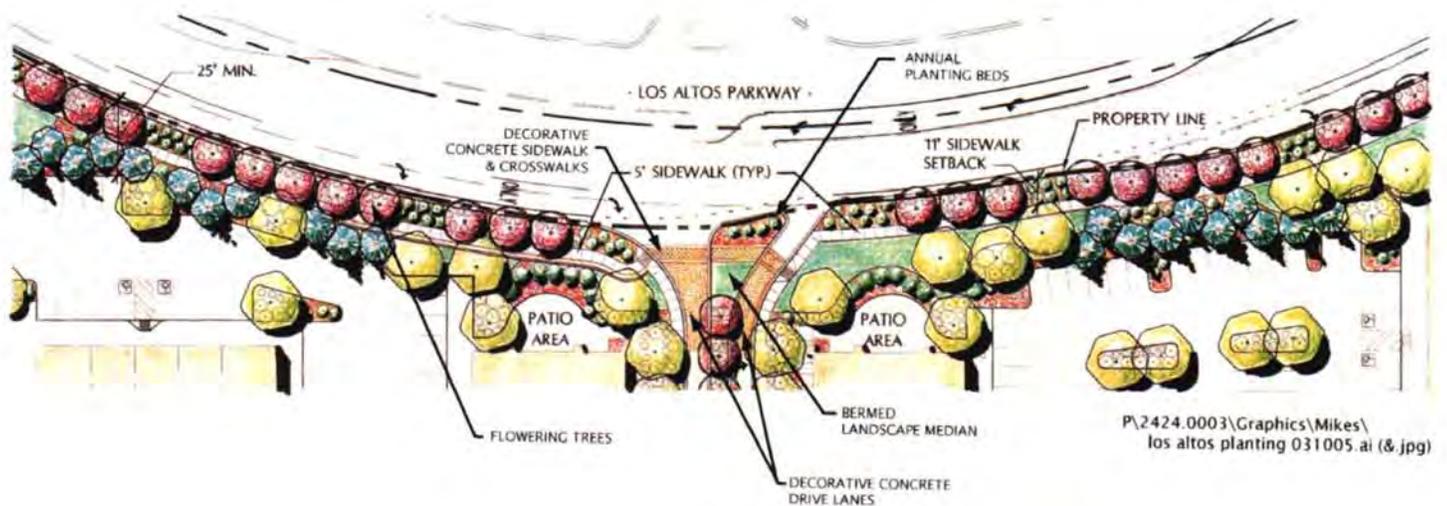


Figure 2-3 – Los Altos Parkway Streetscape

2. Galleria Parkway:

A 20-foot landscape buffer shall be provided along Galleria Parkway. No buildings or parking areas shall encroach into this area.

A 5 foot meandering concrete sidewalk shall be constructed within the Galleria streetscape, including the granting of a public access easement to the City of Sparks. The sidewalk shall be located a minimum of 5 feet from back-of-curb.

Trees shall be planted at an overall rate of one tree per 12 lineal feet of Galleria Parkway site frontage. Plantings shall include a mix of evergreen ($\pm 40\%$) and deciduous trees ($\pm 60\%$) clustered in order to create visual interest and frame project architecture, entries, etc.

Deciduous trees shall be a minimum 2-inch caliper at time of planting, with evergreen trees at a minimum of 8-feet at time of planting. A minimum of 6 shrubs (5 gallon minimum) shall be planted for every required tree within the Galleria Parkway streetscape.

Use of lawn, berming, and landscape boulders may be incorporated into the Galleria Parkway streetscape design.

The standards outlined above shall apply to all Galleria Parkway entries. See Figure 2-4 (below) for a plan view of the Galleria Parkway streetscape standards.

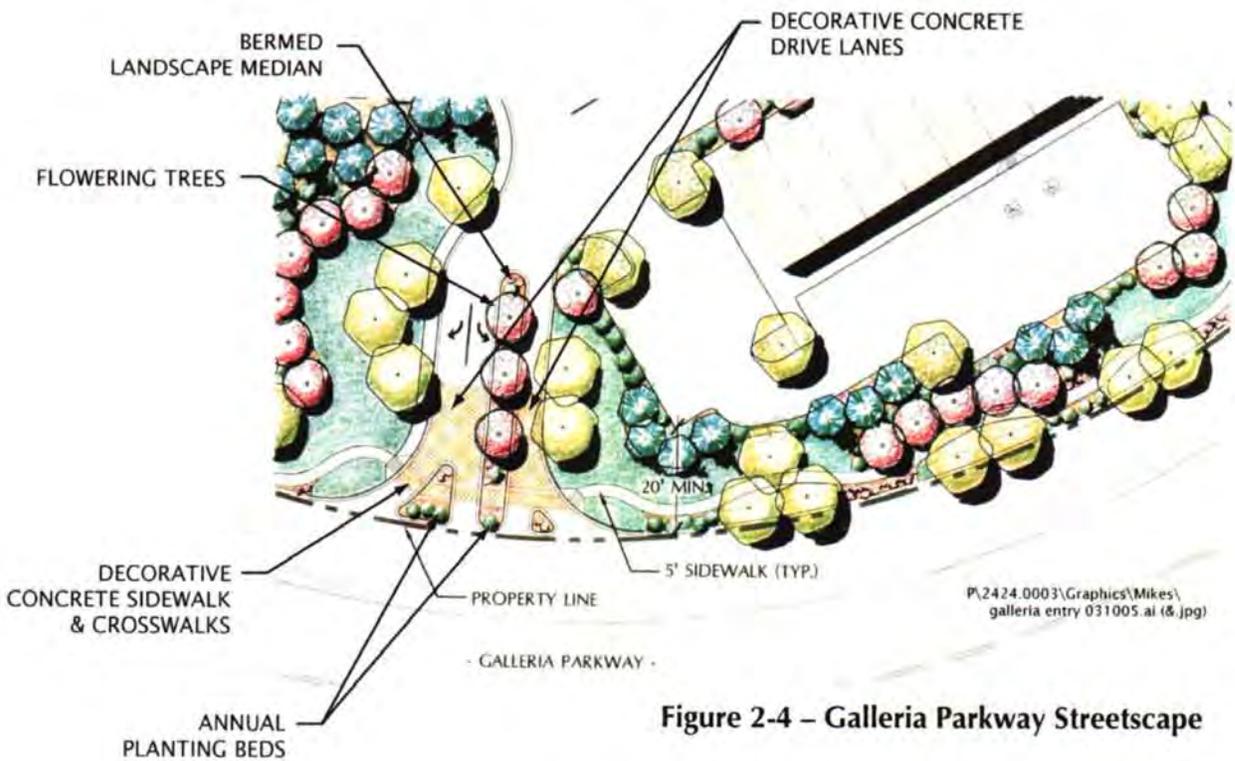


Figure 2-4 – Galleria Parkway Streetscape

Entryway Median Plantings:

1. Entryway medians shall include 2" caliper deciduous trees planted 15 feet on center.
2. Entryway medians occur at the two Los Altos entries as well as the two Galleria Parkway entries.

Plaza Areas:

Two primary public plazas are included in the Sparks Crossing plan as detailed in the following Chapter. These areas are intended to provide for outdoor seating areas, create visual breaks between buildings, and add character to the overall development.

Plaza locations are defined/depicted in Chapter 3. The standards below provide required landscaping for plaza areas.

1. A mix of shade, evergreen, and flowering trees shall be provided within plaza areas. Shade trees shall be a minimum 3-inch caliper at time of planting, with flowering trees at 2-inch minimum caliper, and evergreens at 8-feet at time of planting. The overall tree mix shall consist of 60% shade trees, 20% flowering trees, and 20% evergreen trees.
2. Trees within the West Plaza (refer to Chapter 3 for location) shall be planted at a rate of one tree per 360 square feet of plaza area.
3. Trees within the Center Plaza (refer to Chapter 3 for location) shall be planted at a rate of one tree per 570 square feet of plaza area.
4. No shrubs shall be required within the West Plaza area due to the paving and character of these spaces. For the Center Plaza, there shall be 3 shrubs per tree.
5. Refer to Chapter 3 for plaza hardscape treatments and standards.

Building Frontage Walkways and Planters:

1. A mix of shade, evergreen and flowering trees shall be provided within building frontage walkways. Shade trees shall be a minimum 3-inch caliper at time of planting, with flowering trees at 2-inch minimum caliper and evergreens at 8-feet at time of planting. The overall tree mix shall consist of 60% shade trees, 20% flowering trees, and 20% evergreen trees. Refer to Figures 3-2, 3-3, and 3-4.

2. Trees shall be planted at a rate of two trees for each anchor demising wall and one tree per three in-line shops.
3. All planters shall be planted with shrubs (5 gallon) and living groundcovers (1 gallon) so that mature size plants will grow together and cover the ground.
4. A minimum of 32 square feet of planter area shall be provided for every 20 lineal feet of store frontage. Planter areas shall include a minimum 4-foot width and may be clustered to add visual interest. Maximum separation between planters shall not exceed 40 feet.

Irrigation System:

1. A fully automated system shall be installed for all landscape areas within Sparks Crossing.
2. All landscaping/irrigation systems shall be designed by a Nevada registered Landscape Architect.

Flowering Trees:

Flowering trees occur along the storefront planting islands located adjacent to the primary shopping center frontage (not at buildings located along Los Altos Parkway) within Sparks Crossing, as depicted in Figure 2-5 (below).

1. Flowering trees will be used in the following application:
 - a. For special interest, as associated with plaza space (i.e. between "A" Shops and "Mini A"/"Mini C" and "B" Shops as previously depicted in Figure 2-2 and Figures 3-5 and 3-6).
2. Flowering trees shall either be columnar or round to accommodate benches or to avoid blocking the line-of-sight of store front signage.

3. Sizes of flowering trees shall be two inch caliper (at time of planting) in parking lot applications, and three inch caliper when placed adjacent to pedestrian spaces, building facades, or within plaza spaces.
4. Flowering trees planted in parking lot planters shall be counted towards required parking lot landscaping in areas where they occur.

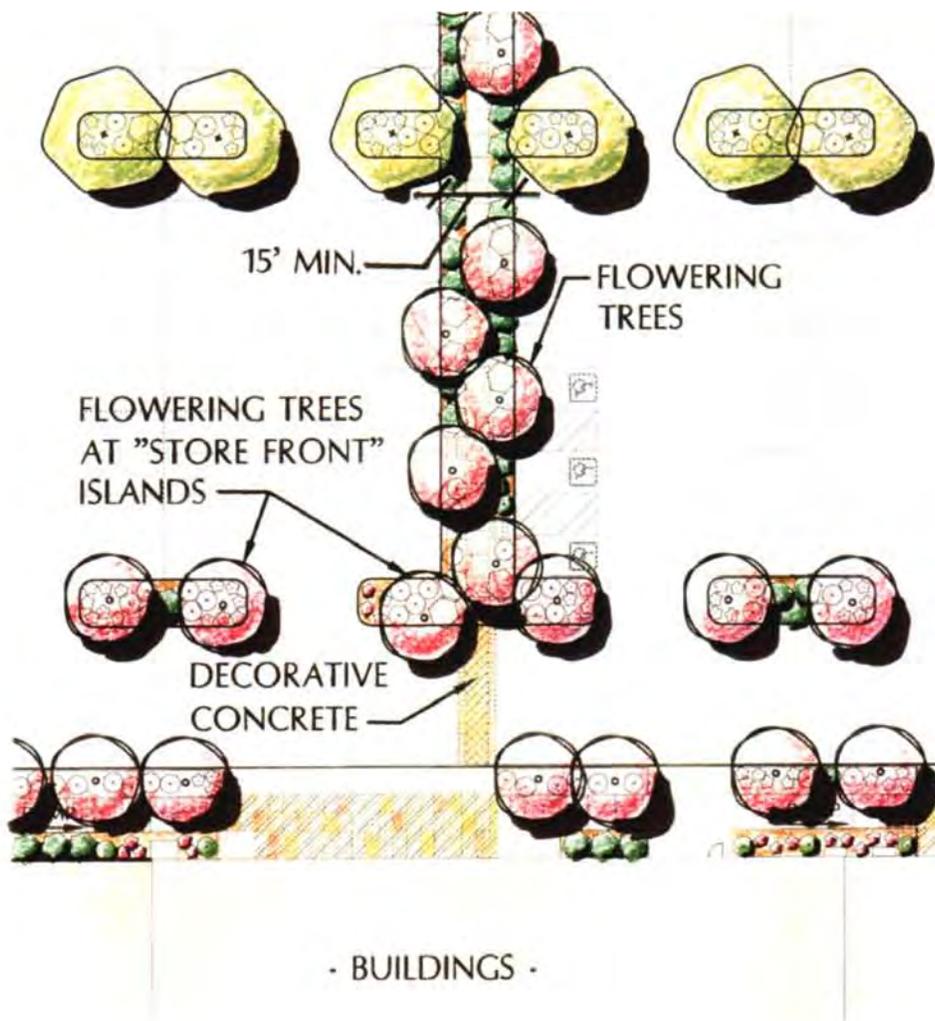


Figure 2-5 – Accent Plantings

Primary Entry Aisle (Los Altos Parkway):

1. Evergreen and/or deciduous columnar trees, planted 20 feet on center, shall be used along the primary entry aisle connecting with Los Altos Parkway, serving to frame the project architecture. Evergreen trees shall be a minimum of 8 feet at time of planting. Deciduous trees shall be a minimum of 2-inch caliper.
2. 5 gallon shrubs shall be planted along the primary entry aisle at a rate of 6 shrubs for every required tree.
3. Minimum planter width for the primary entry aisle shall be 15 feet.

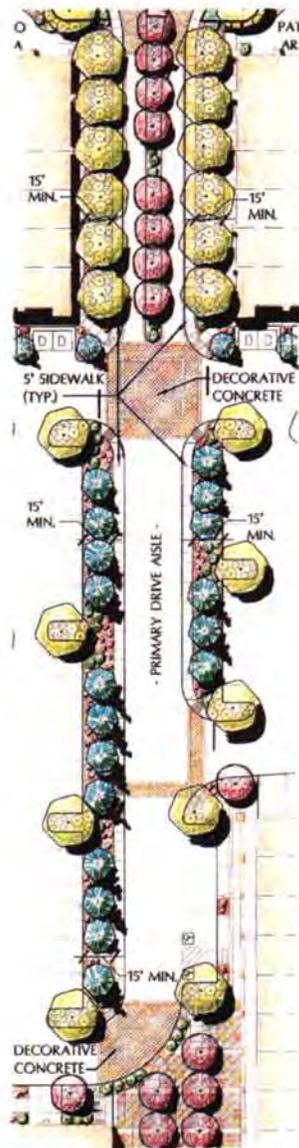


Figure 2-6 – Primary Entry Aisle Planting Concept

Parking Lots:

1. A mix of deciduous ($\pm 75\%$) and evergreen ($\pm 25\%$) trees shall be used throughout parking areas to create shade and break-up the mass of parking areas, creating a “smaller” feel (refer to Figure 2-7, below). A minimum of one tree per every 10 parking spaces shall be provided.
2. Shrubs (5 gallon minimum) shall be planted at a rate of 6 per required tree.
3. Deciduous trees within parking lots shall be a minimum 2 inch caliper, with evergreen trees at a 6-foot minimum.
4. Sidewalk “islands” providing pedestrian connections between parking areas and storefronts shall be lined with shrubs (5 gallon minimum) and/or 2-inch caliper columnar trees at 5 feet on center. Minimum sidewalk width shall be 5 feet with a minimum 5-foot interior width planter on either side. A licensed Nevada Landscape Architect shall specify plantings that minimize potential “overhang” impacts created by parked cars in these areas, avoiding the need for wheel stops. There shall be a 6-inch curb provided on both parking lot edges. A minimum of 4 north/south sidewalk islands shall be provided within the main parking area, spaced intermittently to provide efficient pedestrian circulation.
5. Planting islands shall be provided at the ends of parking rows that are a minimum of 300 square feet for double loaded parking and 150 square feet for single loaded, with a 9-foot minimum interior width. A 6-inch curb shall be provided on all sides of the island in order to protect plantings.
6. Interior landscaped parking islands shall be a minimum of 9 feet wide (exterior) and protected by a minimum 6-inch curb.

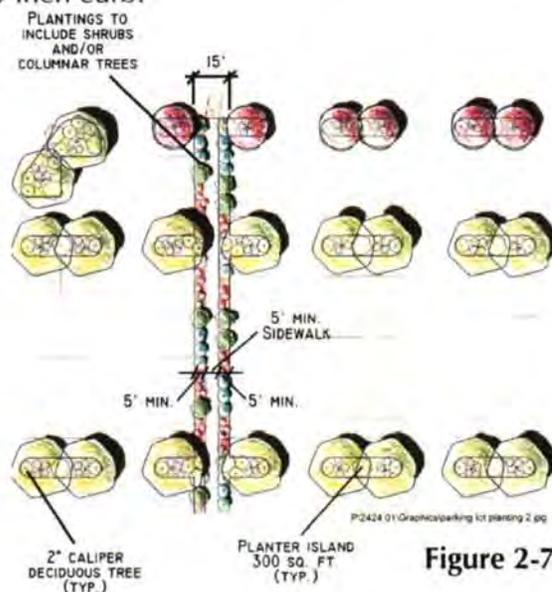


Figure 2-7 – Parking Lot Planting Concept

South Property Boundary:

1. The southern property line buffer (minimum 20-foot width), adjacent to the service lane and Galleria Parkway, shall include a mix of evergreen trees, 50% at 6 feet, and 50% at 8 feet in height at the time of planting (refer to Figure 2-8, below). This includes the southern access drive into the project.
2. Deciduous trees (2-inch minimum caliper) shall also be included to add visual interest and color.
3. Trees shall be planted at a rate of one tree per 15 lineal feet and shall be staggered in order to buffer service areas.
4. The overall planting mix shall consist of $\pm 80\%$ evergreen plantings, and $\pm 20\%$ deciduous.
5. Shrubs shall be planted at a rate of 6 shrubs (5 gallon minimum) for each required tree.
6. Retaining walls within the landscape buffer shall be permitted and included within the landscape calculation.

**Figure 2-8 – South Property Line Planting Concept**

West Property Boundary:

1. The western property line shall include a mix of evergreen trees, 50% at 6 feet, and 50% at 8 feet in height at the time of planting (refer to Figure 2-9, below).
2. Deciduous trees (2-inch minimum caliper) shall also be included to add visual interest and color.
3. Trees shall be planted at a rate of one tree per 25 lineal feet and shall include clustering in order to create visual interest, buffer service areas, and frame architectural elements.
4. The overall planting mix shall consist of $\pm 50\%$ evergreen plantings, and $\pm 50\%$ deciduous.
5. Shrubs shall be planted at a rate of 6 shrubs (5 gallon minimum) for each required tree.
6. Retaining walls within the landscape buffer shall be permitted and included within the landscape calculation.
7. The western boundary on the parcel labeled "Future Development" (southeast corner of Los Altos Parkway and Pyramid Lake Highway) shall provide a 25-foot landscaped perimeter following the landscape planting standards for Galleria Parkway, excluding the meandering sidewalk.

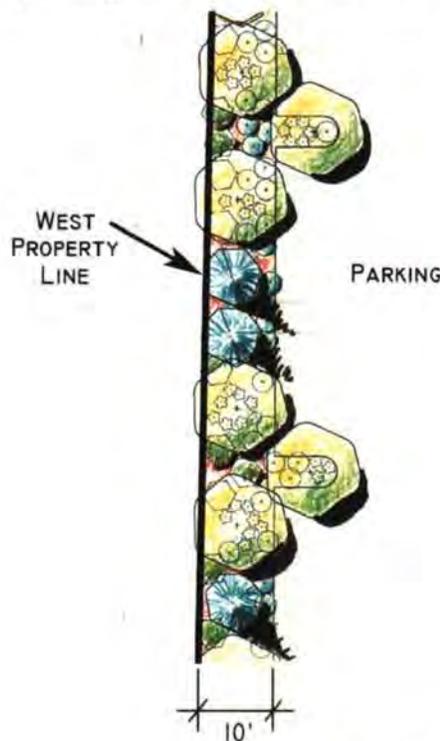


Figure 2-9 – West Property Line Planting Concept

Lawn Areas:

Where possible, a “water catchment area” or a planting bed shall separate lawn areas from hard surfaces such as streets and sidewalks in order to catch runoff (refer to Figure 2-10 below).

In areas where turf is used, the tree and shrub standards for the applicable perimeter or other areas shall apply.

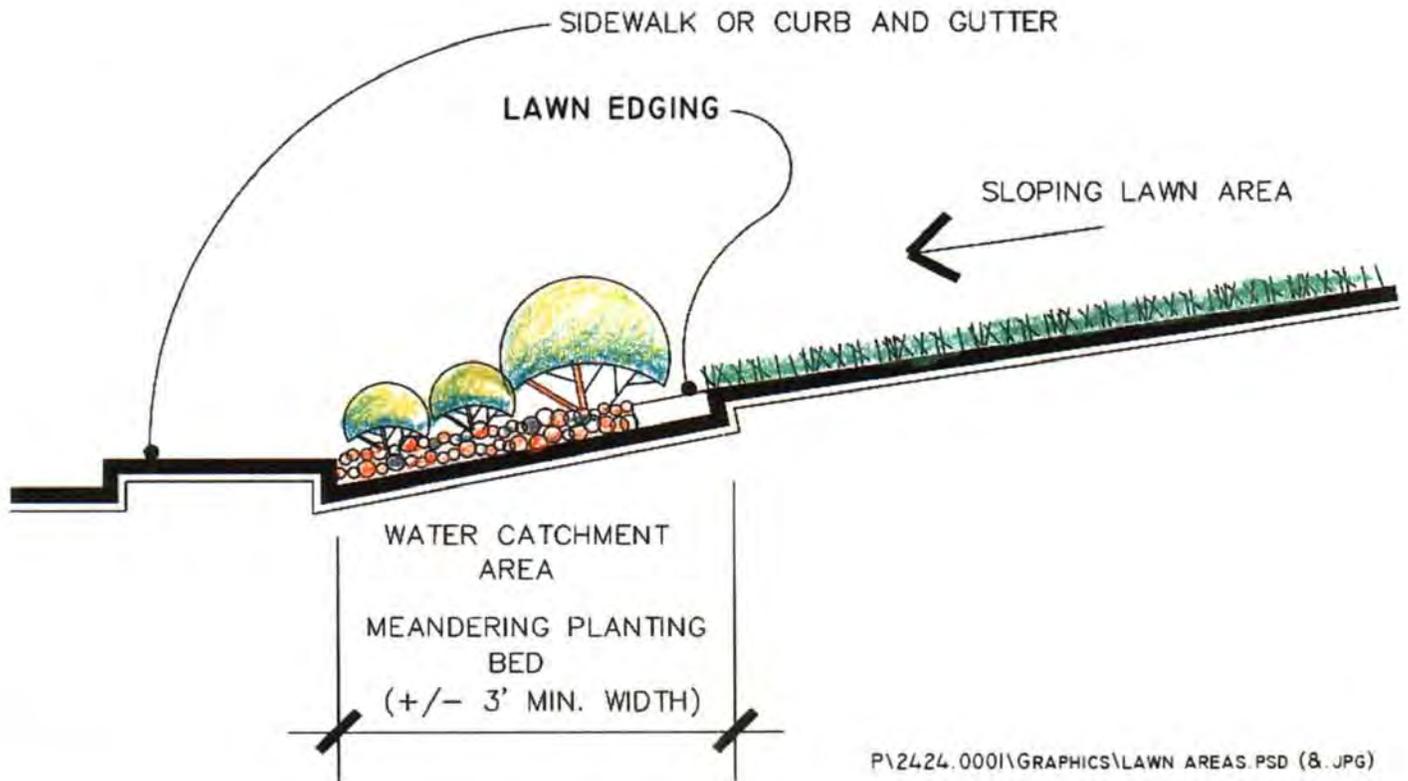


Figure 2-10 – Lawn Areas

Trash Enclosures:

1. Trash enclosures visible from Los Altos Parkway, Galleria Parkway, primary parking lots, and plazas shall be screened with evergreen plant material, combining both tree and shrub varieties, as well as vines (refer to Figure 2-11, below). Colors and materials of the enclosure must match the overall architectural theme of the project. The planter area shall be a minimum of 4 feet surrounding three sides of the trash enclosure.
2. A minimum of one 6-foot (at time of planting) evergreen tree and 6 shrubs (5-gallon minimum) shall be planted for three sides of the trash enclosure.
3. Trash enclosures and trash compactor units located at the back sides of buildings do not require landscape treatments as long as they are screened through the use of masonry walls which blend with the building architecture (colors and materials).
4. Trash enclosure openings shall include gates that provide for a minimum 80% screening of dumpster areas. Chain-link and slatted chain-link gates shall be prohibited. Trash compactor units shall not be subject to this standard.

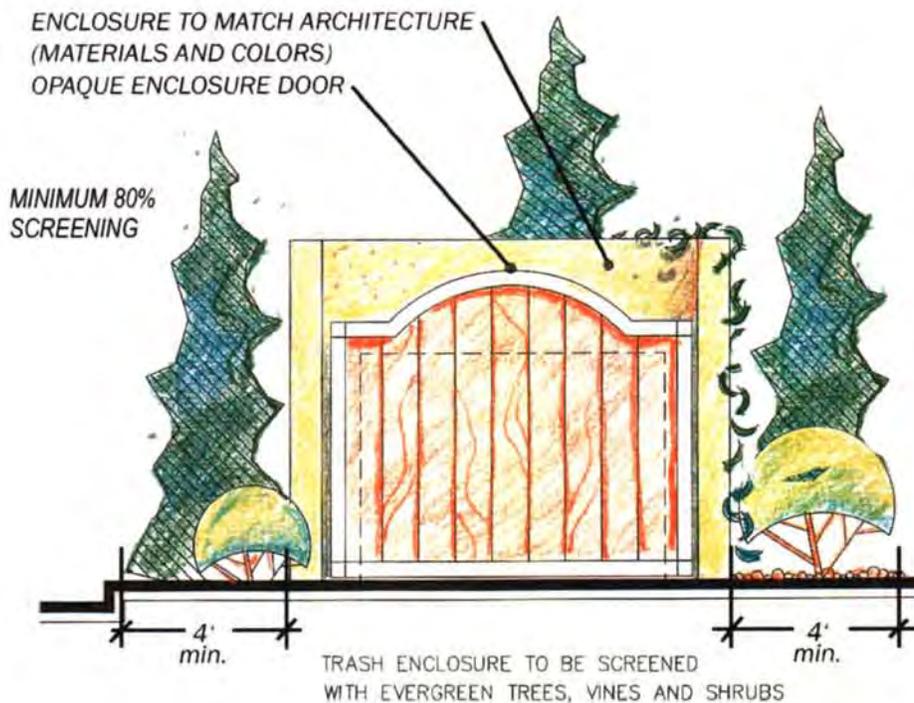


Figure 2-11 – Trash Enclosure Screening Concept

Maintenance:

1. Maintenance of common areas, parking lots, perimeter landscaping, streetscape, etc. as well as the general upkeep of the center shall be the responsibility of the Master Developer, listed below:

AIG Baker Sparks, L.L.C.
1701 Lee Branch Lane
Birmingham, Alabama 35242
Tel: (205) 969-1000 Fax: (205) 969-1017

2. At project build out, the Master Developer may choose to form a Tenant's Association to assume maintenance responsibilities. Any Association formed shall be approved by the City of Sparks Community Development Department.

Site Grading

The overall grading concept for Sparks Crossing is to provide for building areas and efficient circulation while ensuring that negative visual impacts from cut slopes do not occur. Grading will be used to ensure proper relationships between Sparks Crossing and adjacent properties and roadways.

Standards:

1. Views shall be considered with the final grading plan to ensure a minimal visual impact to surrounding properties.
2. Naturalistic grading will be used where complex re-contouring and revegetation must occur. Continuous expanses of land forms will be created to look natural as opposed to contrived or manmade. Where used, architectural or structured berms (i.e., retaining walls, earth buildings, sculptural land forms, etc.) will be an integral part of the architectural and landscape theme of the project, including consideration of color. Darker colors are preferred except where native materials are used.
3. Transitions at top and toe of graded slopes will be rounded to blend with the natural terrain. Abrupt, squared off transitions are not permitted, except where part of a traditional/symmetrical landscape design, or where less than 4-5 in height.
4. Retaining walls consisting of materials such as native stone or modular blocks will be used where grading dictates. Maximum height of rockery walls shall not exceed 6 feet per terrace (minimum 6-foot wide bench). Walls in excess of 6 feet without terrace shall utilize a modular system.

5. Wall colors must be consistent within a given area. Rockery walls shall incorporate the use of staining in order to blend with the natural and built environments.
6. The design of Sparks Crossing accommodates the grade differential across the project site. This shall be achieved by providing grade transitions between anchor tenants. Maximum transition shall be 2 feet between building faces (excluding plaza areas). North facing grade transitions shall be heated (excluding plaza areas) to prevent ice build up during winter months. Furthermore, all grade transitions used along any building sidewalks will meet or exceed ADA standards (maximum grade of 5% or 20:1). Any remaining grade differential shall be made up with the drives to Shoppers Way and Galleria Parkway.

Subsequent Review Requirements

As outlined under the Allowed Uses section, all uses within Sparks Crossing require subsequent review by the City of Sparks Community Development Department in the form of either a Site Plan Review or Special Use Permit.

Site Plan Reviews/Special Use Permits for uses within Sparks Crossing shall use the City of Sparks application forms and include the applicable filing fee, complying with all submittal requirements as outlined on the City application form(s). Processing of these requests shall follow the guidelines/timeframes established by the City of Sparks.

Project Phasing

By traditional phasing standards, Sparks Crossing is a single phase shopping center being developed by a single Master Developer. The Master Developer for Sparks Crossing shall be required to submit improvement plans for the entire 41.5 acre site addressing site access, circulation, parking areas, building pad areas, underground utilities, common/landscape areas, parking lot landscaping, plazas, perimeter landscaping and building pad locations. All of these improvements will be constructed with the initial development. There shall be no permit issued until review and approval of the improvement plans. No approval or issuance of permits until there is written proof of approval from the Orr Ditch Company provided to the City.

Undeveloped pad areas not included in the initial phase of construction will either be seeded with turf or treated with a Washoe County District Health Department approved dust palliative with perimeter silt fencing in order to ensure that blowing dust does not occur.

All new construction will process a Site Plan Review or Special Use Permit, as described in the Subsequent Review Requirements section above.

Administrative Approval for Minor Revisions

The Administrator shall have the authority at his/her discretion to administratively approve minor deviations in the plans, standards, and guidelines as requested by the Master Developer prior to the submission of a final development plan for each phase or pad area. Minor deviations include, but are not limited to, parcel configurations, parcel sizes, irregular lots, building heights, setback conditions, etc. (up to 20%). Any deviation or modification that exceeds 20% will require an amendment of the Final Planned Development Handbook.

The boundaries depicted in the Sparks Crossing Conceptual Plan are conceptual and subject to modification. A parcel developer may submit a commercial subdivision, parcel map, or boundary line adjustment, that deviates no more than 20% from the planning area boundaries shown on the Sparks Crossing Conceptual Plan. Proposals to modify the boundaries in excess of 20% shall constitute the need for an amendment to the Final Planned Development Handbook.

Design Flexibility

The final development plan, standards, and regulations contained in this handbook are intended to depict the general nature and intensity of the development proposed within Sparks Crossing. Sufficient flexibility shall be allowed to permit detailed planning and design at time of actual development. The configuration and acreage of development parcels and phases may be altered from what is shown on the Conceptual Site Plan to accommodate detailed site conditions.

Omissions

In cases where the handbook does not specifically address a standard/subject, the provisions of the Sparks Municipal Code, Sparks Design Standards Manual, and NRS in effect at the time shall prevail.

Chapter 3 – Architectural Development Standards

Introduction

1. The purpose of these Architectural Development Design Standards is to provide the design criteria and standards necessary to achieve a visually coordinated, balanced and appealing shopping environment within Sparks Crossing while encouraging individual expression and identification through design criteria.
2. The standards described herein have been created to enhance Sparks Crossing as a regional shopping center. The design standards have been developed in relation to the site architecture, landscaping and surrounding environment. The requirements for building elements described herein are intended to maintain the design character and provide architectural consistency throughout Sparks Crossing.
3. Performance as per the Design Standards shall be rigorously enforced by the Master Developer. Any non-conforming construction shall be removed by the tenant or his contractor, at tenant's sole expense, upon demand by the Master Developer.

Design Review and Approval Process

Improvements Requiring Review:

1. All Buildings, Pads and Parcel improvements constructed by Tenant or any other entity within Sparks Crossing will require review and approval by the Master Developer prior to the initial submittal to the City.
2. Improvements include, but are not limited to: new structures, tenant improvements, existing building modifications, wall and fascia signs, window signage and monument signs.
3. All new building construction will process a Site Plan Review or Special Use Permit as required by other sections in this Handbook.

Plan Submittal Requirements:

1. All plans for building and or site improvements submitted by the tenant or other entity shall be in accordance with the current requirements of the City of Sparks. All plans shall be prepared by a state-licensed professional architect, engineer, landscape architect, contractor or other approved designer.

2. Upon obtaining approval from The Master Developer and prior to developing construction documents, each tenant shall submit Master Developer approved and signed drawings, specifications or other supporting documentation, applications and associated fees to the appropriate City of Sparks authority for Design Review (Site Plan Review or Special Use Permit) approval. The minimum drawing requirements for Master Developer review shall consist of the following:

Floor plan:

Indicate and dimension exterior walls, doors, ramps or stairs, canopy columns and pilasters as occur.

Exterior Elevations:

Front, sides and rear elevations depicting all exposed walls/canopies, loading dock and trash enclosure screening, roof-top equipment screening, utility locations and any other exterior appurtenances. Provide an exterior materials sample board with materials keyed from the elevations.

Once approved by the Master Developer, appropriate plans, etc. may then be submitted for applicable construction permits to the City of Sparks.

3. All submittals to the City of Sparks will require proof of approval by the Master Developer.

Regulations:

1. These Development Standards are intended to conform to all standards of the City of Sparks Municipal Code and Design Standards Manual. Nothing contained herein is intended to prevent the erection, construction and maintenance of official traffic, safety, fire and police activities or structures as mandated by public authorities.
2. The Design Standards contain restrictions regulating the construction and maintenance of Sparks Crossing and is subject to interpretation by the Master Developer and Administrator. The Master Developer may amend the Development Standards Handbook from time to time to accommodate specific site or temporary requirements necessitated by future tenancy within Sparks Crossing provided such amendment is consistent with the basic objectives of the Design Standards. All amendments must be approved by the City of Sparks.

Site Design Standards

General:

1. The overall site design concept will establish a set of individual standards creating a site organization and design in which walkways, landscaping, lighting, signage, amenities and building designs reinforce each other and create a cohesive project.

Visual Orientation:

1. Create a strong sense of visual orientation within the project. Allow patrons to consistently find points of reference within the project to locate and orient them by. Moreover, arrange the site elements in such a way that the organization of the buildings, public areas, entries, gateways and site circulation are clear and easily oriented to, and reinforce the project design.

2. Tower Elements:

Provide seven (7) Tower Elements where indicated on Figure 3-1. Tower elements shall be used to provide visual orientation within the site and will typically be used at corners at plazas and at end locations of shop buildings. Refer to Figures 3-17, 3-18, 3-33, 3-34, 3-37, 3-39, 3-43, 3-46, 3-51 and 3-52 for conceptual Tower Element designs. Towers may be free-standing or attached to the buildings.

Where a tower element doubles as a tenant entrance feature, it shall be allowed to have signage placed upon the lower portion of the facade per the Sign Standards.

Refer to Tower Elements on page 3-22 for design details.

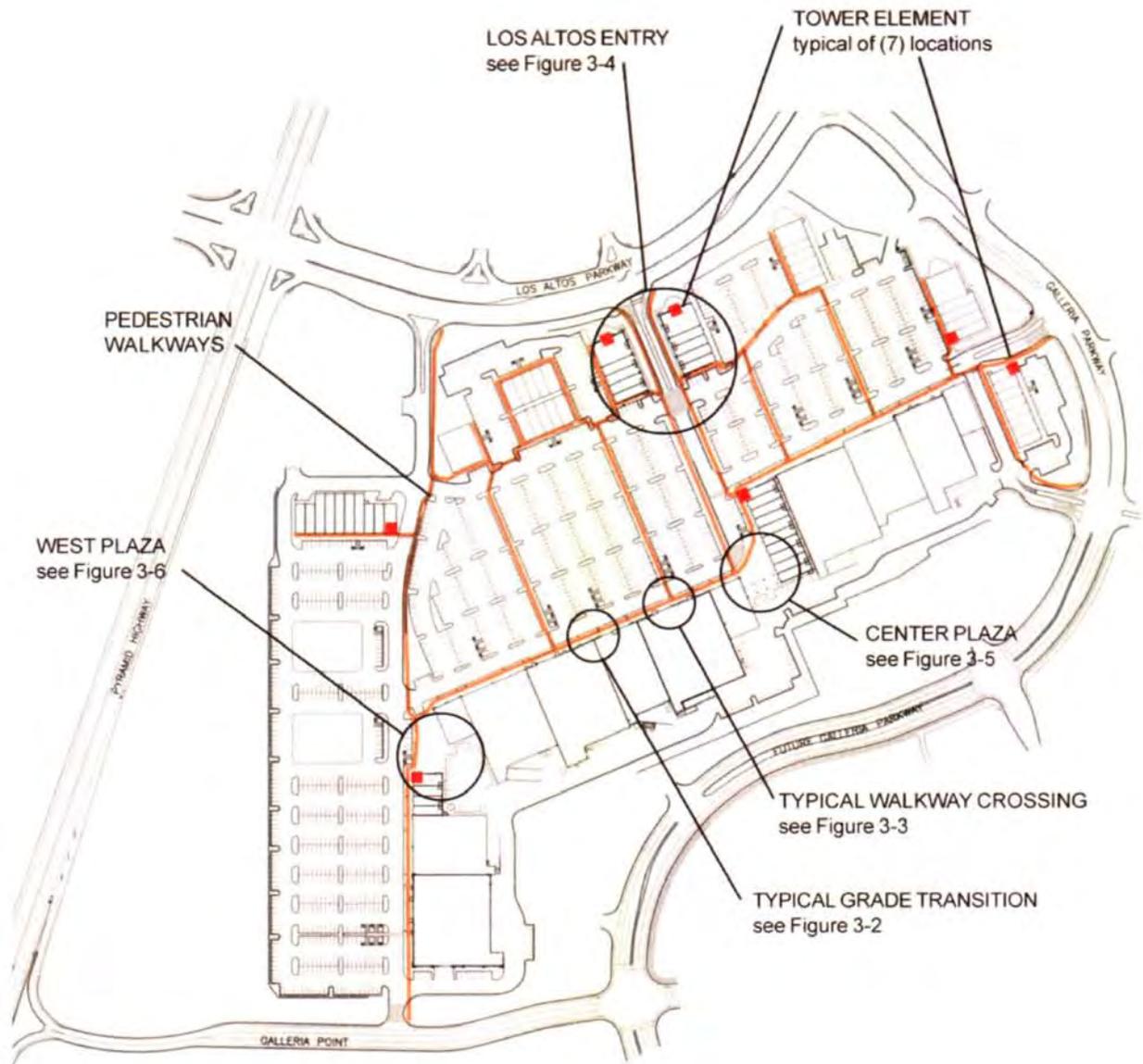


Figure 3-1 - Reference Site Plan

Pedestrian Circulation:**Walkways and Sidewalks:**

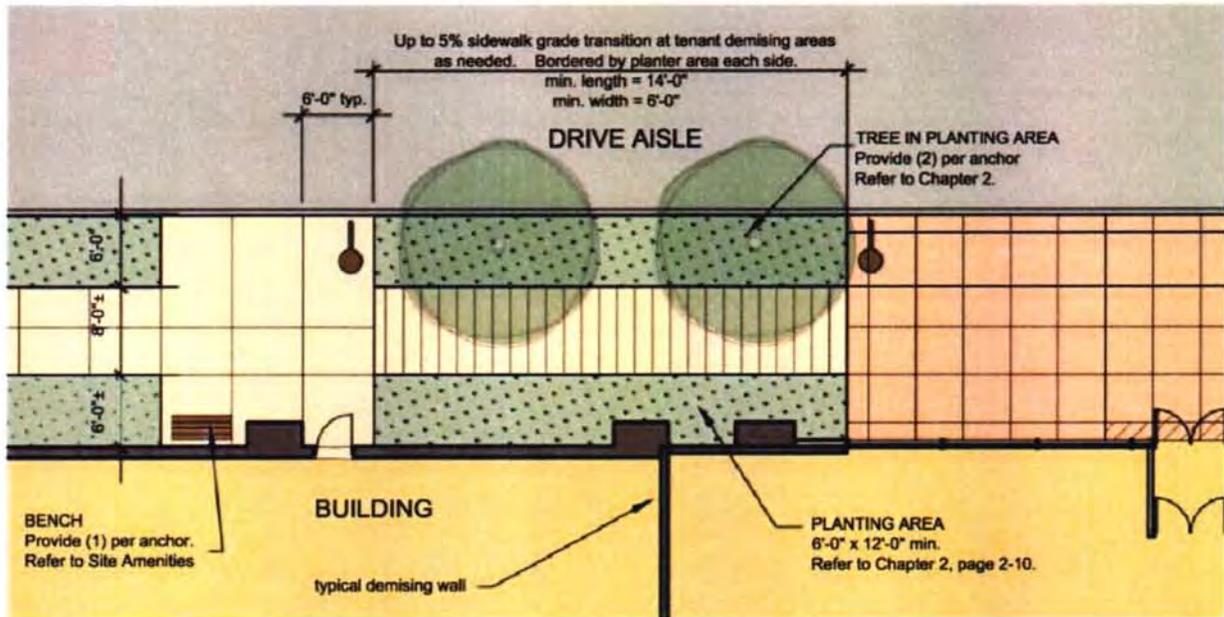
1. Provide a pedestrian route or "corridor" from all drive entries connecting with the interior circulation system. This corridor shall consist of a landscape buffer area on each side of the main driveway and a minimum 5 foot wide sidewalk on one side leading from the street to the first cross drive, except at the main Los Altos Parkway entry where sidewalk is required on two sides. Refer to Figures 3-1, 3-4 and 3-5.
2. Provide a clear and well developed internal pedestrian circulation system. This system is to inter-connect all tenants and lease spaces within the project along pleasant, clearly defined walkways. Refer to Figure 3-1 Reference Site Plan.
3. The minimum clear sidewalk width which passes or is adjacent to the front of a building shall be a minimum of 8 feet wide at a Major Anchor building. Mini Anchor, Shops and Pad buildings shall be a minimum of 6 feet wide. All such sidewalks shall provide a connection to the internal on-site pedestrian circulation system.
4. Provide a minimum 5 foot wide sidewalk connecting any bus stop directly adjacent to Sparks Crossing with the internal pedestrian circulation system.
5. Provide a minimum 5 foot wide sidewalk connection from any designated on-site "Park and Ride" area to the internal pedestrian circulation system.
6. Provide walkway, landscaping planter areas and benches as illustrated in Figure 3-2 at the front of all anchor tenant buildings. Refer to landscaping standards for building frontage walkways in Chapter 2, Page 2-10 for additional requirements.

Pedestrian Crossings:

1. Typical pedestrian crossings shall be colored concrete with 1" deep hand-tooled joints. Expansion joints are to be sealed with traffic bearing sealant. Refer to Figure 3-3, 3-4 and 3-5.

Typical Grade Transitions

1. The transitional areas between anchor tenants are to be provided with pedestrian scale landscaping, benches and wall or pole mounted lighting. Refer to Figure 3-2.
2. The grade change transition area shall be accomplished by the use of a 5% maximum slope ramp area bordered by planting areas.
3. At north-facing buildings only: Provide heated sidewalk at grade transitions to prevent ice build-up. This provision shall apply to any north-facing pedestrian sidewalk area where ramps occur. Refer to Figure 3-2 and Chapter 2, Page 2-20.



Note: Sidewalk grade transition to be heated at north-facing buildings.

Figure 3-2 - Plan of Example Hardscape and Planters at Anchor Building

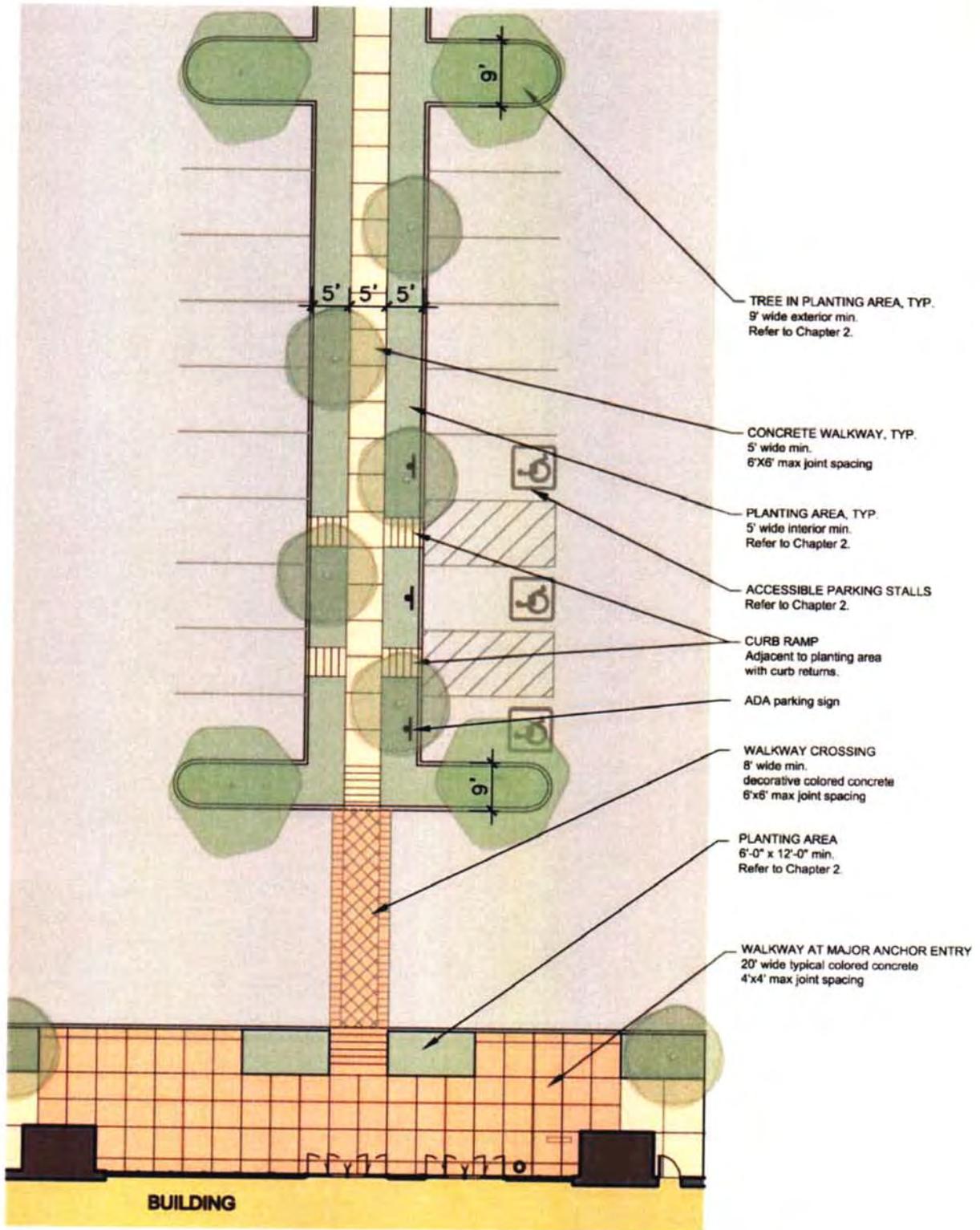


Figure 3-3 - Plan of Typical Walkway Crossing

Public Spaces:

(Refer to Chapter 2 for additional standards)

Los Altos Parkway Entry

1. Create a gateway to the retail district reinforcing the flanking buildings with landscaping, walkways, lighting amenities and signage. Refer to Figure 3-4.

Galleria Parkway Entry

1. Create a gateway to the retail district reinforcing the flanking buildings with landscaping, walkways, lighting amenities and signage. Refer to Figure 3-4 for similar arrangement of elements.

Plaza Areas:

1. Create several small public plazas, at different scales, with different characters to serve as the visual and social foci of the project. Plazas shall be located as indicated on Figure 3-1 - Reference Site Plan.
2. Outdoor Seating with tables is allowed and encouraged, particularly when the plaza is located in the vicinity of restaurants, bookstores, coffee shops and other similar uses. A minimum 8 foot clear pedestrian walkway shall be maintained where the plaza encompasses the sidewalk.

Center Plaza:

1. Provide a Center Plaza as located on Figure 3-1 to serve as the active center of the retail district and to accommodate drop-off and pick-up functions. Refer to Figure 3-5.

West Plaza:

1. Provide a West Plaza as located on Figure 3-1 to serve as a more passive public space for the retail district and to accommodate potential outdoor seating areas for the adjacent tenants. Refer to Figure 3-6.

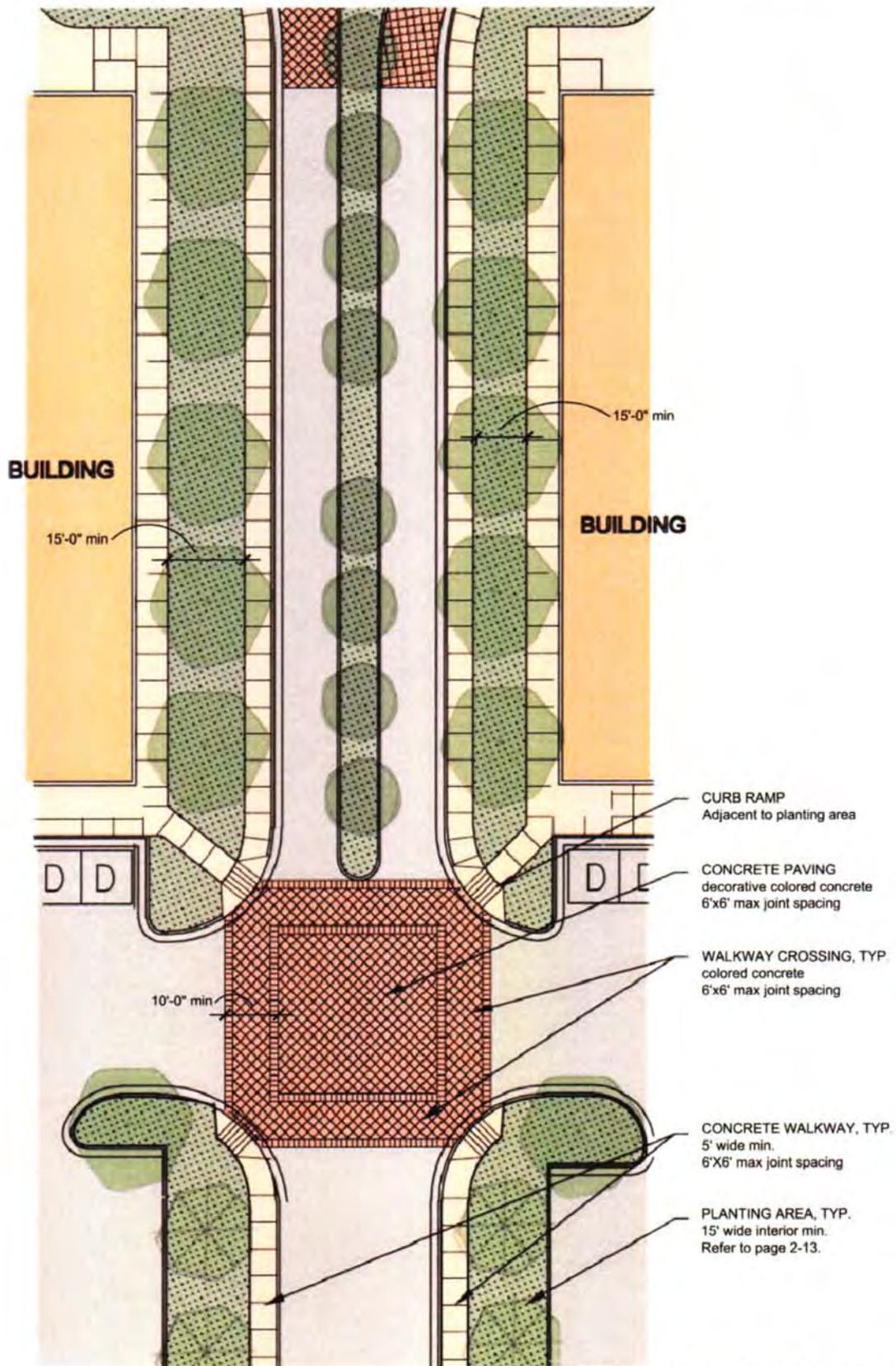


Figure 3-4 - Plan of Los Altos Parkway Entry

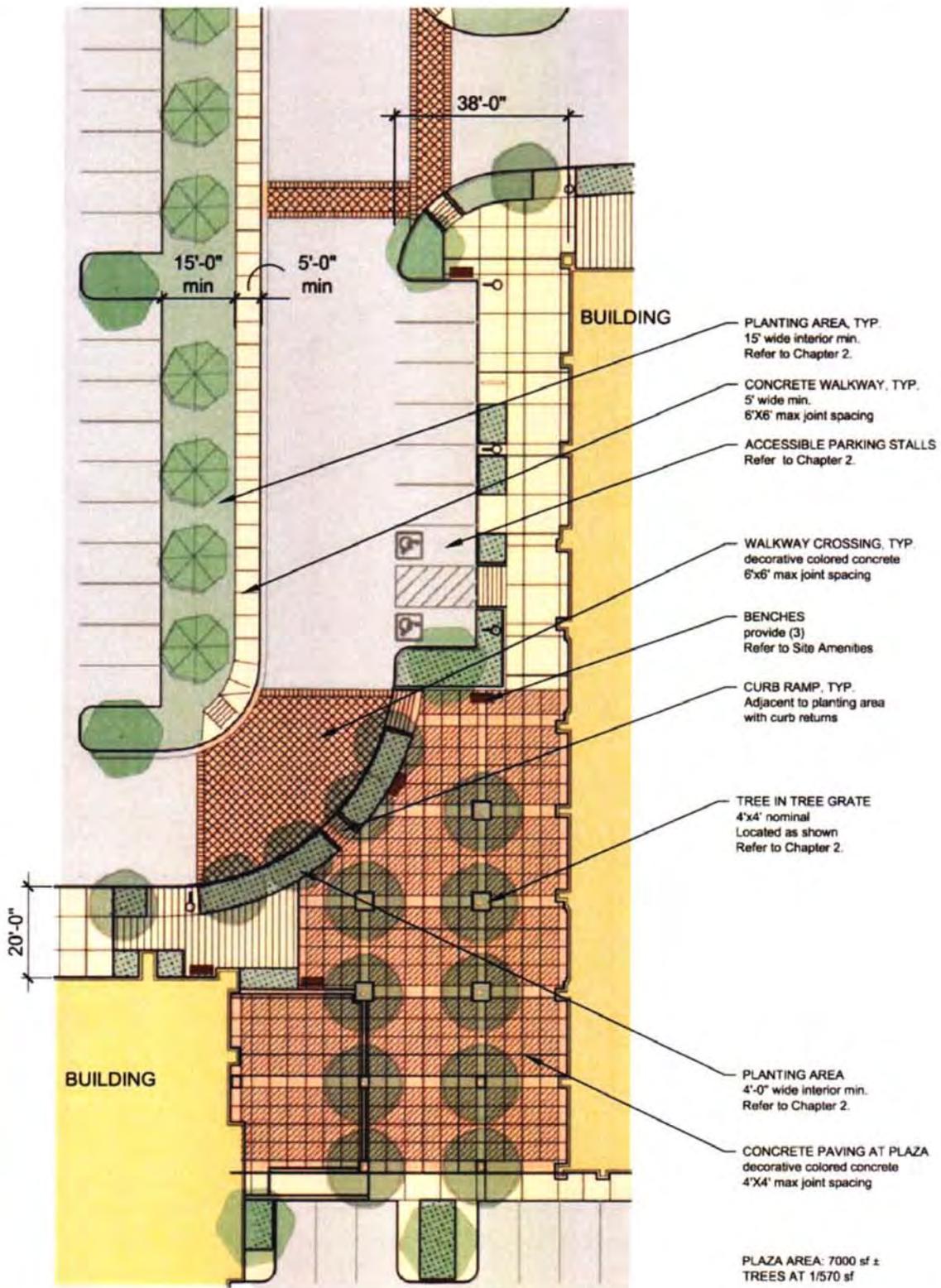


Figure 3-5 - Plan of Center Plaza

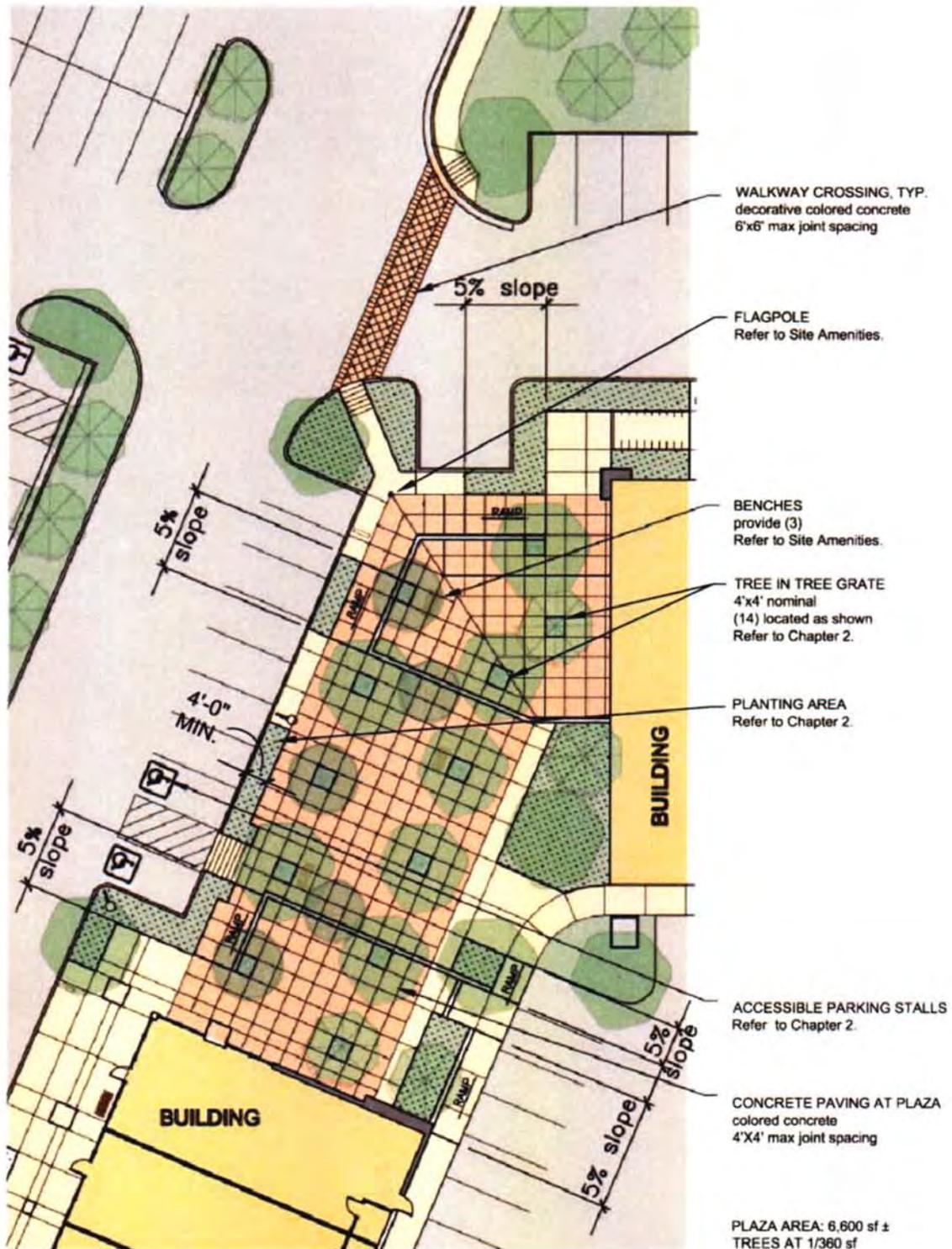


Figure 3-6 - Plan of West Plaza

Site Lighting:

Building Mounted Sidewalk Lighting

1. Accent light fixtures shall be placed on the simulated stone columns flanking each side of the anchor tenant entries and shall be controlled photocell on and time clock off. Refer to Figure 3-7 for example. Actual product is intended to be all black.
2. The accent light fixtures and bracket mounting shall be as manufactured by Architectural Area Lighting (or equivalent). Provide the following product:

2-UCS VSL FLR 50MH BLK



Figure 3-7 - Building Mounted Sidewalk Lighting

Building Mounted Area Lighting

1. Provide 400 watt metal halide wall packs at 40 feet on center along the rear (loading area) walls of all buildings or as per the photometric lighting plans as required to provide one (1) foot candle minimum at rear of buildings. Color shall be black. Free standing buildings shall utilize decorative accent light fixtures on all sides. Wall packs are to be controlled photocell on and time clock off. Cut-off type fixtures shall be used to ensure no light spills over to the adjacent right-of-way or boundary of Sparks Crossing. Refer to Figure 3-8.



Figure 3-8 - Building Mounted Area Lighting

Pole Mounted Sidewalk Lighting:

1. This type of lighting is to be pole-mounted at a height of twelve (12) feet above the sidewalk and shall not conflict with adjacent on-site traffic flow or the minimum clear sidewalk width described elsewhere herein. Refer to Figure 3-9.
2. Pedestrian scaled light fixtures shall be as manufactured by Architectural Area Lighting (or equivalent). Provide the following product:

UCM VSL FLR BLK 100HPS

3. Provide a minimum of: Two (2) sidewalk light fixtures per anchor tenant. Mini-anchor tenants and Shop buildings shall be provided with sidewalk light fixtures at forty (40) feet on center. Lighting shall be 100 watt HPS. Fixtures are to be controlled photocell on and time clock off.
4. Metal poles, fixture housings, access covers, etc. shall be factory finished in black.



Figure 3-9 - Pole Mounted Sidewalk Lighting

Pole Mounted Area Lighting:

1. Site lighting and signage is to be photocell on and time clock off, except that a minimum of 25% of the site lighting fixtures shall be photocell on and off and shall be evenly dispersed throughout the site. There shall be no light spillage onto adjacent right-of-way or beyond the boundary of Sparks Crossing. "Cut-off" type fixtures shall be used.
2. Site light fixtures shall be 30 feet above the finished surface of the parking lot. The fixtures shall be on 27 foot high poles mounted to a 2 foot diameter, 3 foot exposed height concrete base.
3. Metal poles, fixture housings, access covers, etc. shall be finished in black. See Figure 3-10.

4. Quantity of light fixture heads per light standard shall be as per the photometric lighting plans as required to provide up to a maximum of seven (7) foot candles and shall not spill onto the adjacent right-of-way or beyond the boundary of Sparks Crossing.



Figure 3-10 - Pole Mounted Area Lighting

Site Amenities:

1. Outdoor Public Seating:

The Master Developer shall provide and install metal benches anchored to sidewalks with expansion type anchors. Benches shall be a minimum of six feet long. Color to be black. Install a minimum of 1 bench per anchor store and 1 bench per 100 linear feet of shops and/or mini-anchor space. Refer to Figure 3-1. Provide the following model:

DUMOR 58 SERIES (see Figure 3-11)

The placement of benches shall not interrupt or otherwise conflict with the minimum sidewalk clear width. Refer to the "Plaza Area" section contained herein for a description of additional seating required in those areas.



Figure 3-11 - Outdoor Public Seating

2. Trash Receptacles:

Provide and install decorative metal trash receptacles anchored to sidewalks with expansion type anchors. Color to be black. They are to be located in a conspicuous location near tenant entries and/or bench locations. Removal of debris and maintenance of the receptacles shall be the responsibility of the common area maintenance company designee to be chosen by the Master Developer. Receptacles shall be 32-gallon capacity with cover and shall be the following model:

DUMOR 84 SERIES, MODEL 84-32 WITH DOME COVER
(See Figure 3-12)

Minimum quantity requirements:

- One per anchor tenant;
- One per shop or mini-anchor tenant building or one per 100,000 SF;
- One per free-standing building/tenant;
- One per plaza area;



Figure 3-12 - Trash Receptacles

3. Bicycle Racks:

The Master Developer shall provide and install metal bicycle racks surface mounted to sidewalks with theft-resistant expansion type anchors. Color to be black. Each major anchor, free-standing tenant building, shops building and mini-anchor tenant building shall provide a minimum of two (2) bicycle parking spaces per building. In order to preserve pedestrian walkways, up to eight (8) spaces may be grouped together where tenants/buildings are adjacent to each other. Bicycle racks shall be located so as to not impede pedestrian walkways. See Figure 3-13.



Provide serpentine shape for locations which require multiple bicycle parking spaces



Provide inverted U-shape for locations which require two bicycle parking spaces.

Figure 3-13 - Bicycle Racks

5. Flagpoles:

The Master Developer shall provide a flagpole, with a United States of America flag, suitable for on-site wind conditions as per local code or ordinance. Pole to be Fifty (50) feet tall maximum, fabricated from seamless aluminum cone tapered tubing 3/16" thick minimum. Flag size shall be determined by pole size, height and wind restrictions. Provide with finial ball, external halyard, flag snaps, clips and all accessories necessary for a complete installation. Finish to be selected by the Master Developer.

Provide in-ground metal halide up-lighting with photo cell on and off functions, to be approved by the Airport Authority of Washoe County.

6. Bollards:

The Master Developer shall provide and install metal bollards where required for storefront security as determined by the master developer, or where parking is flush with sidewalk. Bollards shall be a minimum of three feet tall and 6" in diameter. Color to be black. Provide the following model: CREATIVE PIPE CBR-6-3MB-E-P-D or equivalent. See Figure 3-14.



Figure 3-14 - Bollard

7. Tree Grates:

The Master Developer shall provide and install metal tree grates in sidewalks at plazas as indicated in Figures 3-5 and 3-6. Tree grates shall be a nominal 4' x 6' or 4' x 4'. Color to be black. Provide the following model: FAIRWEATHER STA 4872 or 4848. See Figure 3-15.



Figure 3-15 - Tree Grate

8. Shopping Carts:

Where any given tenant requires exterior cart storage at the entrance, said enclosure shall effectively screen the carts. The enclosure shall be integrated with the building architecture and be of the same materials and finishes as the adjacent buildings and shall not reduce sidewalk to less than the required minimum.

Where any given tenant requires shopping cart corrals to be located in the parking lot, the corrals shall be located at the center of every other parking aisle in front of that tenant/business. Cart corrals located in the parking lot shall be provided and installed by tenant and shall not occupy required parking spaces.

Building Design Standards

Introduction:

1. The intent of these Design Standards is to establish standards for building form, materials, colors, lighting and signage which will assure a cohesive development with consistent architectural character. While allowing individual expression between different tenants and uses, the provisions will accommodate a range of building sizes, uses and tenancy.
2. Acknowledging that each tenant has specific criteria, consideration of a tenant's regional or national identity will be taken into account during the Master Developer's and the City of Sparks review.
3. The color palette draws from the surrounding landscape, and is made up of saturated and muted tones of copper, terra cotta and sage. Refer to Appendix Exhibit K.
4. Building forms shall be simple, balanced compositions that respond to the adjacent buildings and to adjacent pedestrian walkways. See specific provisions for each building type.
5. The local climate provides a comfortable outdoor environment for most of the year; however, provisions for sun shading devices are to be provided when the public side faces south or west.
6. It is intended that all buildings will be designed incorporating four-sided architecture concepts. Blank wall architecture is prohibited; see specific provisions that follow for each building type.
7. Refer to Figure 3-16 for a reference key map for the 2-dimensional representation of the building architecture for each building or group of buildings indicated at Figure 3-17 through Figure 3-53.
8. Refer to Figure 3-54 through 3-76 for a 3-dimensional representation of the building architecture and surrounding landscape/hardscape.
9. The colored elevations are subject to interchanging elements while maintaining the rhythm and intensity of the design. The minimum depth dimensions for entry elements, towers, walls, pilasters, canopies, wall fins and secondary elements shall be maintained at the minimum or greater dimensions as stated in the standards and or figures.

General Architectural Standards:

The design requirements for buildings at Sparks Crossing are as defined below. With each building *element*, Roofs, Parapets, Walls, etc, are listed the respective *design requirements*, such as **Roof Form Variation**, **Parapet Height Variation**, etc. The application of each *design requirement* is defined for each building type in the *Building Types* section that follows.

1. Roofs:

The visible profile of any roof line shall not run continuously for more than fifty (50) feet. Methods of varying the roof profile include horizontal and vertical offsets of the ridge, eave and overhang lines, or similar design elements. Refer to specific provisions for each building type as illustrated in Figures 3-16 through 3-53 for the following:

Roof drains, leaders and downspouts shall be incorporated into the exterior building design.

2. Parapets:

Defined as the portion of a vertical wall that extends above the plane of the roof.

Parapets shall conceal flat roofs and roof-top equipment from public view as viewed from the adjacent right-of-way.

Provide dimensional cornice treatments at all parapets with the vertical dimension of sixteen (16) inches minimum and the horizontal projection of six (6) inches minimum.

- **Dimensional Cornice;**

The visible profile of the parapet shall not continuously run more than fifty (50) feet at Major Anchors, forty (40) feet at Shop Buildings and thirty (30) feet at Mini Anchors and Pad Buildings without providing for horizontal or vertical offsets of two (2) feet minimum. Refer to specific provisions for each building type as illustrated in Figures 3-16 through 3-53 for the following:

- **Parapet Height Variation;**

Any parapets or sidewalls above roofs shall be finished with EIFS or other finish material to match color used at adjacent walls.

3. Building Walls:

Blank wall architecture is not permitted. All buildings are required to provide visually consistent design on all sides. There shall be no long expanses (max. 50 feet at Major Anchors, max. 40 feet at Shop Buildings and max. 30 feet at Mini Anchors and Pad Buildings) of flat wall planes vertically or horizontally on all sides of buildings; see specific provisions, including limits on length of flat wall planes, for each building type.

Articulation shall include the use of pilasters, horizontal or vertical wall offsets, recessed openings and storefronts, covered arcades, canopies, wall projections, color, and texture and material changes. Refer to specific provisions for each building type as illustrated in Figures 3-16 through 3-53 for the following:

- **Wall Face Variation;**
- **Pilasters;**
- **Upper Wall Break;**
- **Wainscot;**
- **Canopies and Wall Fins;**
- **Corner and Demising Wall Variation;**

All buildings along the streets (includes Mini Anchor G, all Shops Buildings, Pad Buildings and "Future Development") are to provide a minimum 4'-0" high stone veneer wainscot . The Major Anchors, Mini A, Mini C and Mini F shall have a wainscot of painted split face CMU or stone veneer to be reviewed and approved with Site Plan Review. Refer to Exhibit L. This height may be increased for good proportionality. The wainscot may be interrupted by building fenestration or storefront glazing. See Building Types text for application.

Building façade materials and wainscot used on the front elevation are to be used on the side and rear elevations in a consistent manner.

4. Entry Elements:

Entry elements shall be provided consistent with the provisions of each building type. Refer to specific provisions for each building type as illustrated in Figures 3-16 through 3-53 for the following:

- **Entry Element;**

Provide manufactured stone veneer at columns or pilasters to the top of parapet or bottom of roof structure or eave line.

5. Secondary Elements:

Secondary elements shall be provided consistent with the provisions of each building type. Refer to specific provisions for each building type as illustrated in Figures 3-16 through 3-53 for the following:

- ***Secondary Elements;***

Provide manufactured stone veneer full height at columns or pilasters to the top of parapet elevations.

6. Arcades:

Arcades and or covered sidewalks may be provided at primary facades facing south or west. Arcades and or covered sidewalks are not permitted at primary facades facing north. Provide manufactured stone veneer at columns or pilasters to the base of the roof structure or eave line.

7. Storefront & Glazing Systems:

Frames shall be dark bronze anodized aluminum finish. Tenants may propose alternate frame colors per tenant criteria, however all colors shall be complementary to the project color palette as shown in Appendix Exhibit K, and shall be approved by the Master Developer and the City of Sparks. Refer to specific provisions for each building type as illustrated in Figures 3-16 through 3-53 and to Exhibit K for allowable materials and colors.

8. Building Mounted Lighting:

Provide decorative building mounted lighting at entry columns or pilasters. At arcades, wall or column mounted lighting may be used in lieu of pole mounted lighting. See Building Mounted Sidewalk Lighting.

9. Building Color:

All building elements must conform to the approved color palette as shown in Appendix Exhibit K.

Exception:

Large, single areas of bright or intense colors up to 25% of any exterior wall expanse for a single tenant are allowed.

10. Accent Elements:

The following materials or elements shall be used as accent elements as shown on the architectural illustrations.

Simulated Stone Veneer;
Ceramic Tile;
Protected wood timbers;
Shutters;
False Windows;
Wall Fins;
Architectural grade pre-cast concrete;
Metal wall panels of various profiles (for use at signage areas)

All accent elements must conform to the approved color palette as shown in Appendix Exhibit K.

Exception:

Bright or intense colors may be used for accent provided that the total area of the accent does not exceed 10% of the total wall area.

11. Tower Elements:

Tower elements shall be a minimum of 40 feet high as measured from the adjacent building finish floor height. Tower elements may utilize a hipped roof with standing seam metal roofing at a minimum pitch of 4:12 or a parapet-type condition with dimensional cornice. All tower elements shall be 4 sided with matching architecture on each side. Refer to Figures 3-16 through 3-53 for additional requirements.

Materials:

1. Concrete Masonry Units:

Split face CMU: Shall be painted with a color as represented in the Color Palette. See Appendix K.

Smooth face CMU: Shall be used as single-course banding or as an accent bordered by other materials listed herein and painted with a color as represented in the Color Palette. See Appendix K.

2. Stone Veneer:

A single source and color shall be selected by the Master Developer and used by all buildings in the project.

3. Exterior Insulation and Finish System: (EIFS)

Texture of finish shall be a smooth, fine sand finish, except that other textures may be considered for Pad Buildings as approved by the Master Developer and the City of Sparks. Color shall be as represented in the Color Palette. See Appendix K.

4. Standing Seam Metal Roofing / Awning:

Provide panels with preformed seams 10" on center. Seam shall be 1-3/4" high. Finish shall be manufacturer's standard Kynar 500 Fluorocarbon finish. Systems utilizing snap on battens are not acceptable. Colors conforming to the Color Palette, see Appendix K, to be approved by The Master Developer and the City of Sparks. Once approved, this shall be the standard for the project.

Panels shall be factory finished and formed sheet panels, intended for lapped seam roofing installation.

5. Flat roofing:

The color for any "flat" roof material is to be white and shall be consistent throughout the entire project. This does not apply to canopies.

6. Canopies and Wall Fins:

Fabricate out of architectural steel tubes or channels with welded joints. Concealed fasteners are required, but exposed fasteners will be considered subject to approval by the Master Developer and the City of Sparks.

All items must conform to the approved color palette as shown in Appendix Exhibit K

7. Coping, Flashing and Sheet Metal:

All metal gutters, downspouts, copings, flashings, cap flashings, etc are to use Kynar pre-finished metal or equivalent as approved by The Master Developer. These items shall architecturally blend with surrounding materials or presented as accent colors.

All items must conform to the approved color palette as shown in Appendix Exhibit K . Galvanized metal is not an acceptable finish for sheet metal flashing.

Drive-Through Facilities:

1. Standards for drive-through facilities are intended to provide safe, efficient circulation and avoid site conflicts with pedestrian and other on-site vehicular movements. Design of these facilities shall provide a clearly identifiable circulation route and provide for a minimum of 8 vehicles stacked up to the first service window or a minimum of 140 feet.

2. Drive-thru driveways, queue lanes, service windows and speaker boxes must provide visual screening, including the light from vehicle headlights, from adjacent public right-of-way, open space or adjacent residential areas by use of plantings, landscape berms, walls, fencing or combination thereof. Speaker boxes shall not be heard beyond the property line.

Service Areas:

1. Loading and Trash Compactor Areas:

These areas shall not be located in a required setback, perimeter landscaping or directly visible from a public street and internal public areas.

Loading areas shall be completely screened by a minimum ten (10) foot high by minimum 70 foot long split-face CMU wall and shall only be open to allow delivery trucks access to the loading door. The screening shall be incorporated with and blended into the building design so that it is indistinguishable from the adjacent building..

Trash compactor areas shall be completely screened by a minimum ten (10) foot high by the trash compactor length split-face CMU wall and shall only be open to allow trash trucks access to the compactor. The screening shall be incorporated with and blended into the building design so that it is indistinguishable from the adjacent building.

2. Utility and Mechanical Equipment Screening:

Utility and mechanical equipment areas (including electrical and gas meters, electrical boxes, fire sprinkler post indicator valves, irrigation controllers, coolers and other such ground mounted mechanical equipment) shall be screened from public view with landscaping, architectural elements with materials and colors consistent with those of the adjacent building or contained within the building architecture. Fire sprinkler rooms shall be contained within the building architecture.

3. Trash Enclosures and Dumpsters:

All trash dumpsters and garbage bins shall be located within an enclosure which shall be designed to match the building and constructed a minimum six (6) foot high, of twelve (12) inch nominal depth split-face concrete masonry material painted to conform to the color palette shown in Appendix K. Each enclosure shall be designed so that the interior is at least 80% screened by the gate design.

Where an enclosure is directly adjacent to a building, it shall be incorporated as part of the building architecture. Approval of the trash enclosure design is required by the Master Developer, City and local waste disposal company.

Trash enclosures shall utilize metal framed doors/gates with solid, opaque metal panels. The gates and panels may be painted or pre-finished. The design of which shall be approved by the Master Developer and the City of Sparks. Colors shall conform to the color palette shown in Appendix K.

Wood or chain link gates (with or without slats) are prohibited.

4. Outdoor Storage:

All storage shall be contained within the building architecture or screened from public view by way of decorative walls and landscaping. Outdoor storage without a screening mechanism is prohibited. The surface of the area to be used for outdoor storage shall be paved with a non-permeable material such as asphalt or concrete or other such material acceptable to the City of Sparks administrator to control fugitive dust.

Chain link fencing and gates (with or without slats) are prohibited.

All outdoor storage shall comply with Sparks Municipal Code Section 20.41.070.

No outdoor storage or display of merchandise on sidewalk areas or parking areas except as allowed by the City of Sparks Municipal Code.

Building Types

Major Anchor Buildings:

1. Defined as any single tenant in a building space equal to or greater than 12,000 square feet.
2. Entry Elements shall be provided such that they are proportionate with the tenant's frontage and are limited in height to forty-three (43) feet above the respective building's finish floor height, and a minimum of six feet above the parapet.
3. Top of parapet heights shall be twenty-four (24) feet minimum above the respective building's finish floor height. The height shall be at least as tall as the tallest roof mounted equipment to screen from sidewalk and parking lot.
4. Refer to Figures 3-16 through 3-53 on the following pages for Major Anchor elevation design requirements and conceptual elevations. Designs based on tenant criteria will be allowed subject to conformance with the provisions of the Building Design Standards and approval by the Master Developer and the City of Sparks Administrator.
5. All elevations shall include enhanced architectural features: stone veneer pilasters full height of building; canopies and wall fins; wall variations (minimum of 2 feet); parapet variations (minimum of 4 feet); mix of building materials; mix of building colors; wainscot treatment.

Exception: Wall variations at the rear truck loading areas of the major anchor tenants may be 12 inches so as not to interfere with truck access.

6. Building height is as defined in Title 20.05 of the Sparks Municipal Code.

Mini Anchor Buildings:

1. Defined as any single tenant in a building space equal to or greater than 4,000 square feet and less than 12,000 square feet.
2. Entry Elements shall be provided such that they are proportionate with the tenant's frontage and are limited in height to thirty-two (32) feet above the respective building's finish floor height.
3. Top of parapet heights shall be a minimum of twenty-four (24) feet above the respective building's finish floor height. The height shall be at least as tall as the tallest roof mounted equipment to screen from sidewalk and parking lot.
4. Refer to Figures 3-16 through 3-53 on the following pages for Mini Anchor elevation design requirements and conceptual elevations. Designs based on tenant criteria will be allowed subject to conformance with the provisions of the Building Design Standards and approval by the Master Developer and the City of Sparks Administrator.
5. All elevations shall include enhanced architectural features such as: stone veneer pilasters full height of building; false windows; canopies and wall fins; wall variations (minimum of 2 feet); parapet variations (minimum of 4 feet); mix of building materials; mix of building colors; wainscot treatment.
6. Building height is as defined in Title 20.05 of the Sparks Municipal Code.

Shop Buildings:

1. Defined as a multi-tenant in-line building with tenant spaces 4,000 square feet or less.
2. Top of parapet heights shall be a minimum of twenty-two (22) feet above the respective building's finish floor height. The height shall be at least as tall as the tallest roof mounted equipment to screen from sidewalk and parking lot.
3. Parapet and arcade front roof lines shall not be continuous for more than forty (40) feet without some variation in height.
4. Refer to Figures 3-16 through 3-53 on the following pages for Shop Building elevation design requirements and conceptual elevations. Designs based on tenant criteria will be allowed subject to conformance with the provisions of the Building Design Standards and approval by the Master Developer and the City of Sparks Administrator.

5. All elevations shall include enhanced architectural features such as: stone veneer pilasters full height of building; false windows; canopies and wall fins; wall variations (minimum of 2 feet); parapet variations (minimum of 2 feet); mix of building materials; mix of building colors; stone veneer wainscot treatment.
6. Building height is as defined in Title 20.05 of the Sparks Municipal Code.
7. Sidewalk planter areas shall be provided at a rate of 32 square feet per each 20 linear feet of primary building frontage. The minimum width of the planter area shall be 4 feet. Planter area may be combined into larger areas to allow design flexibility; however, the entire required area may not be placed in only one location. The intention is that planter areas must be dispersed along the primary frontage of any given building to the extent possible, without conflict with required sidewalk width and/or wall plane variations.

Pad Buildings:

1. Defined as any single tenant to be located in a free-standing building, including in the area indicated as "Future Development" at the corner of Pyramid Highway and Los Altos Parkway.
2. The elevation design requirements for Pad Buildings shall be as required for Mini Anchor Buildings except as modified below.
3. Roof shapes, canopies, awnings and facade design shall be visually consistent around the entire perimeter of the building. Blank walls exceeding thirty (30) feet horizontally are not allowed.
4. Storefront systems shall be complementary to the approved color palette as shown in Appendix Exhibit K.
5. Top of typical parapet height shall be limited to twenty-two (22) feet above the respective building's finish floor height. The height shall be at least as tall as the tallest roof mounted equipment to screen from the sidewalk and parking lot. Where sloped roofs are used, roof shapes and articulation are required such that there shall be a mix of gable end and hipped roof configurations with no continuous straight runs exceeding forty (40) feet.
6. Buildings larger than 12,000 square feet will have higher parapets than smaller tenants; however, in no case shall parapet heights or canopy / entry design elements at the free-standing building be higher than those established for Major Anchor tenants, as measured from the finish floor of the freestanding building.
7. Canopy / Entry design elements may project above the typical parapet line by a maximum of twenty-five (25) percent of the parapet height and laterally the length of 25% of the building side. Such elements shall be 4-sided.

8. All service areas, coolers, and dumpsters shall be fully screened from public view or otherwise incorporated into the building architecture as described in "Service Areas" above.
9. Provide minimum 4 foot high stone veneer wainscot at all sides of Pad Buildings to be constructed at Pad A, Pad B, and at the area indicated as "Future Development". Refer to Chapter 1, Figure 1-3.
10. Building height is as defined in Title 20.05 of the Sparks Municipal Code.
11. Sidewalk planter areas shall be provided at a rate of 32 square feet per each 20 linear feet of primary building frontage. The minimum width of the planter area shall be 4 feet. Planter area may be combined into larger areas to allow design flexibility; however, the entire required area may not be placed in only one location. The intention is that planter areas must be dispersed along the primary frontage of any given building to the extent possible.

Construction, Operation, and Maintenance

General:

1. All construction job-sites within Sparks Crossing shall be maintained in a clean and orderly fashion.
2. The Master Developer reserves the right to provide appropriate and reasonable clean-up activities and assess the cost of this clean-up against the offending entity.
3. All outdoor construction activities shall be limited as follows:
Monday through Friday: 7:00 A.M. to 7:00 P.M.
Saturday: 9:00 A.M. to 5:00 P.M.
Sunday: No construction activities are allowed.

The Master Developer shall provide and install signs at project entries stating these limits for the duration of the construction. The Master Developer shall cause these signs to be removed at the appropriate time.

4. The contractor shall be required to provide curb cuts for all ingress/egress areas onto a paved street. Ramping over curbs is prohibited.
5. In order to prevent construction debris, dirt and mud from transferring from trucks, vehicles or equipment onto the paved street, the contractor shall install pavement, gravel or similar surface treatment at all ingress/egress points a minimum of fifty (50) feet from said access point.
6. Any entity constructing any real property at the project shall provide the name and telephone number of a contact person to the City of Sparks, to be available on call on a 24-hours-a-day, 7-days-a-week basis.

7. Deliveries to the site and noise-generating maintenance such as parking lot sweeping, snow removal and trash service shall be limited to 7 am to 10 pm. No truck idling shall be permitted outside these hours.

Temporary Uses and Structures:

1. All temporary uses and or structures shall comply with Sparks Municipal Code, Section 20.41.

Construction Yards:

1. Construction yards are defined as temporary areas for the storage of materials, supplies, equipment, tools, stockpiles of useable construction materials and other items typically associated with construction projects of this size such as storage containers, construction trailers and temporary office trailers. Mobile set-up permits are required by the State of Nevada and The City of Sparks prior to delivery and set up.
2. Construction Yards shall:
 - a. be approved by the City of Sparks and in compliance with Sparks Municipal Code, Section 20.41;
 - b. be associated with a specific project with an approved permit;
 - c. be fenced and located on private property out of public view to the extent possible. Yards shall not be placed in required parking spaces or block pedestrian/vehicular access;
 - d. be removed prior to final inspection of the last building or final approval for the project.

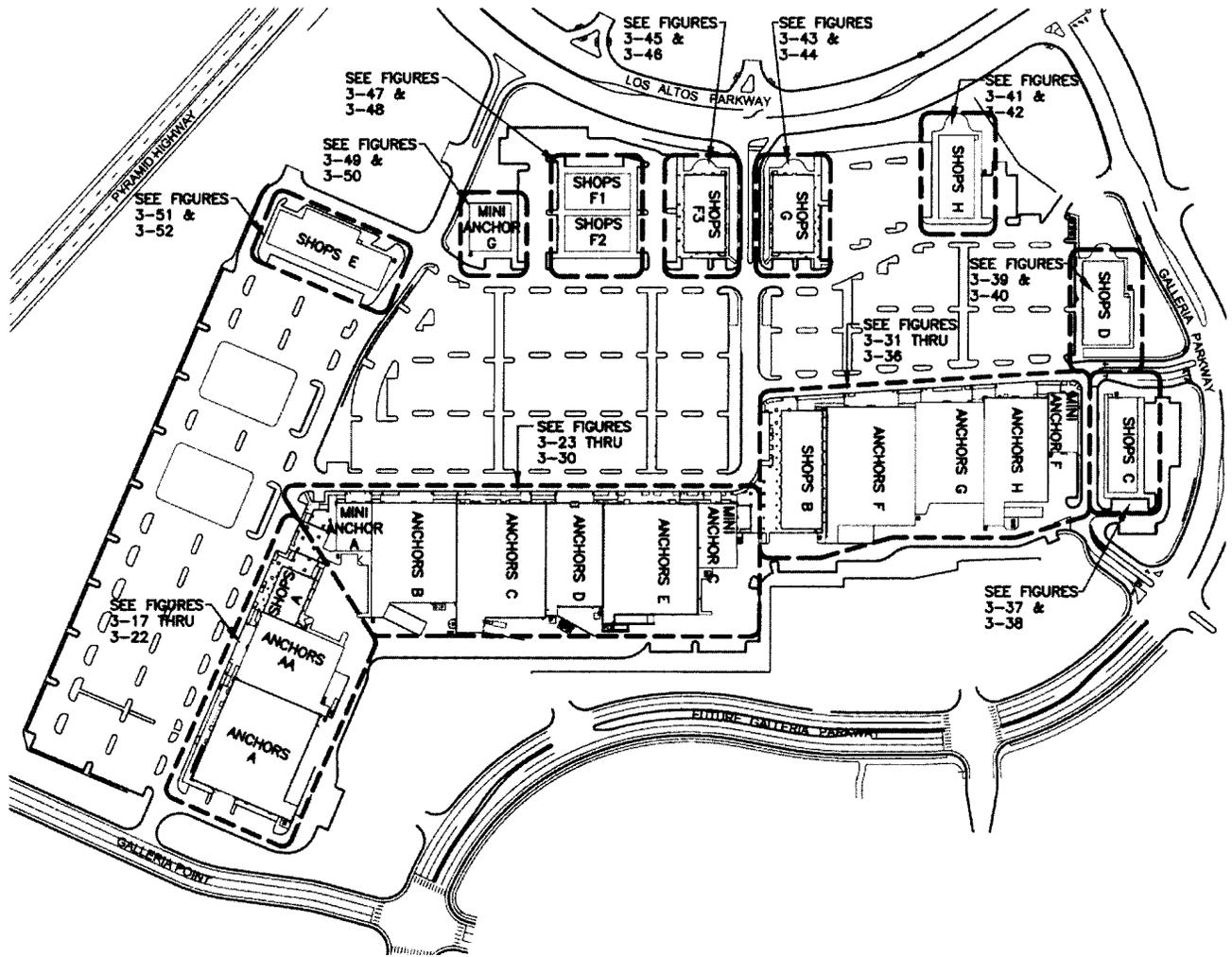
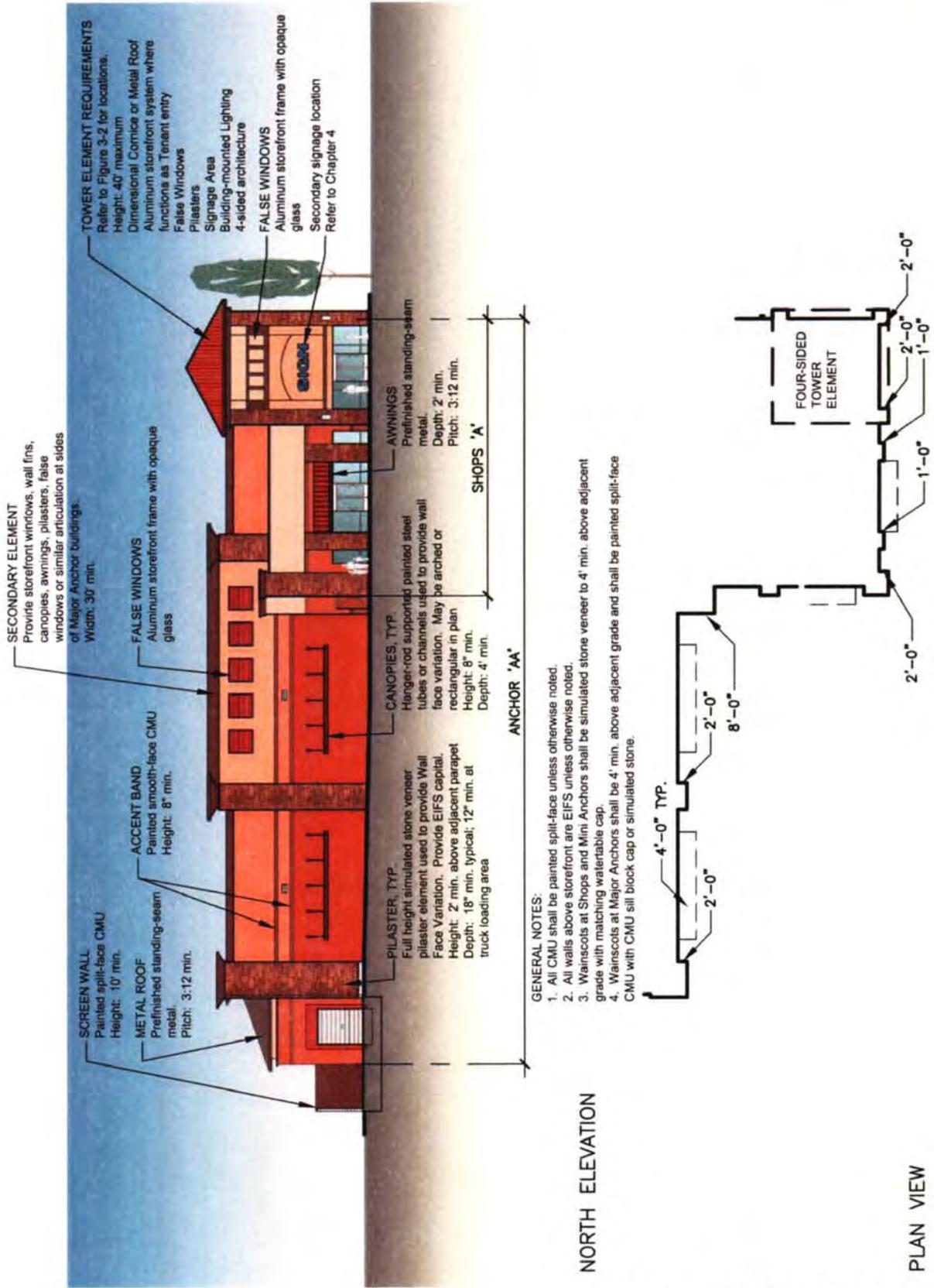


Figure 3-16 – Colored Elevation Key Map



NORTH ELEVATION

PLAN VIEW

Figure 3-17 – Colored Elevations

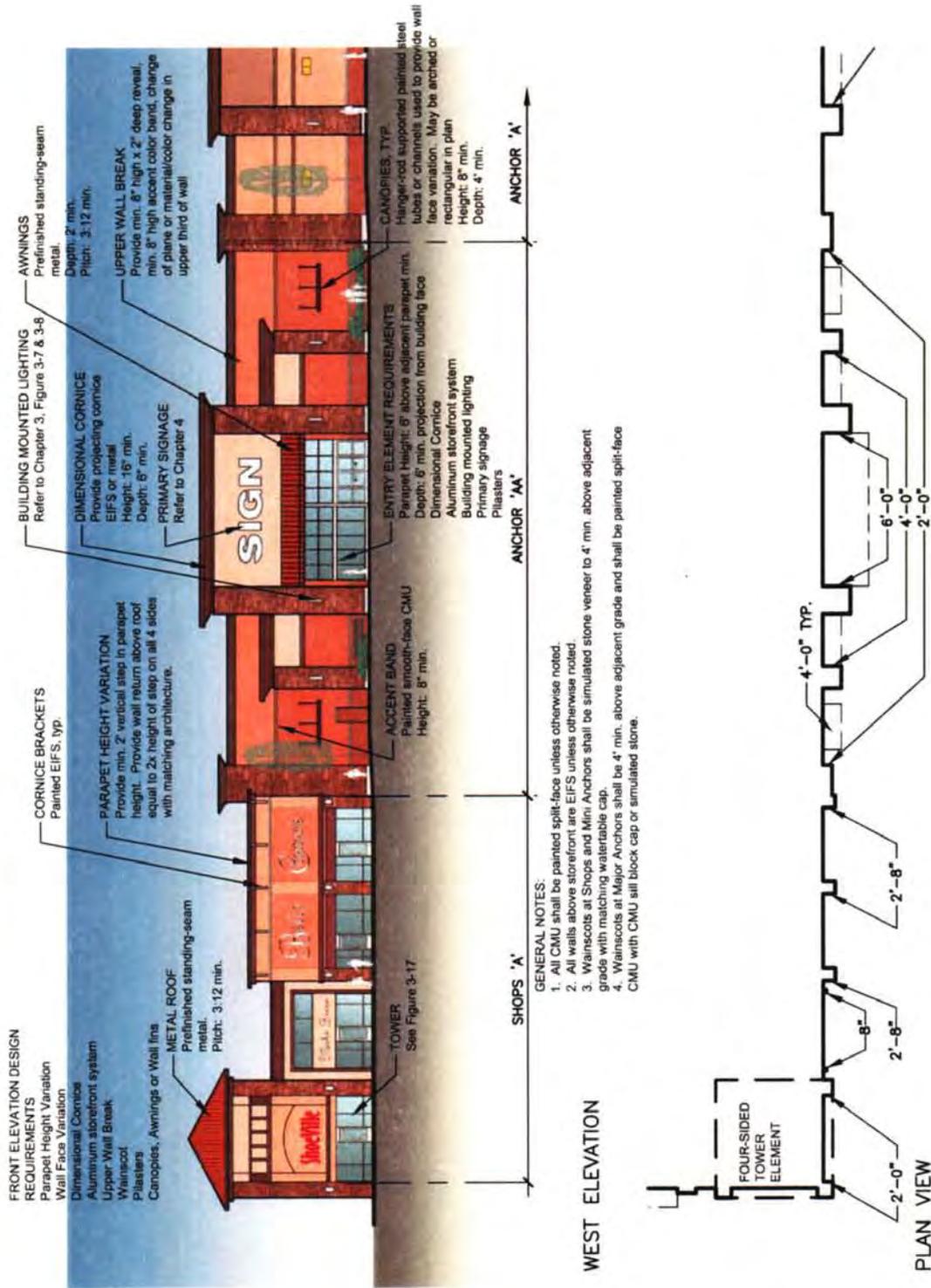
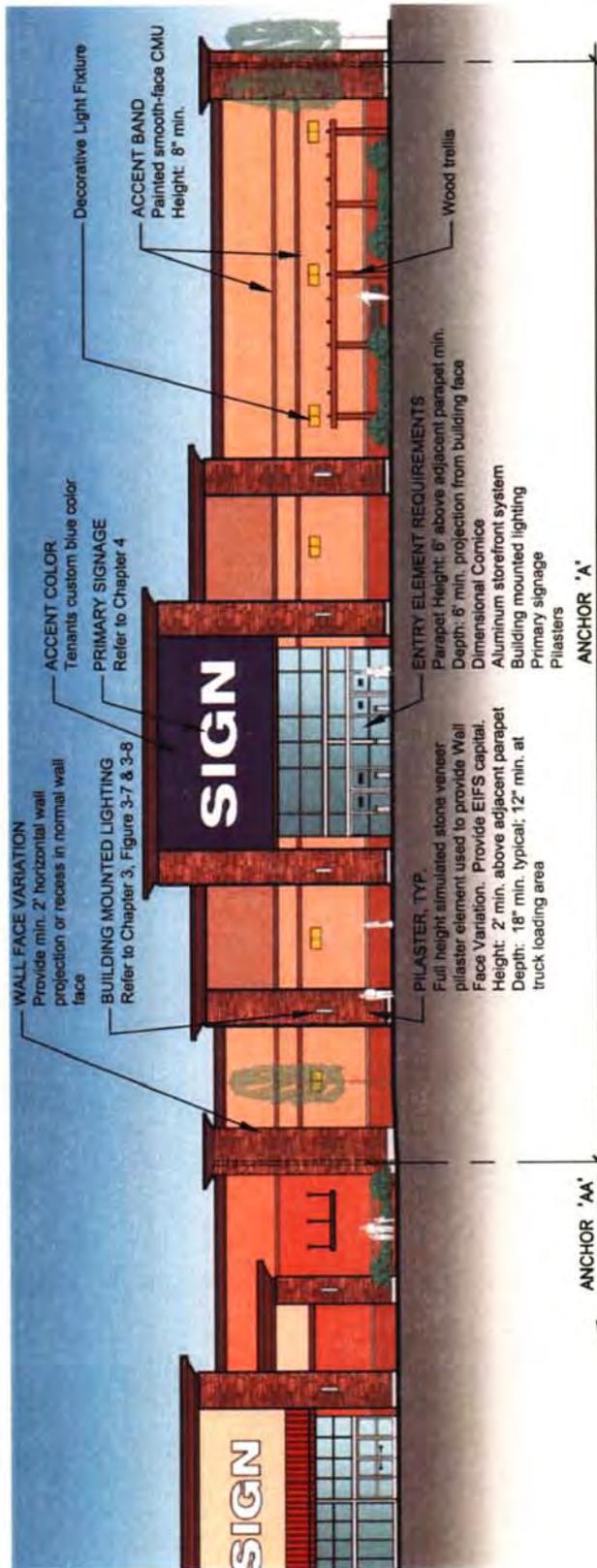


Figure 3-18 – Colored Elevations



GENERAL NOTES:

1. All CMU shall be painted split-face unless otherwise noted.
2. All walls above storefront are EIFS unless otherwise noted.
3. Wainscots at Shops and Mini Anchors shall be simulated stone veneer to 4' min. above adjacent grade with matching waterable cap.
4. Wainscots at Major Anchors shall be 4' min. above adjacent grade and shall be painted split-face CMU with CMU sill block cap or simulated stone.

WEST ELEVATION (CONT.)

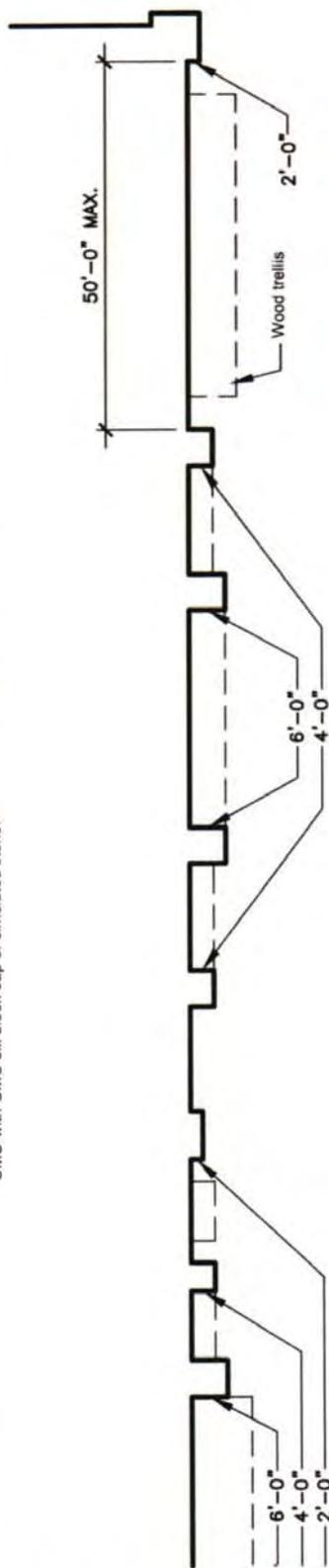


Figure 3-19 – Colored Elevations

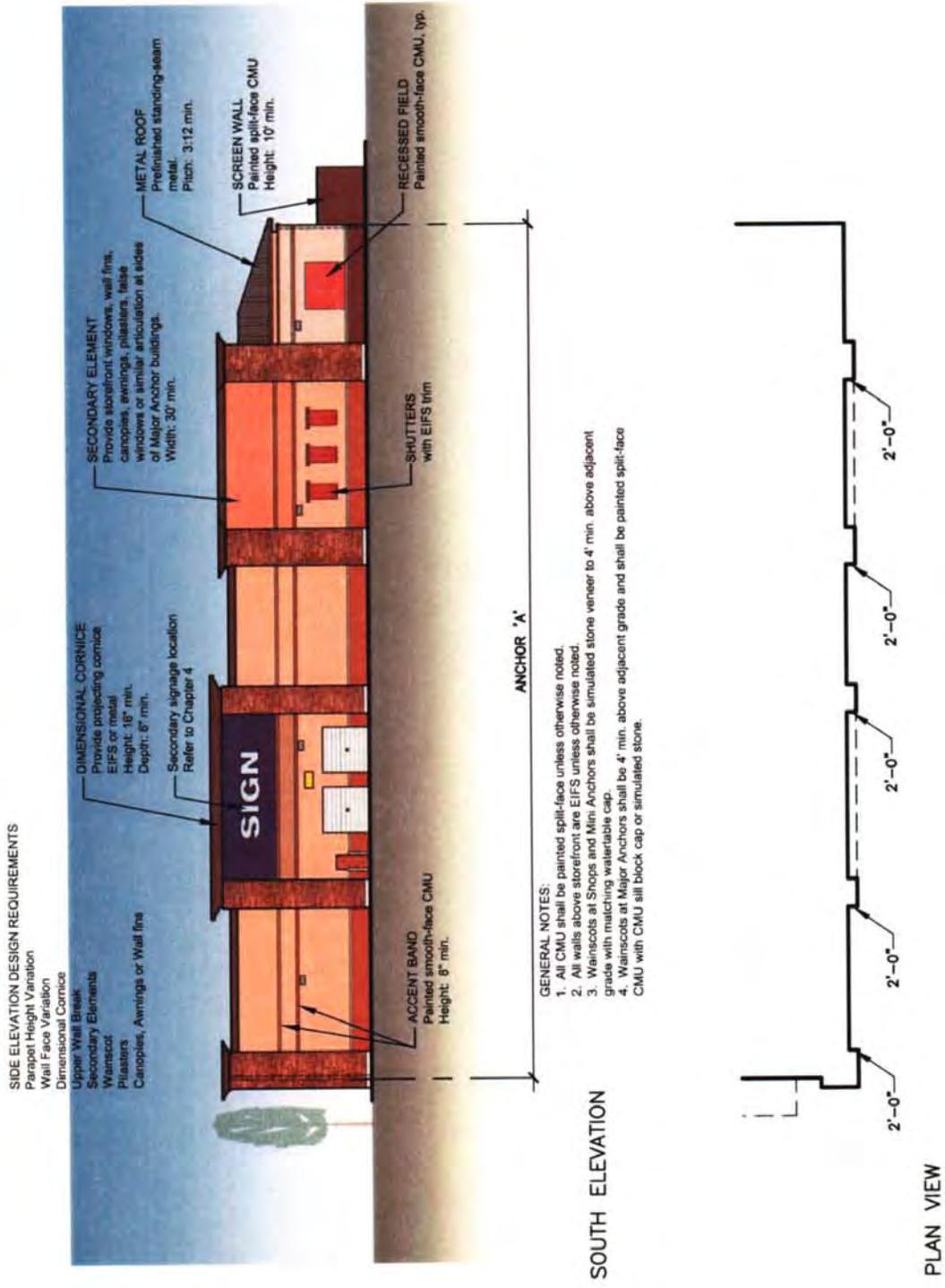


Figure 3-20 – Colored Elevations

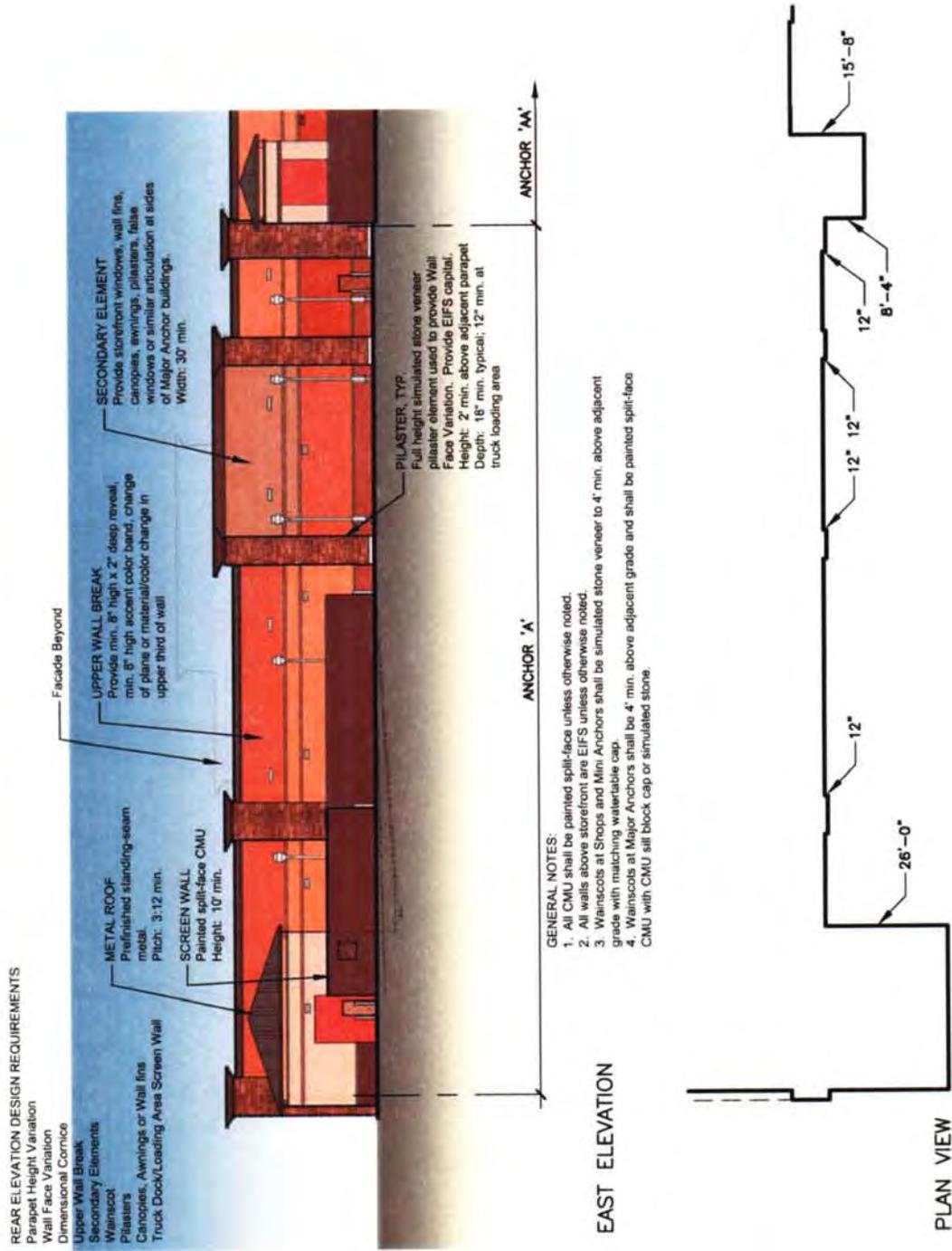
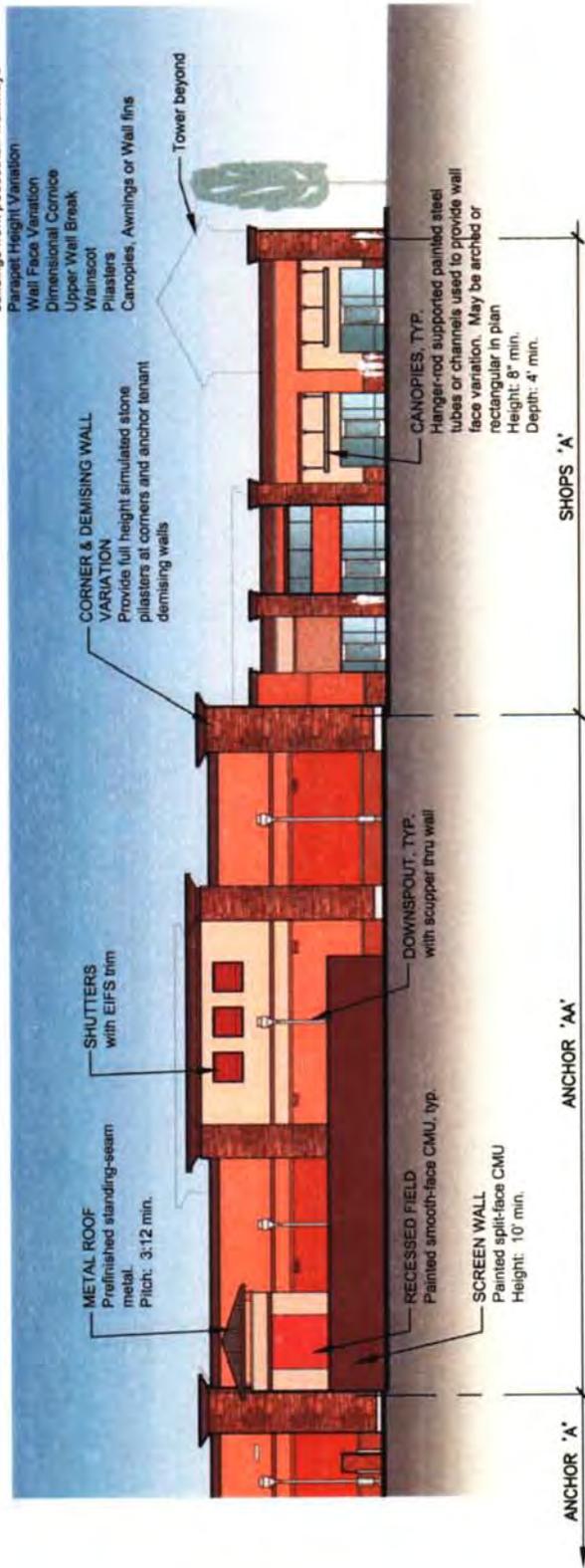


Figure 3-21 – Colored Elevations

SIDE & REAR ELEVATION DESIGN REQUIREMENTS
 Provide storefront windows where Shop buildings front pedestrian walkways



GENERAL NOTES:

1. All CMU shall be painted split-face unless otherwise noted.
2. All walls above storefront are EIFS unless otherwise noted.
3. Wainscots at Shops and Mini Anchors shall be simulated stone veneer to 4' min. above adjacent grade with matching waterable cap.
4. Wainscots at Major Anchors shall be 4' min. above adjacent grade and shall be painted split-face CMU with CMU sill block cap or simulated stone.

EAST ELEVATION (CONT.)

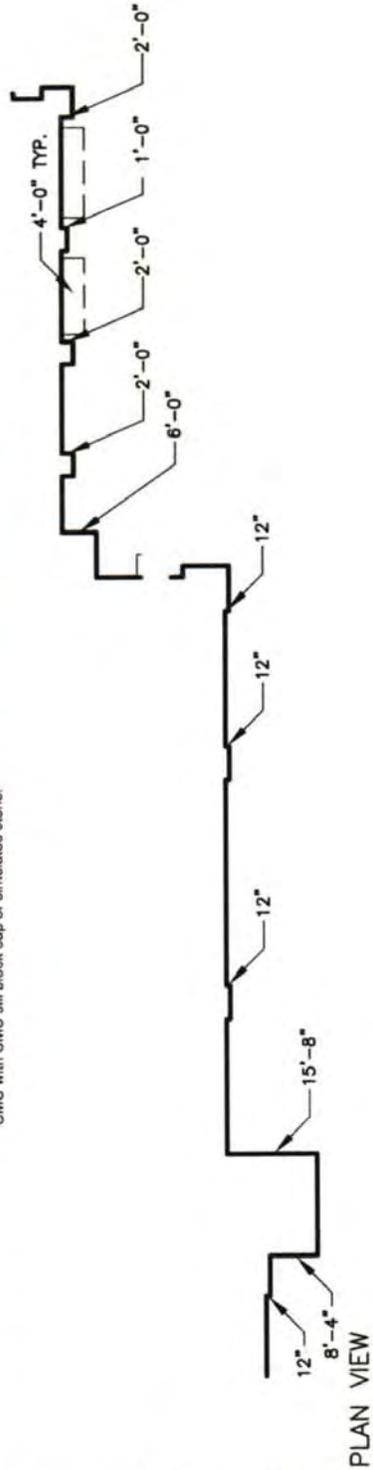
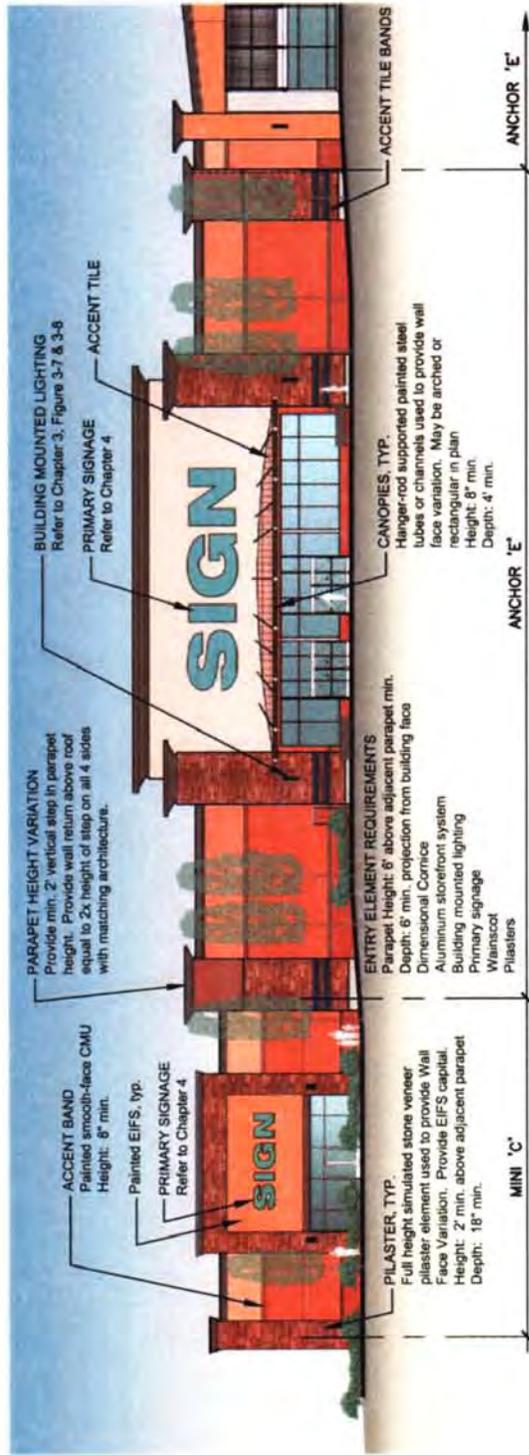


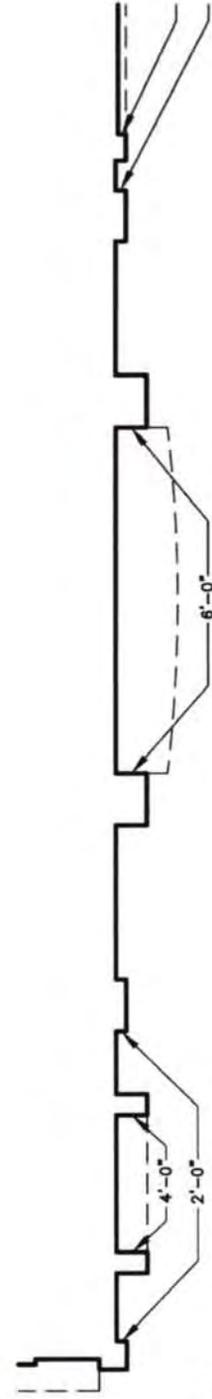
Figure 3-22 – Colored Elevations



GENERAL NOTES:

1. All CMU shall be painted split-face unless otherwise noted.
2. All walls above storefront are EIFS unless otherwise noted.
3. Wainscots at Shops and Mini Anchors shall be simulated stone veneer to 4' min. above adjacent grade with matching waterable cap.
4. Wainscots at Major Anchors shall be 4' min. above adjacent grade and shall be painted split-face CMU with CMU sill block cap or simulated stone.

NORTH ELEVATION



PLAN VIEW

Figure 3-23 – Colored Elevations

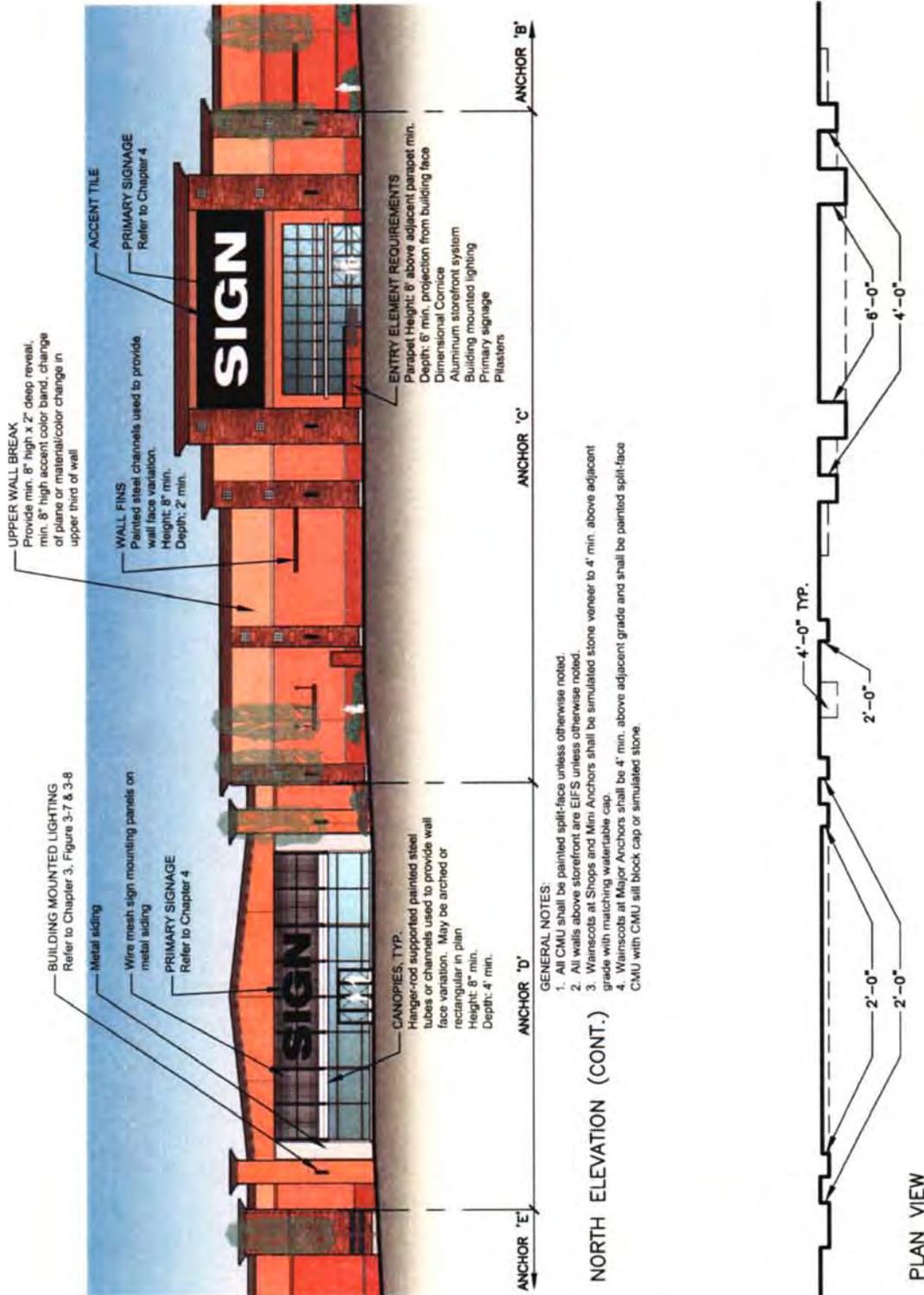
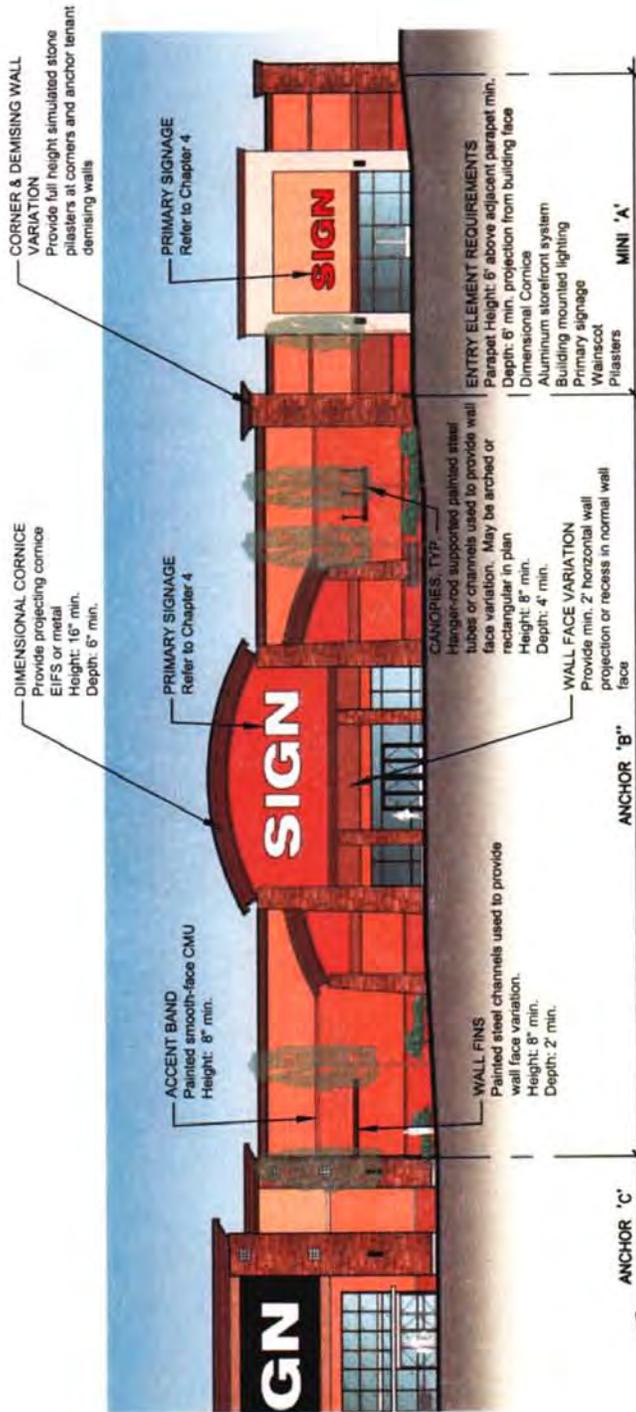


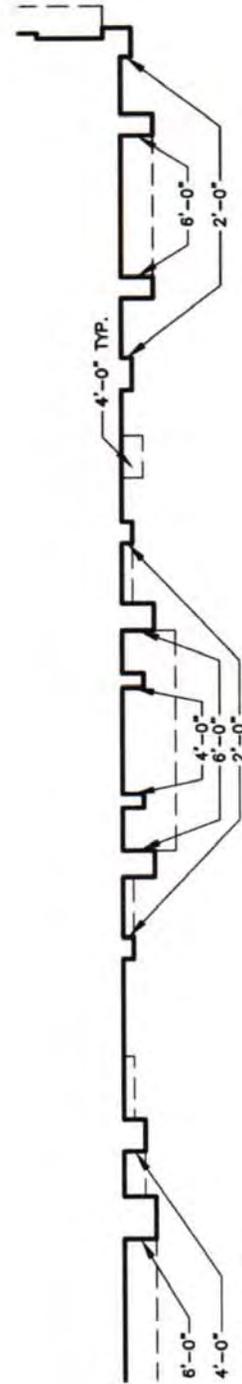
Figure 3-24 – Colored Elevations



GENERAL NOTES:

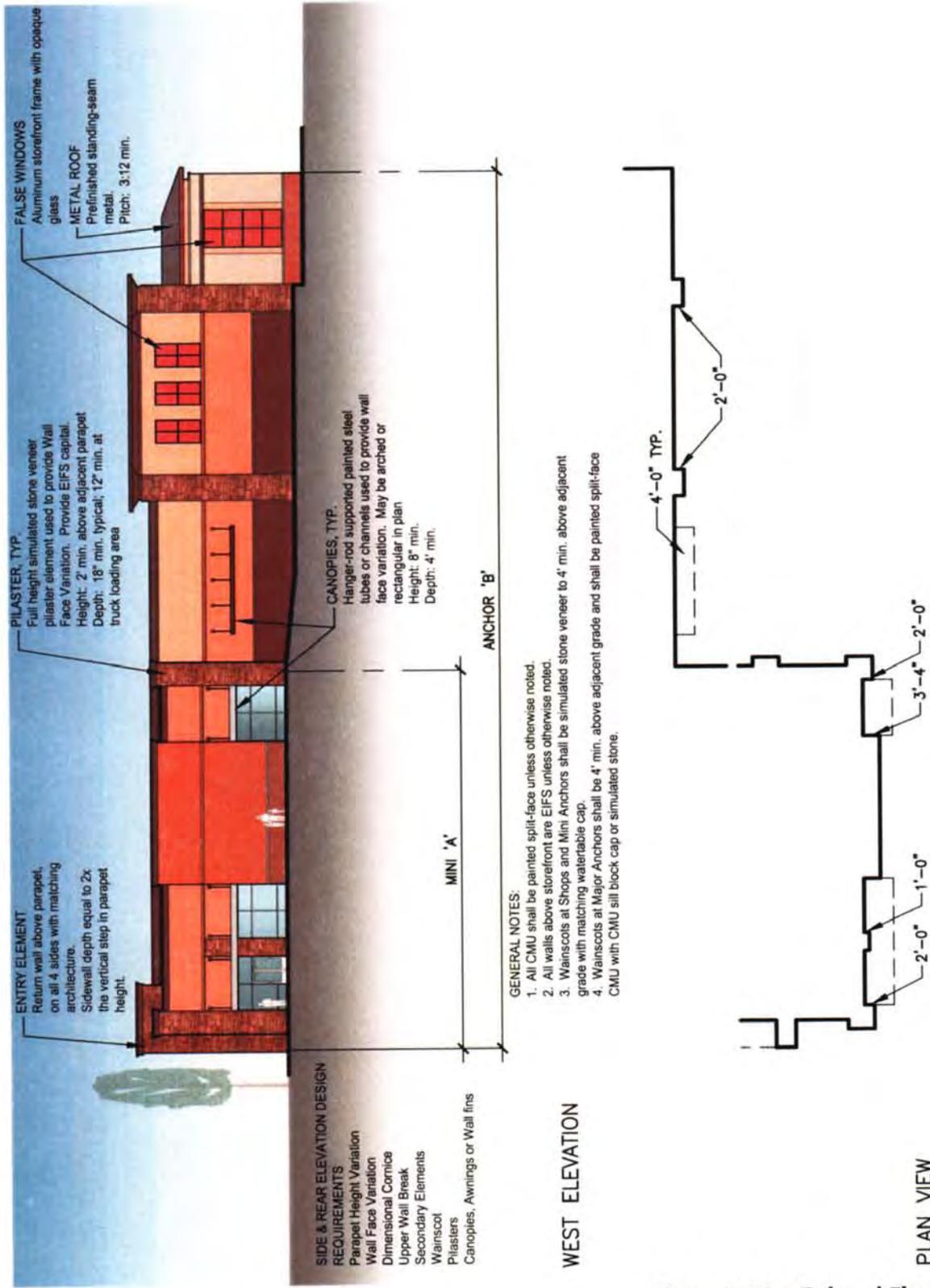
1. All CMU shall be painted split-face unless otherwise noted.
2. All walls above storefront are EIFS unless otherwise noted.
3. Wainscots at Shops and Mini Anchors shall be simulated stone veneer to 4' min. above adjacent grade with matching waterable cap.
4. Wainscots at Major Anchors shall be 4' min. above adjacent grade and shall be painted split-face CMU with CMU sill block cap or simulated stone.

NORTH ELEVATION (CONT.)



PLAN VIEW

Figure 3-25 – Colored Elevations



SIDE & REAR ELEVATION DESIGN REQUIREMENTS
 Parapet Height Variation
 Wall Face Variation
 Dimensional Cornice
 Upper Wall Break
 Secondary Elements
 Wainscot
 Pilasters
 Canopies, Awnings or Wall fins

WEST ELEVATION

PLAN VIEW

Figure 3-26 – Colored Elevations

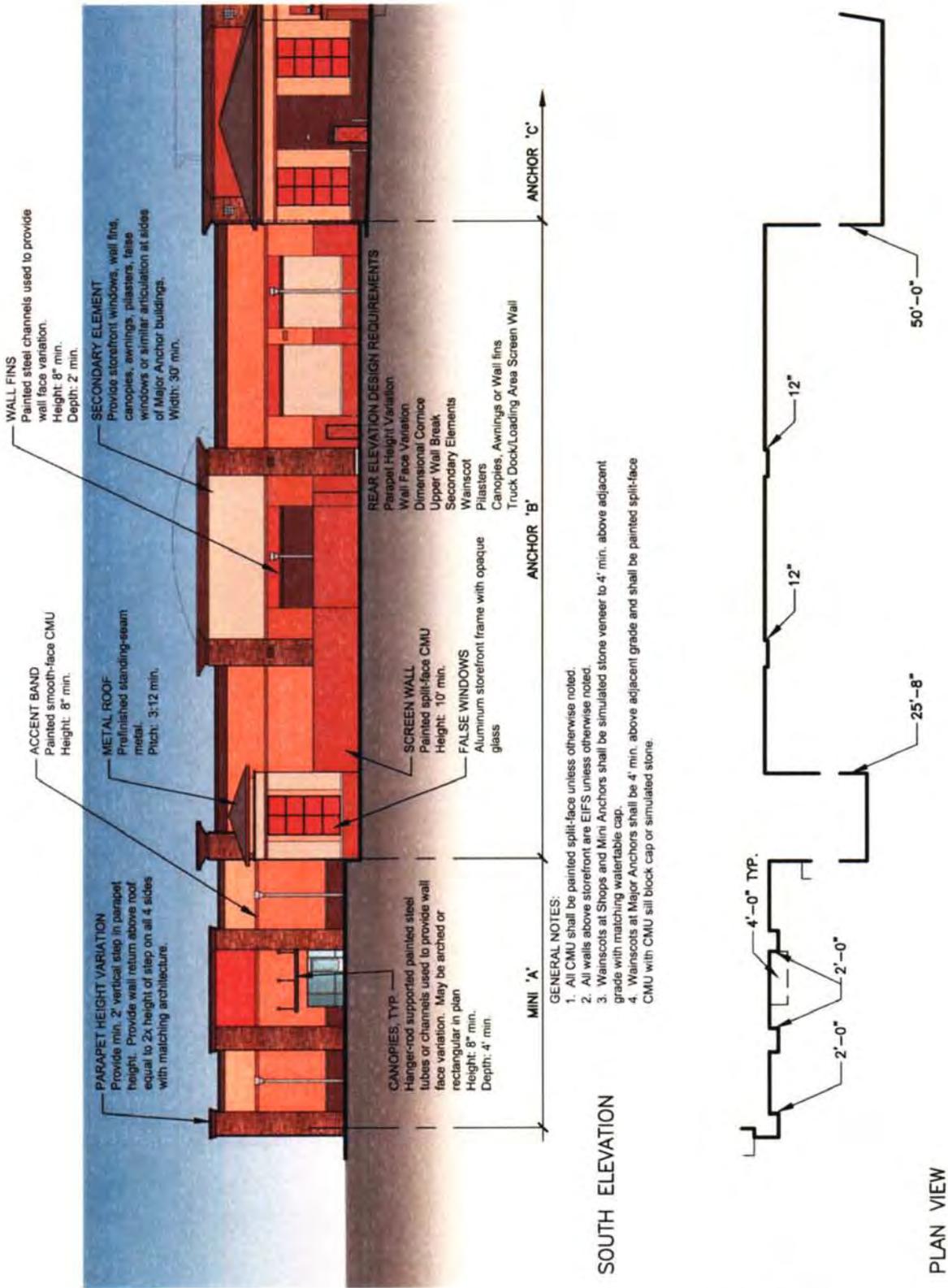


Figure 3-27 – Colored Elevations

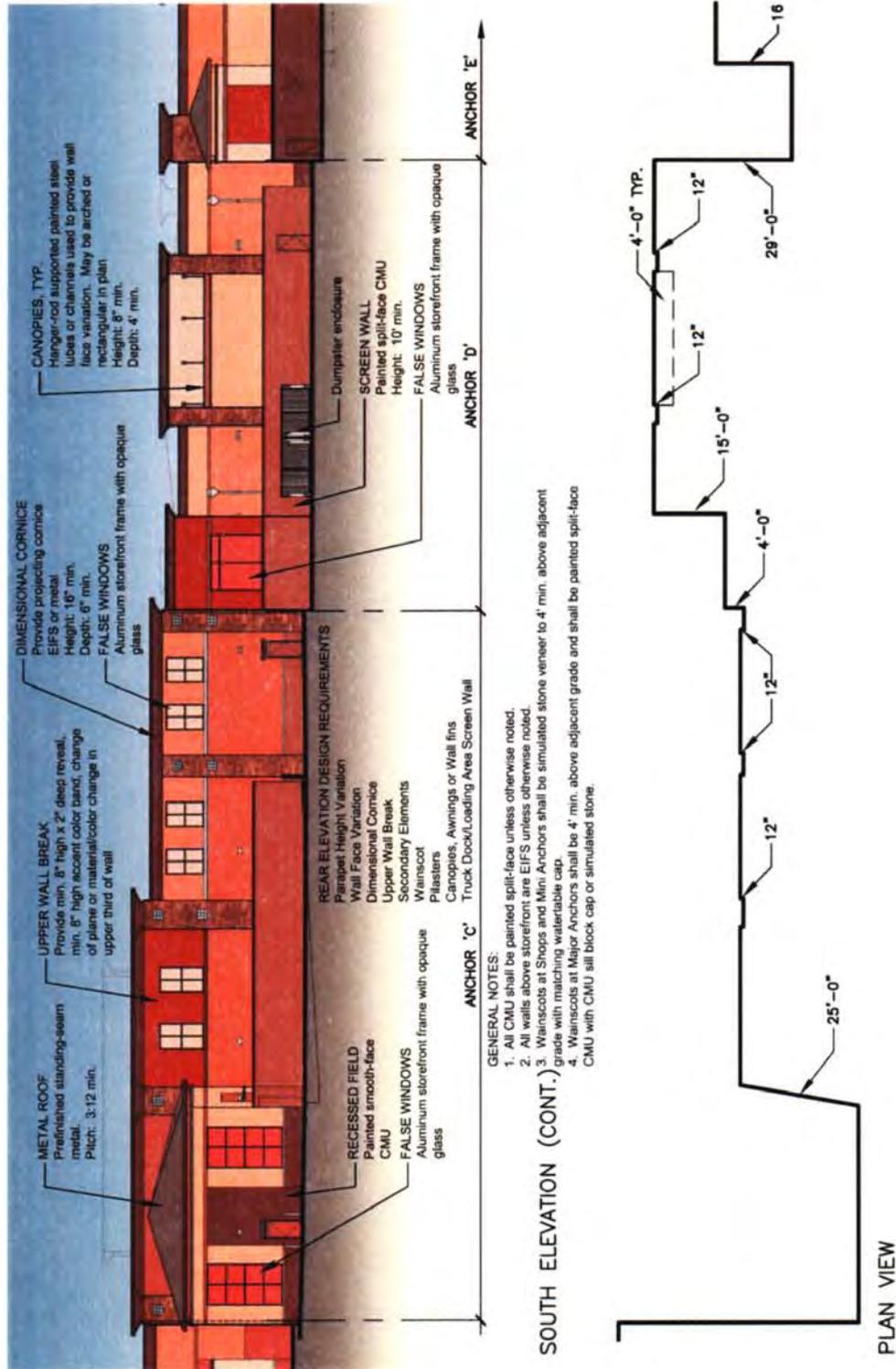


Figure 3-28 – Colored Elevations

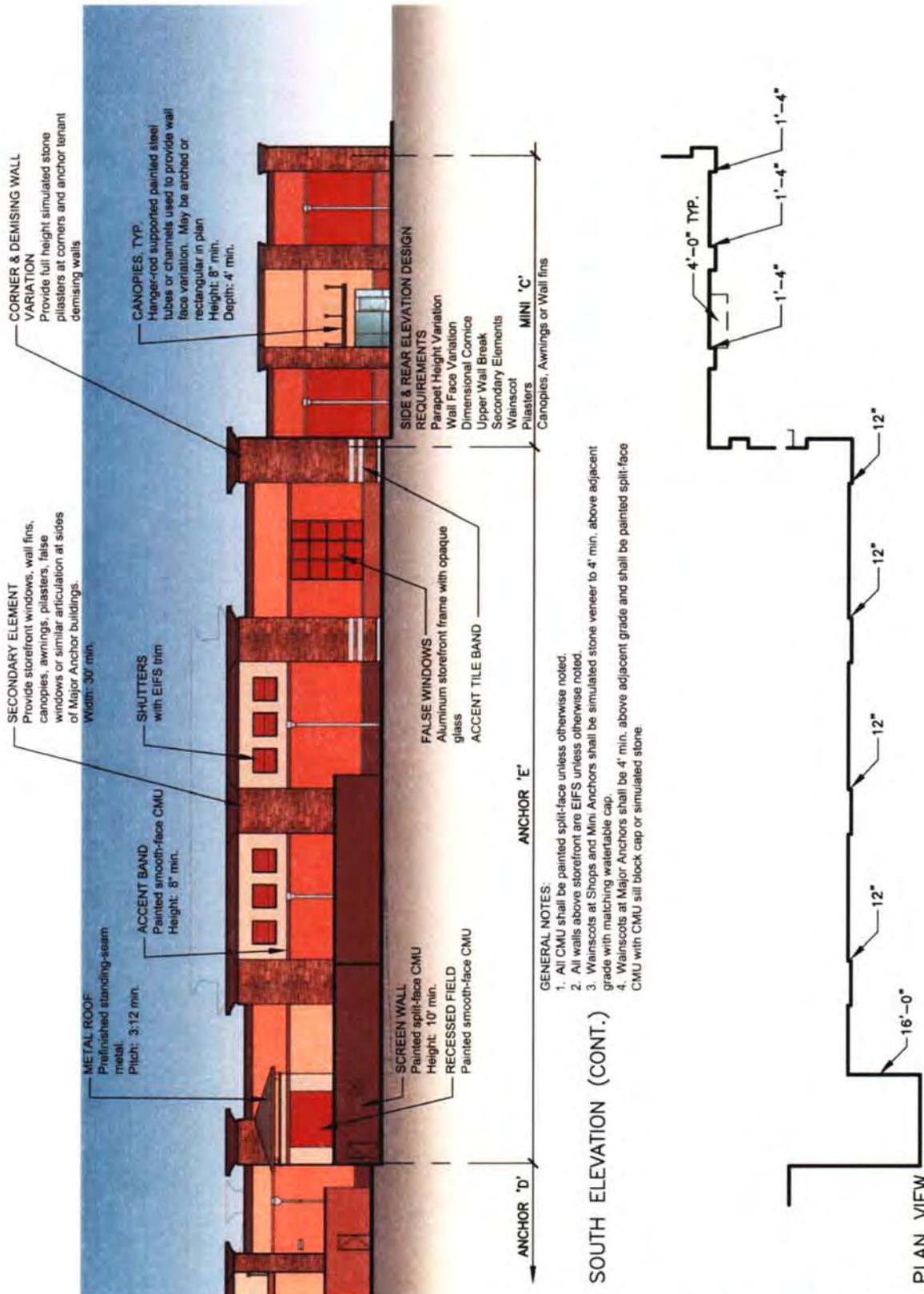


Figure 3-29 – Colored Elevations

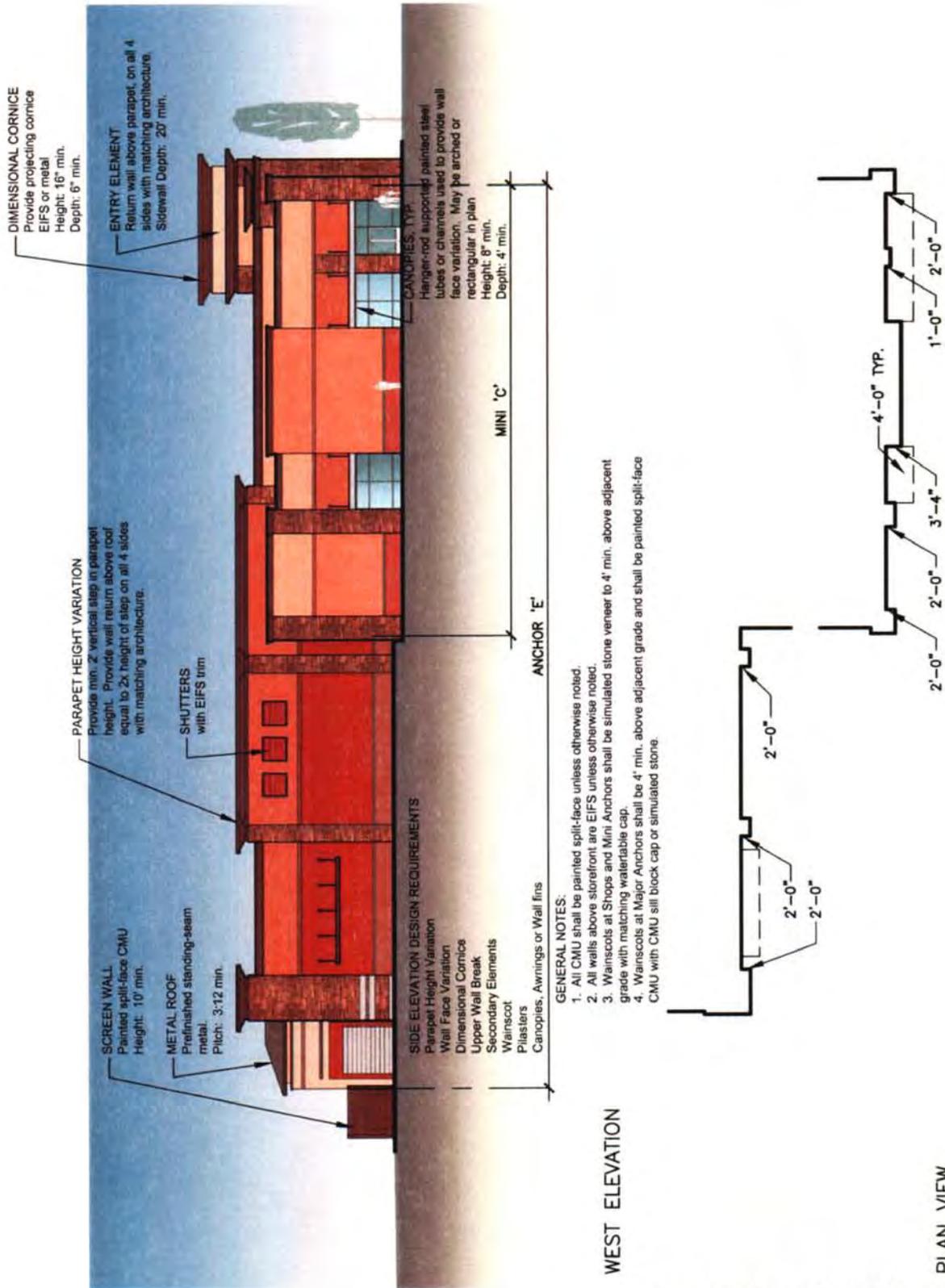


Figure 3-30 – Colored Elevations

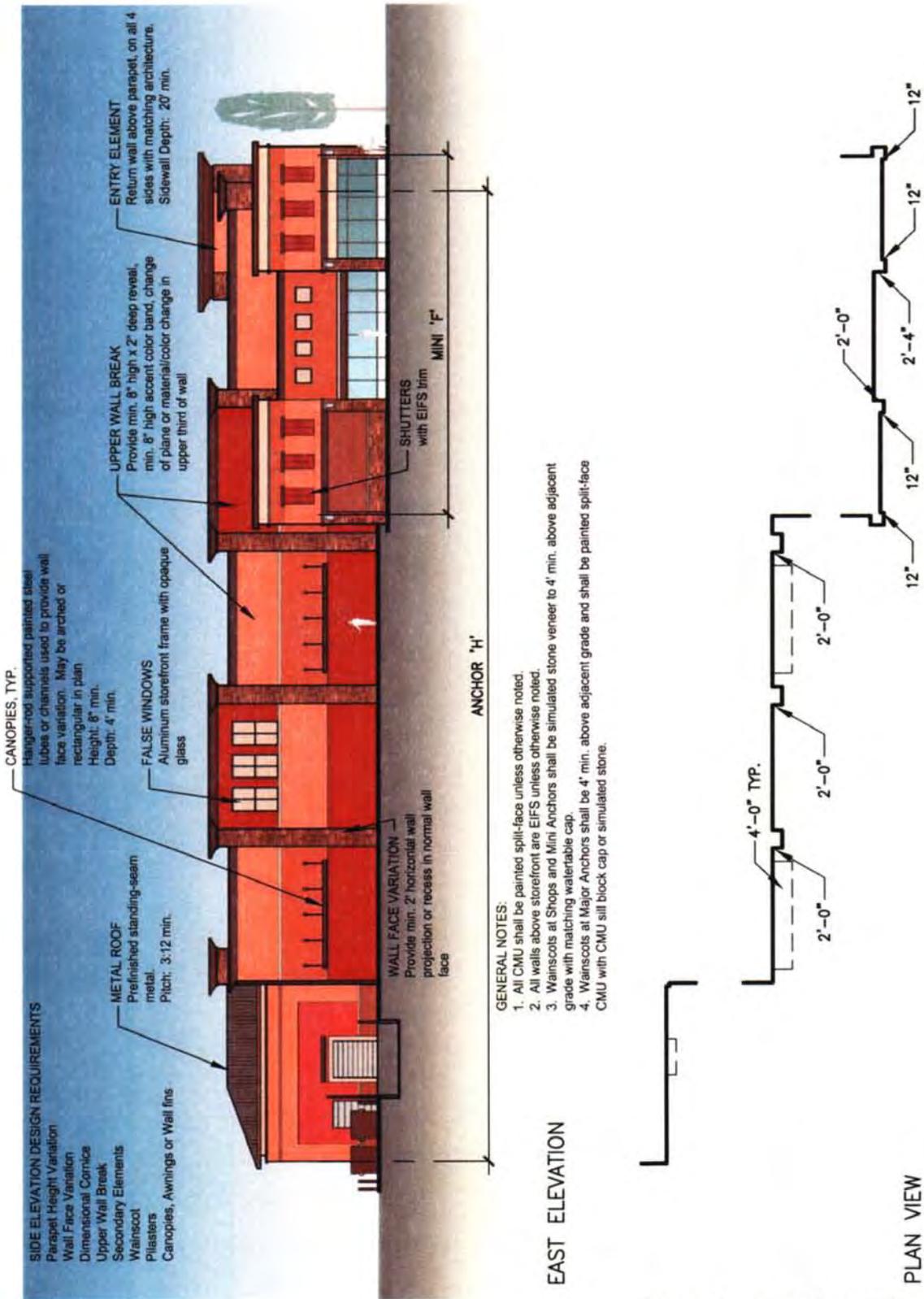


Figure 3-31 – Colored Elevations

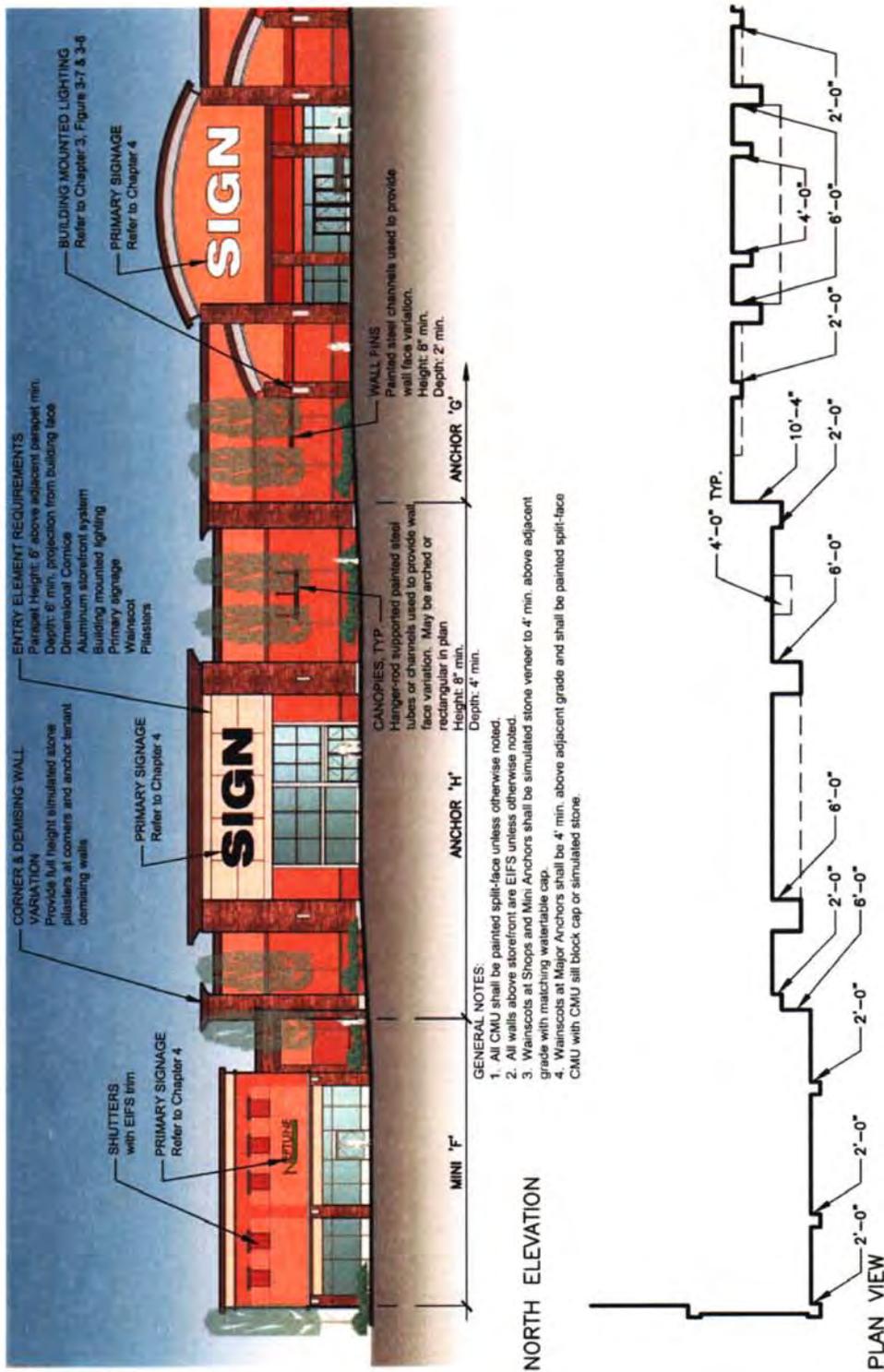


Figure 3-32 – Colored Elevations

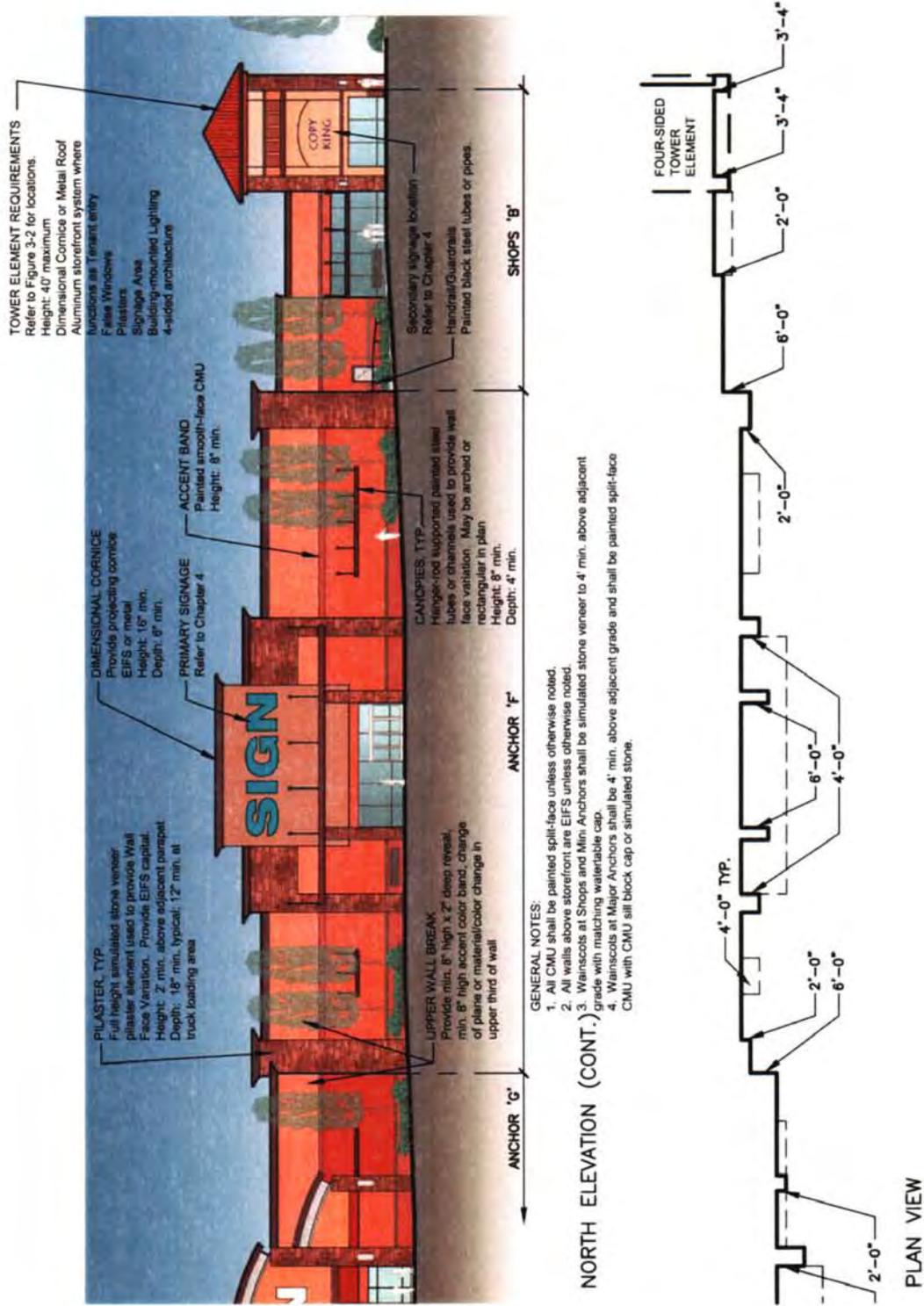


Figure 3-33 – Colored Elevations

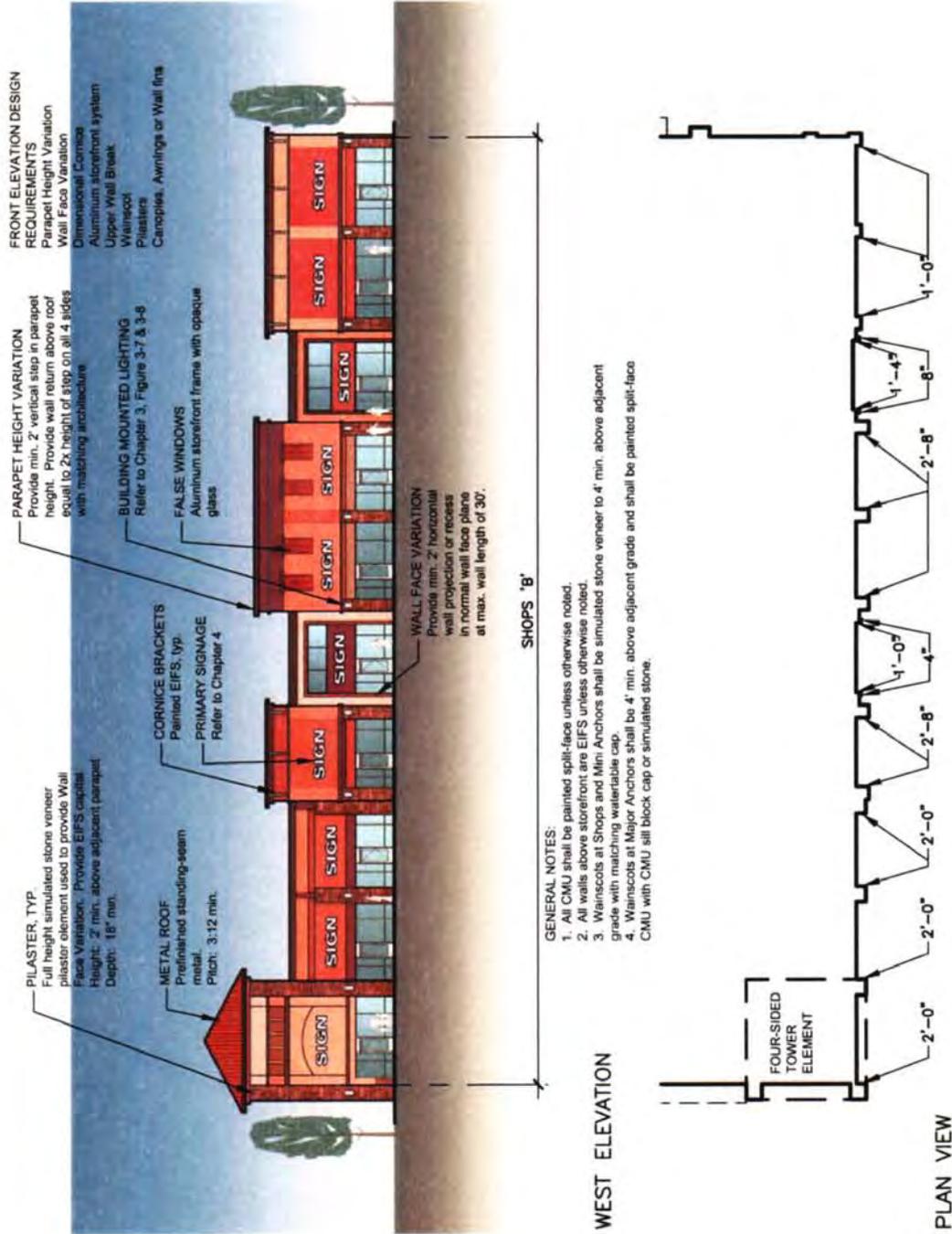


Figure 3-34 – Colored Elevations

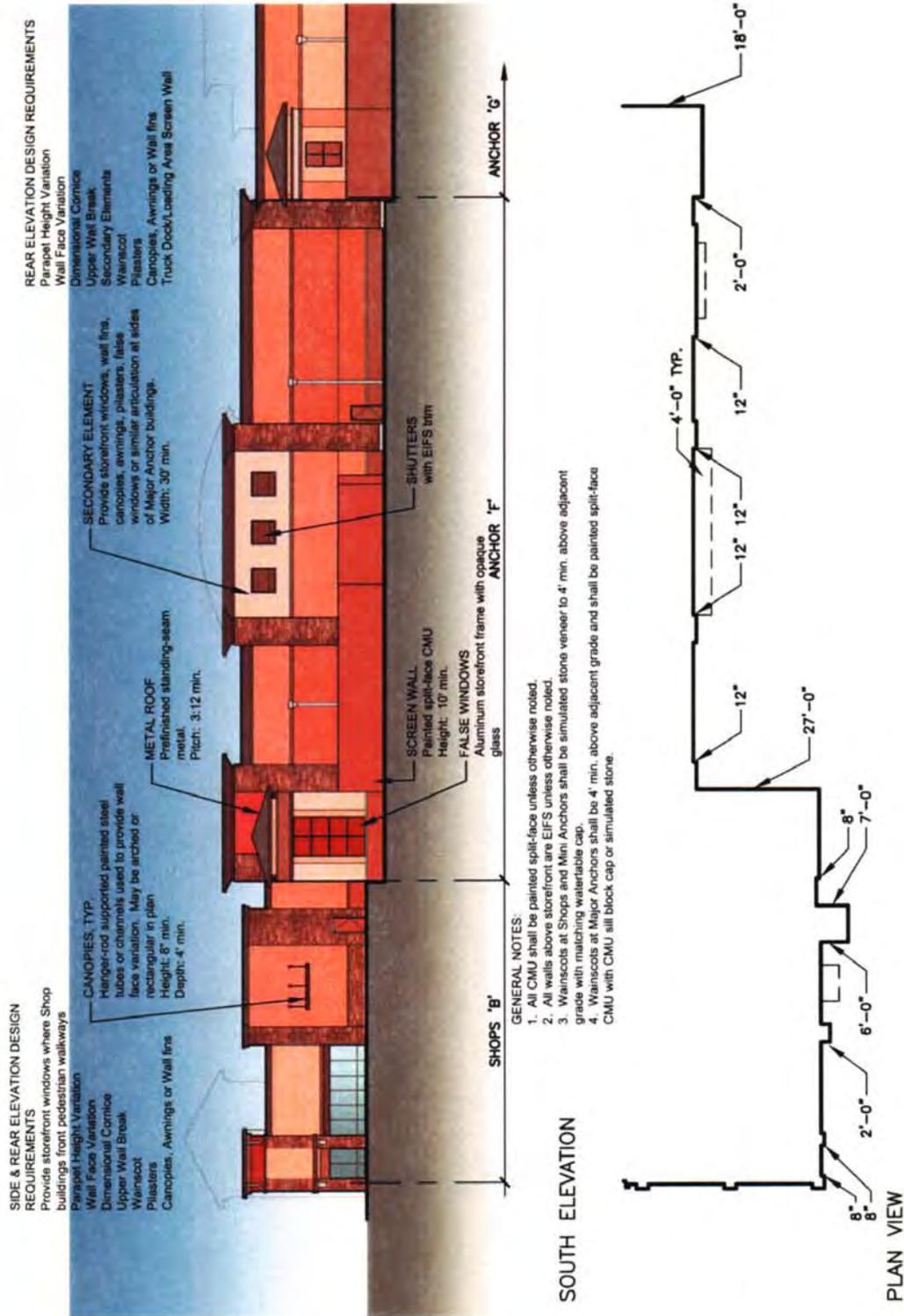


Figure 3-35 – Colored Elevations

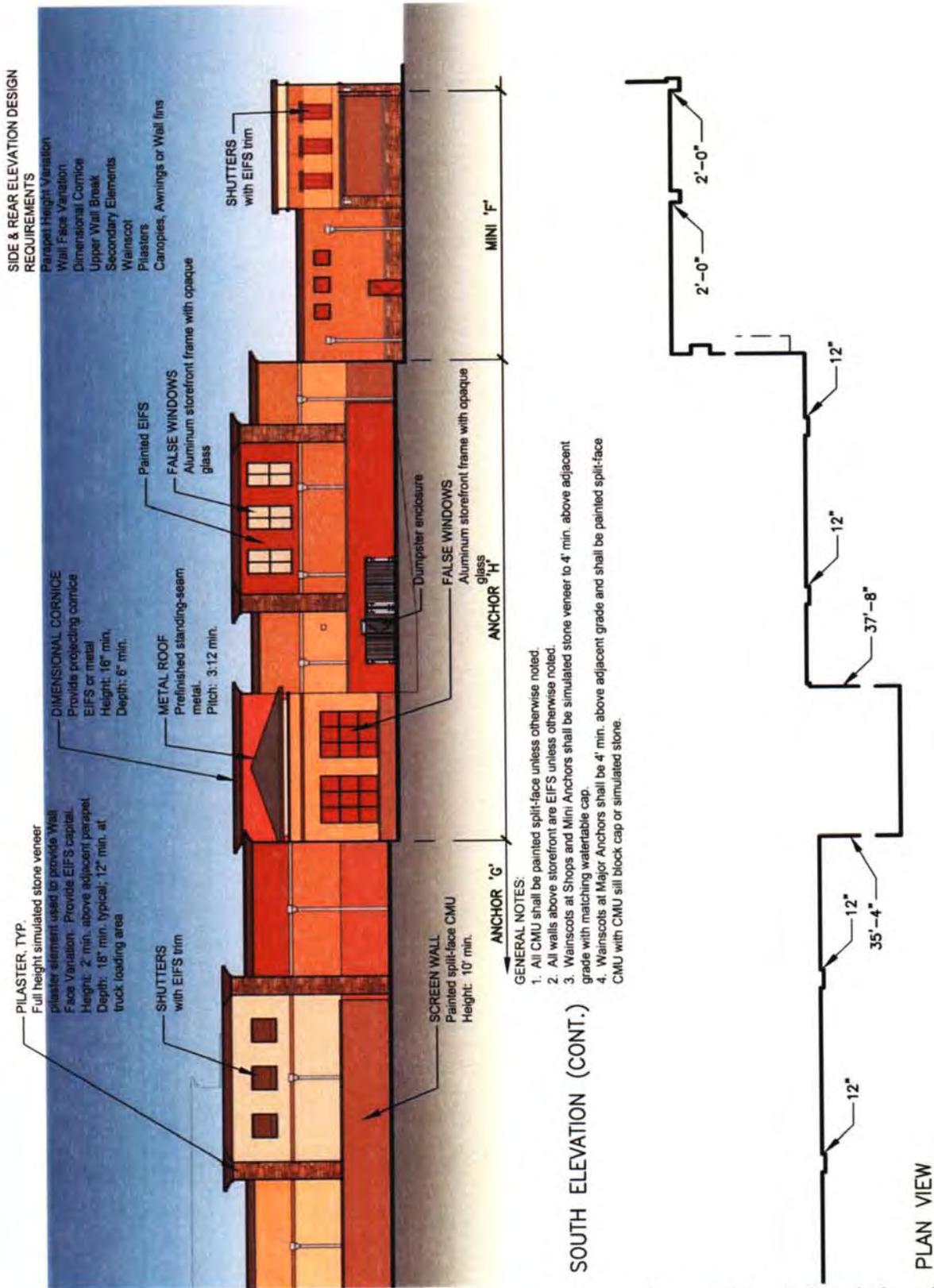


Figure 3-36 – Colored Elevations

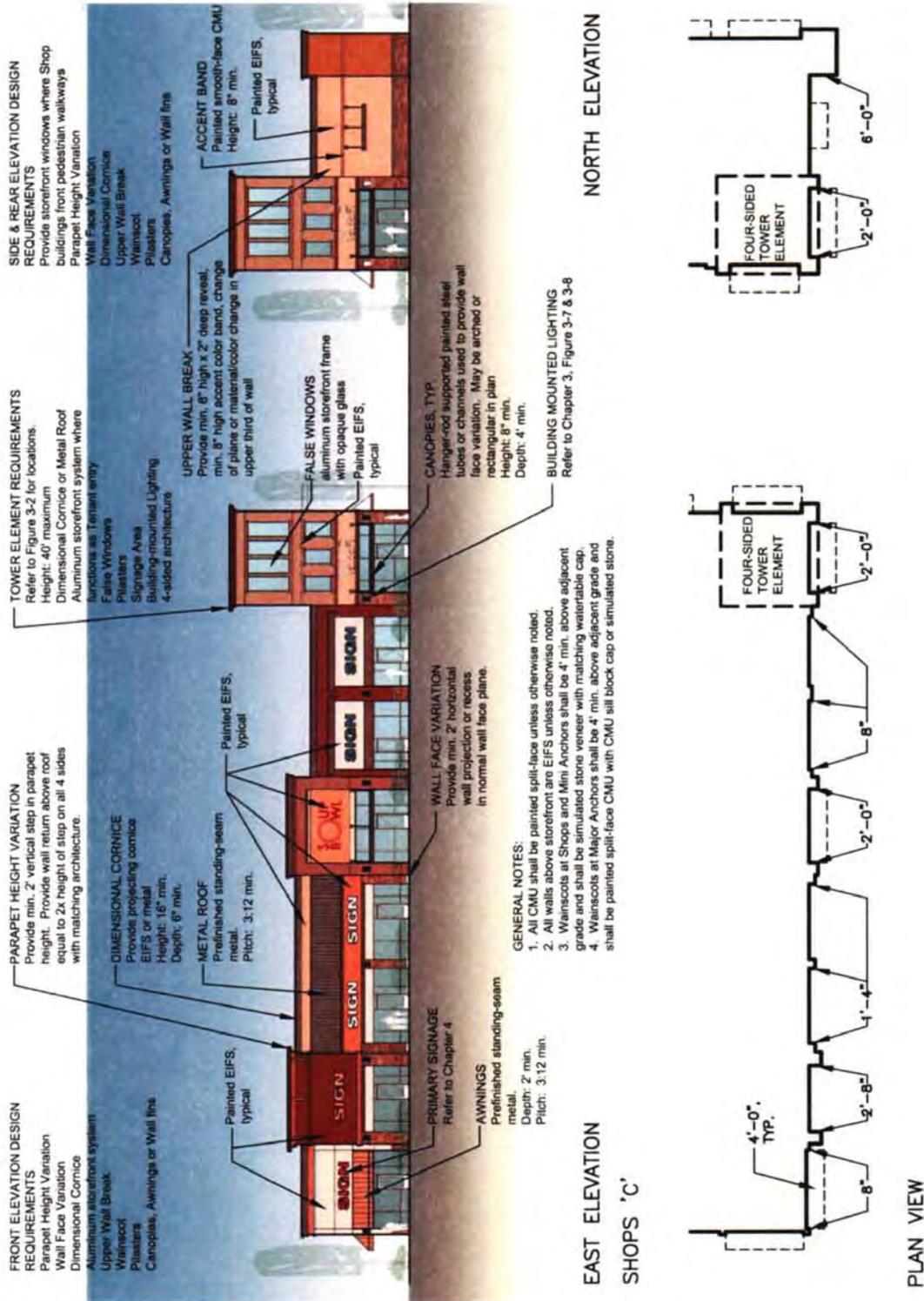


Figure 3-37 – Colored Elevations

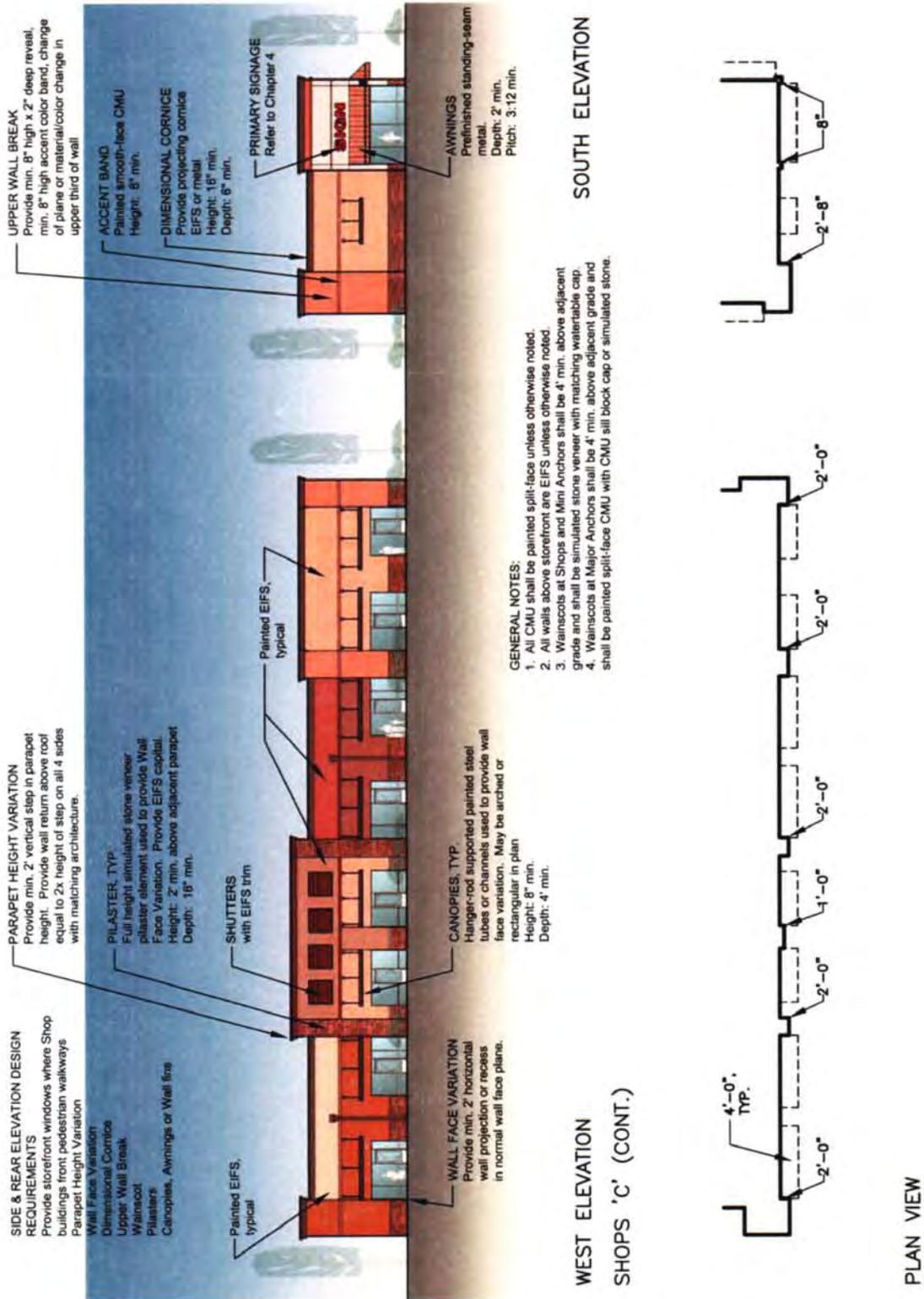


Figure 3-38 – Colored Elevations

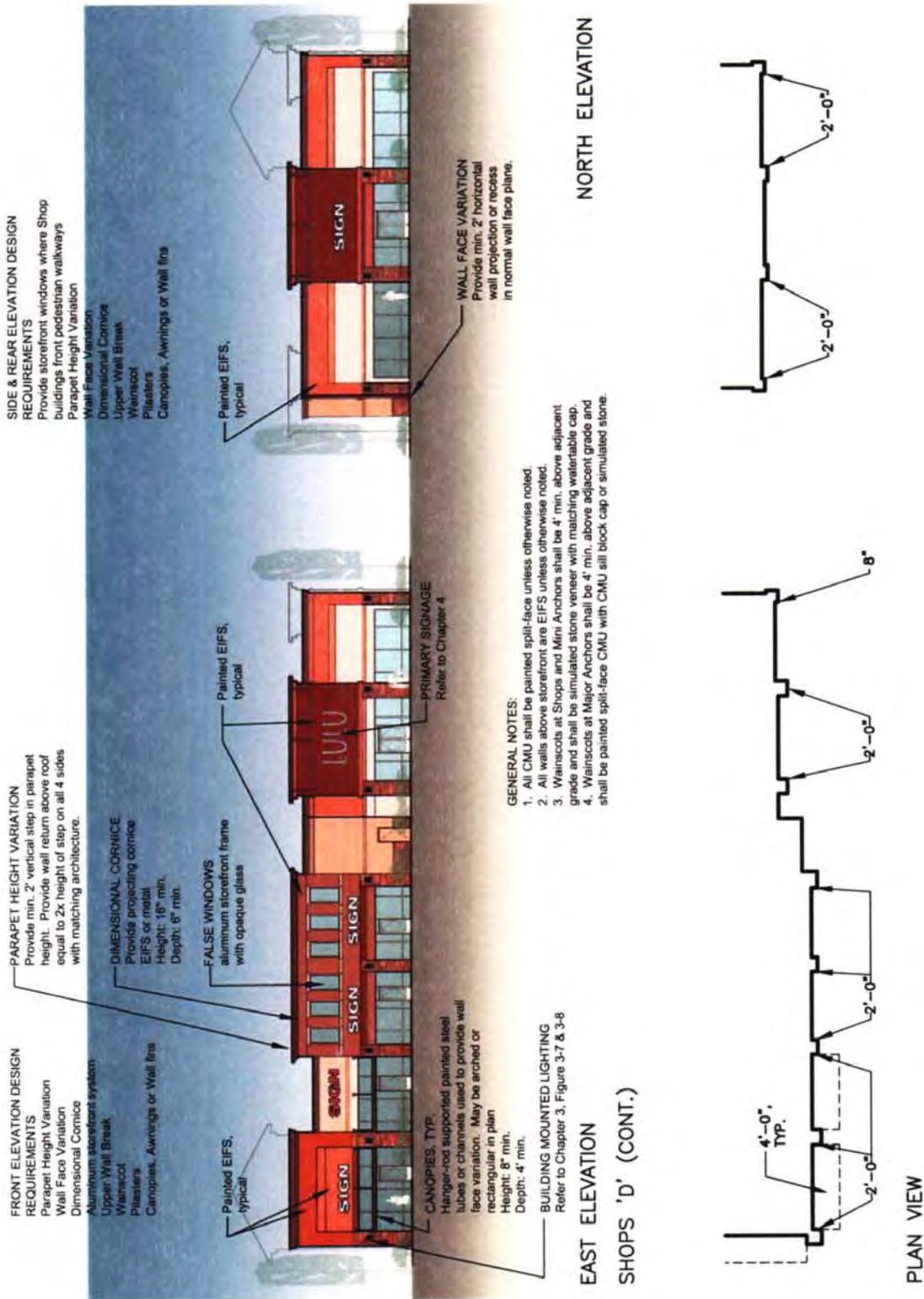


Figure 3-40 – Colored Elevations

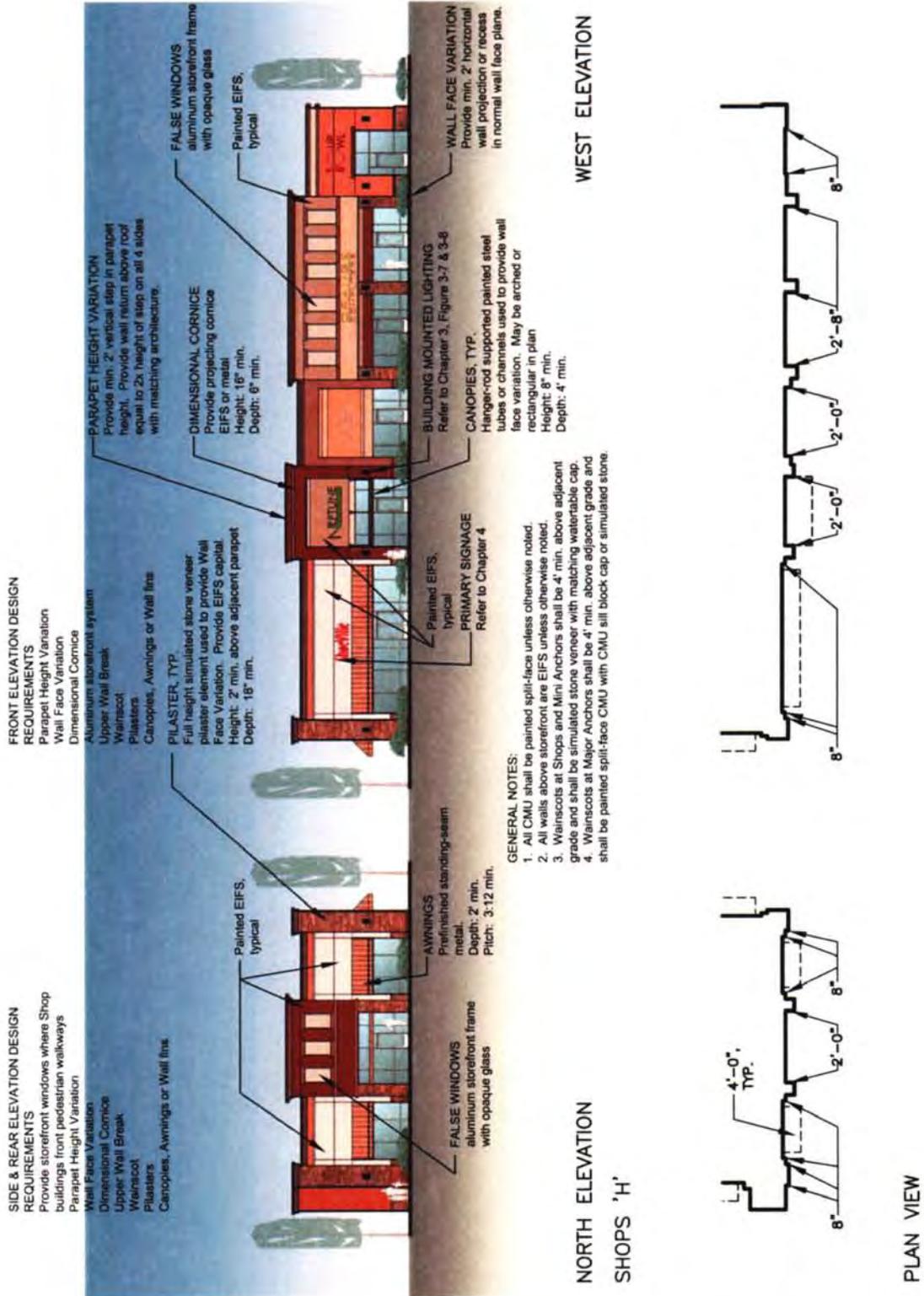


Figure 3-41 – Colored Elevations

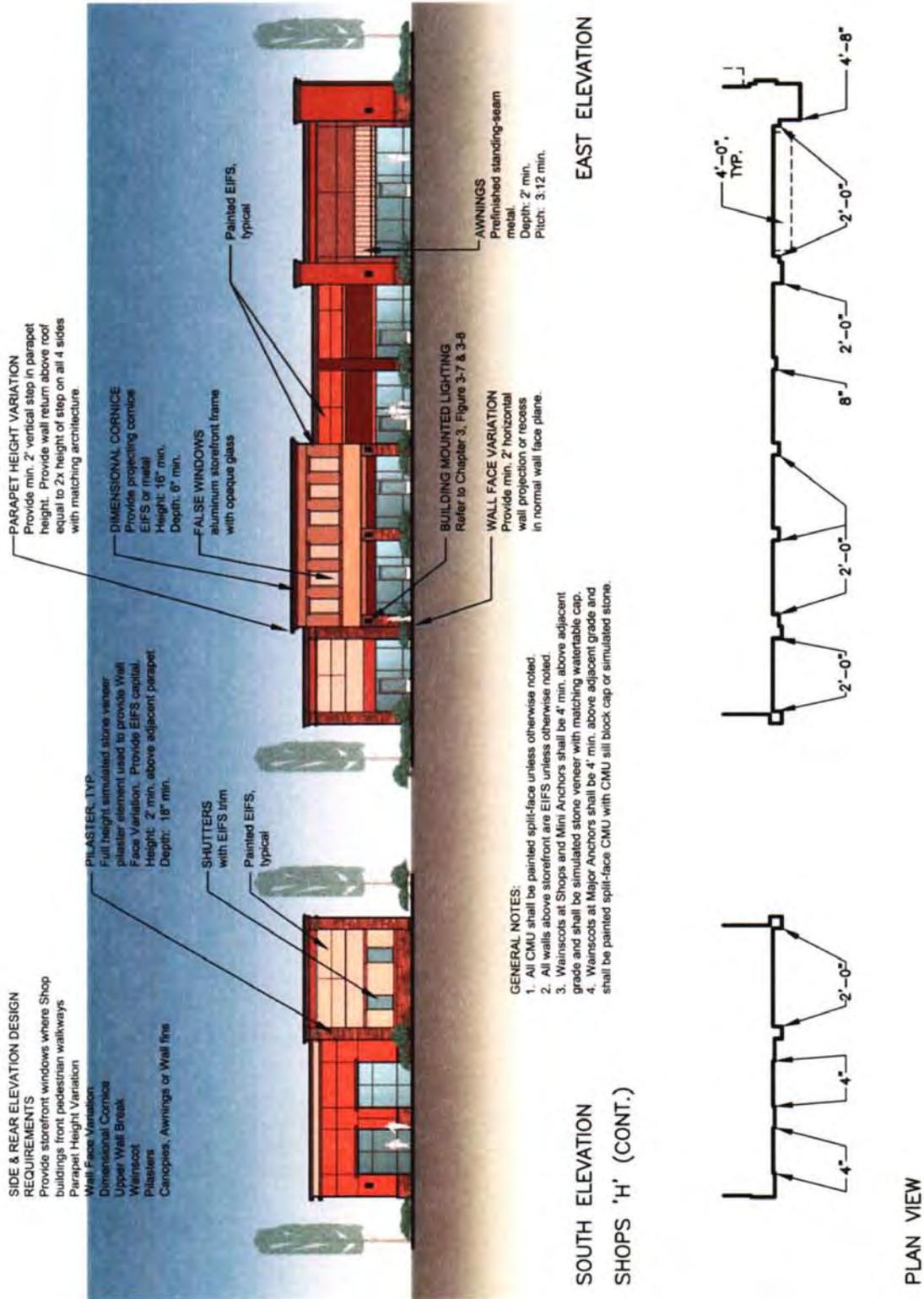


Figure 3-42 – Colored Elevations

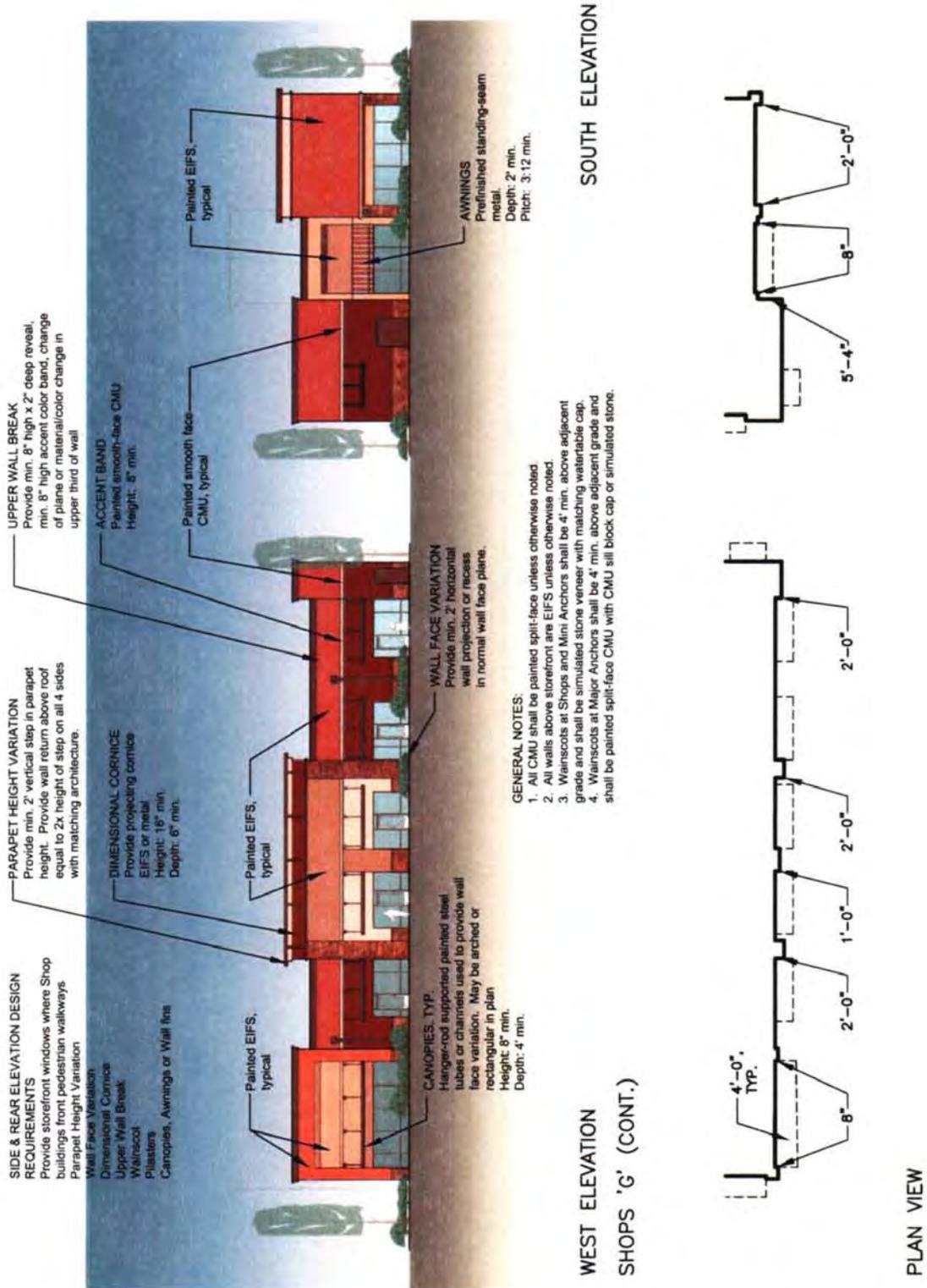


Figure 3-44 – Colored Elevations

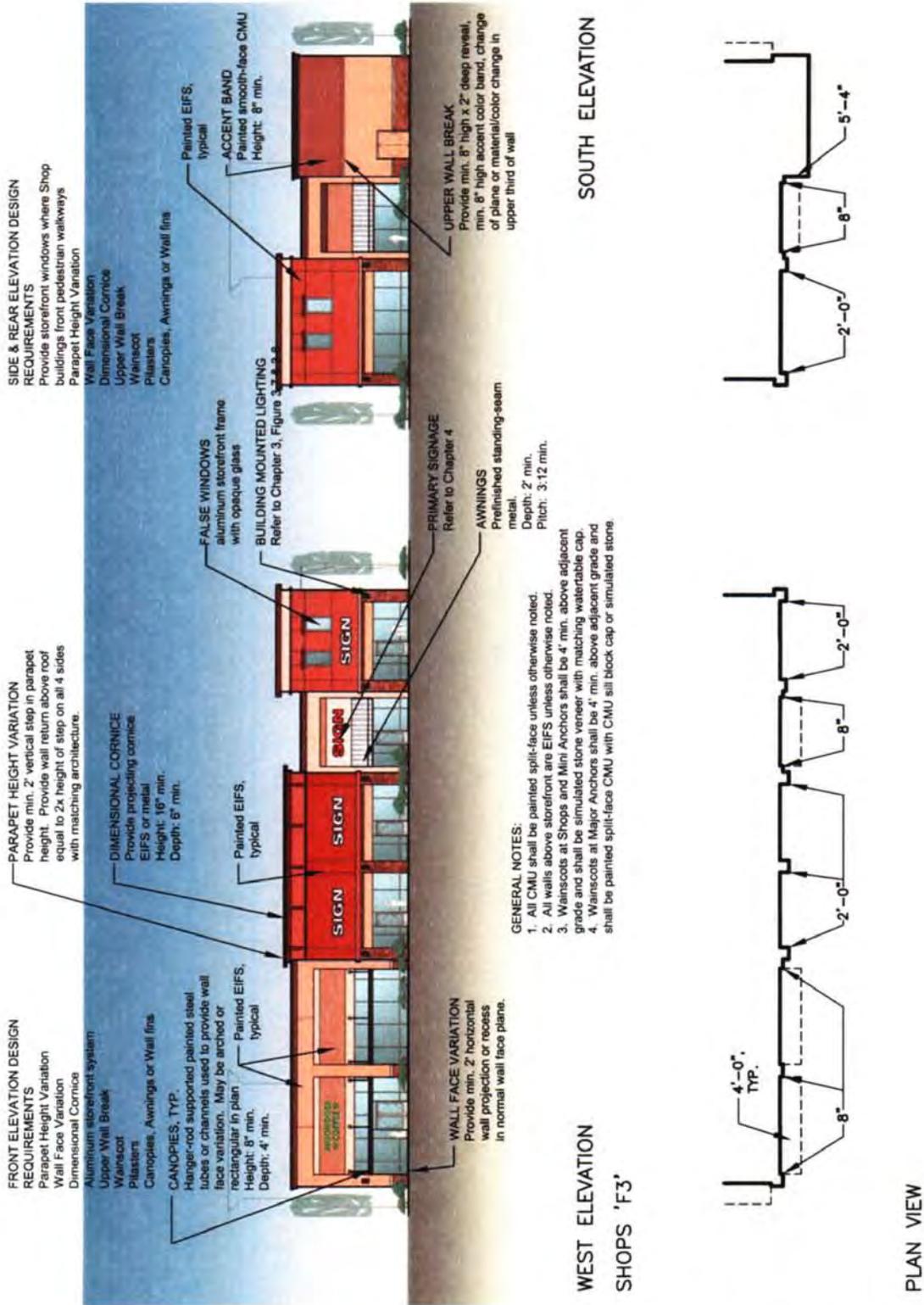


Figure 3-45 – Colored Elevations

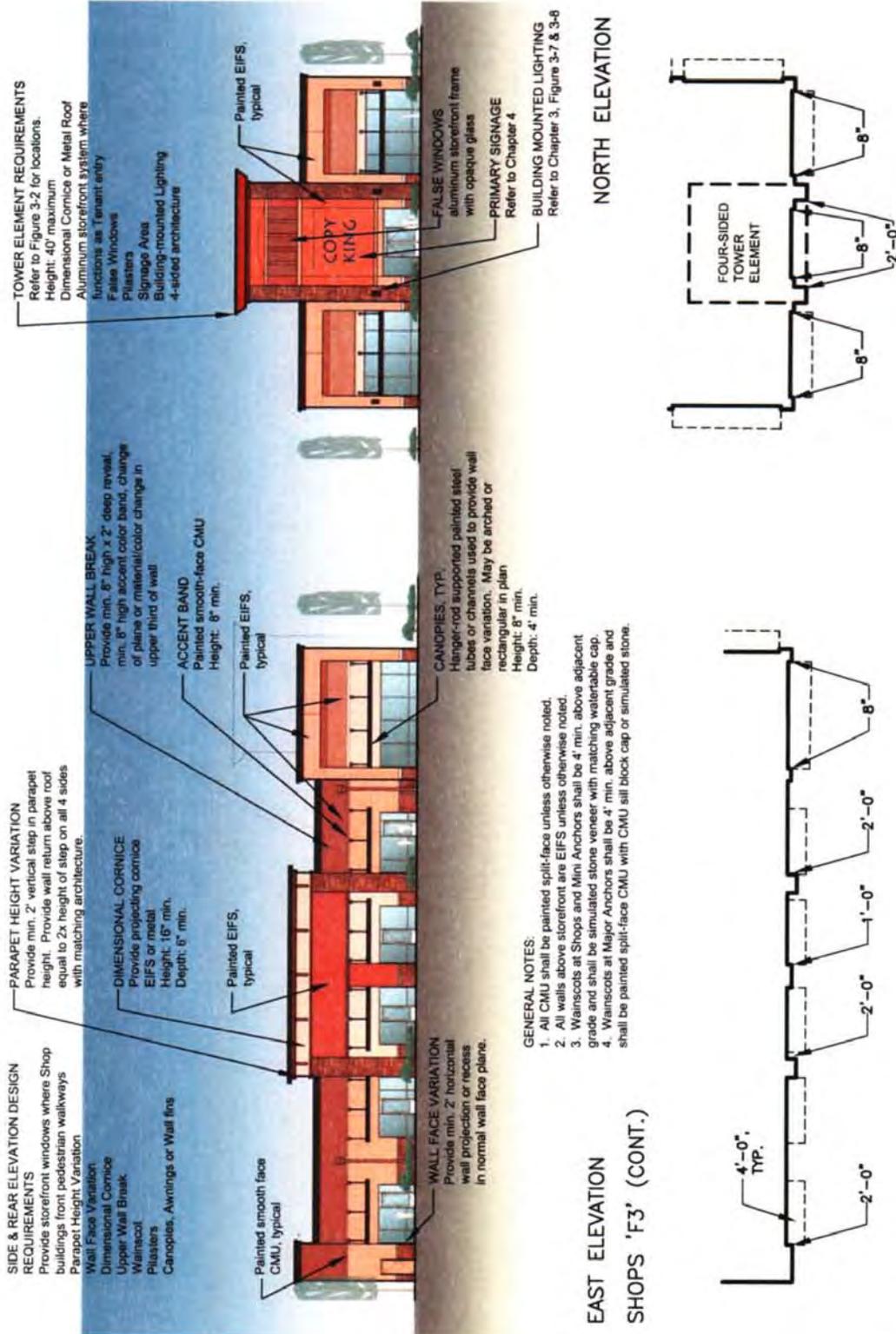


Figure 3-46 – Colored Elevations

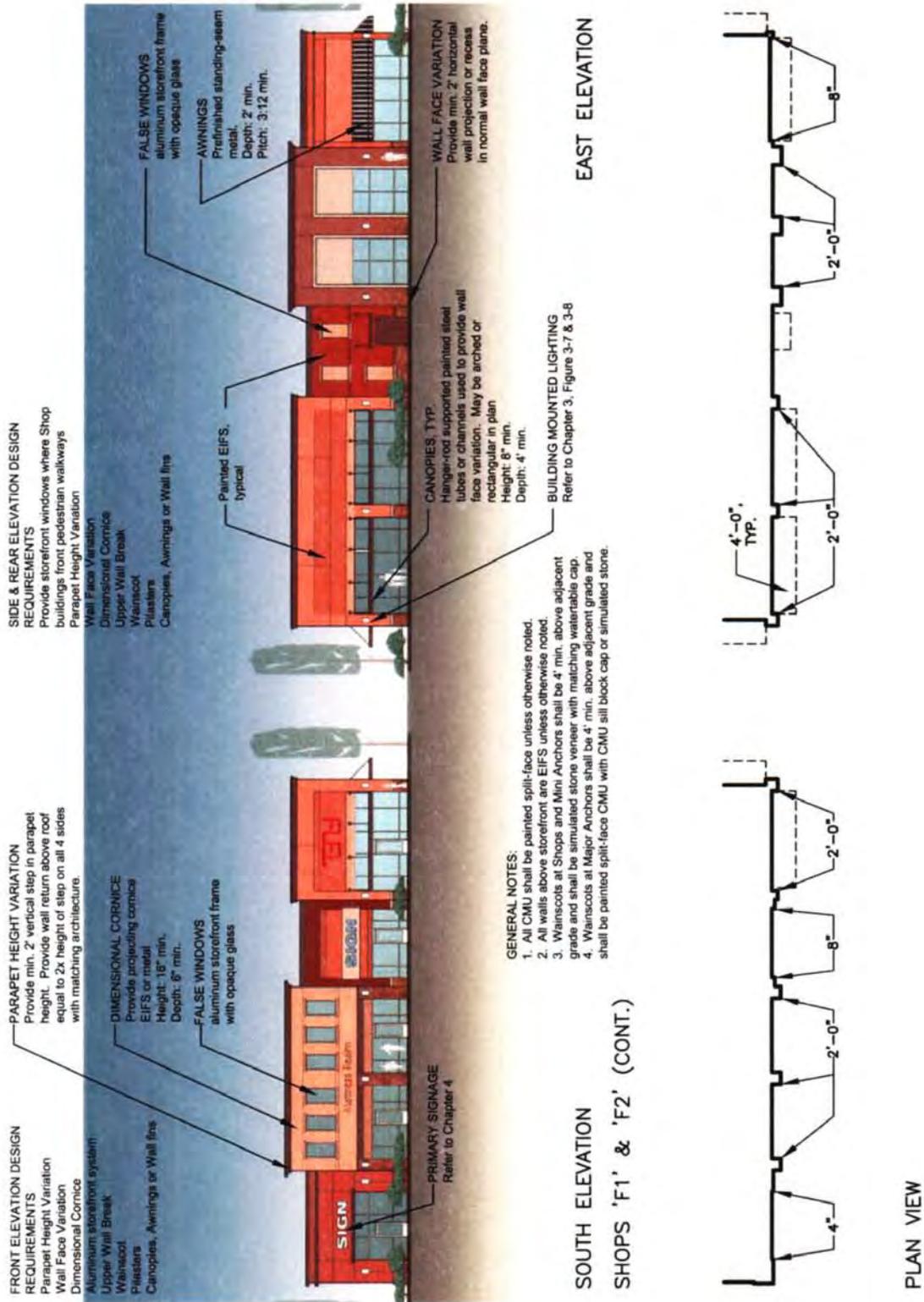
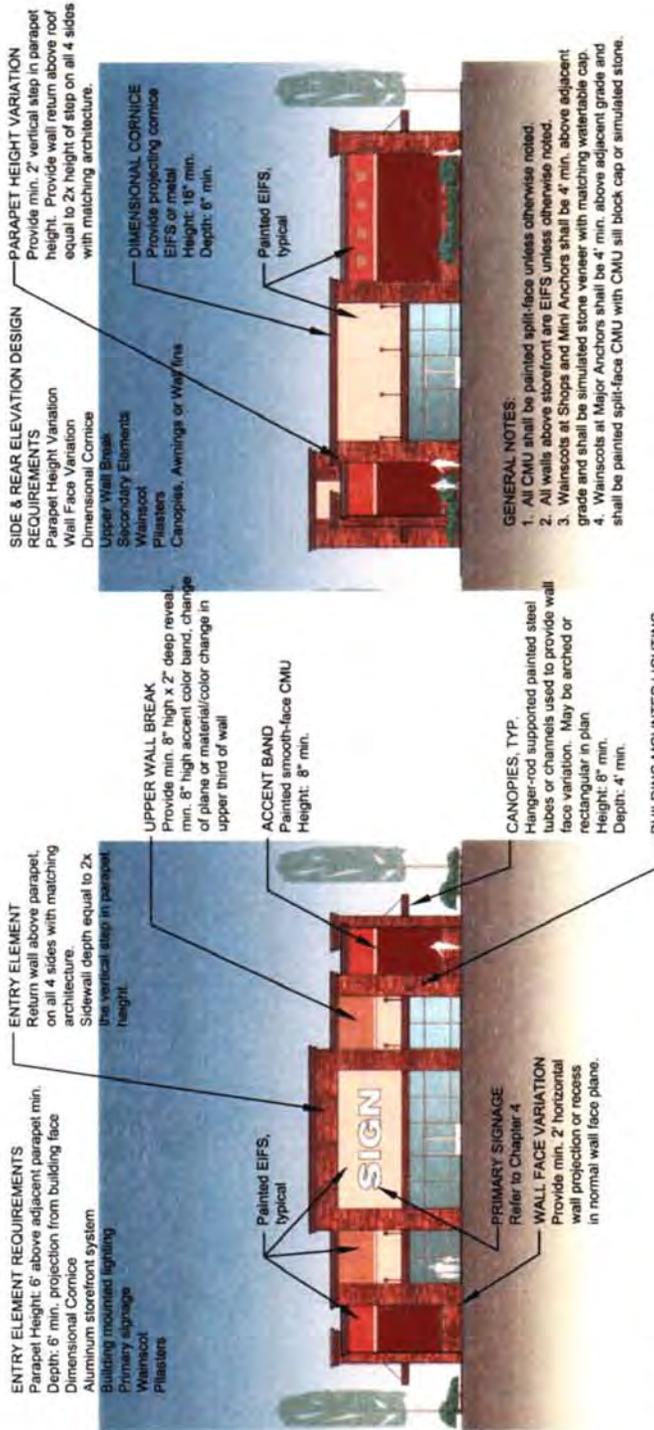
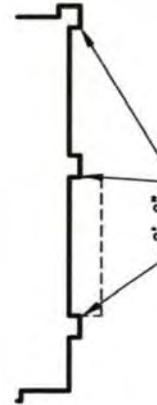


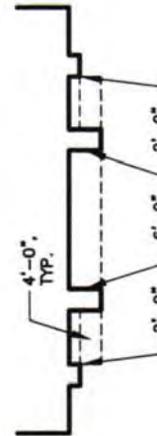
Figure 3-48 – Colored Elevations



NORTH ELEVATION



EAST ELEVATION
MINI 'G'



PLAN VIEW

Figure 3-49 – Colored Elevations

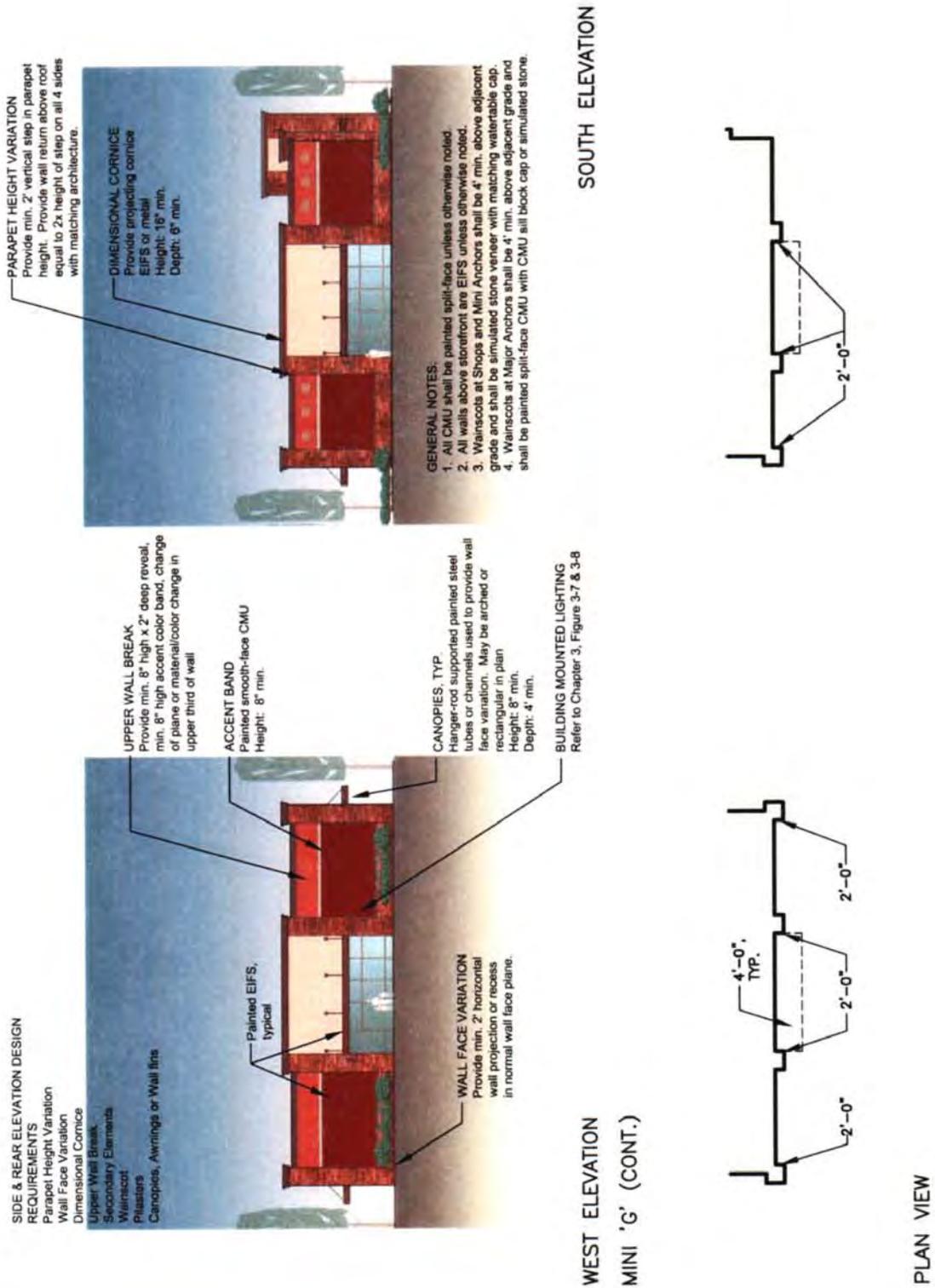


Figure 3-50 – Colored Elevations

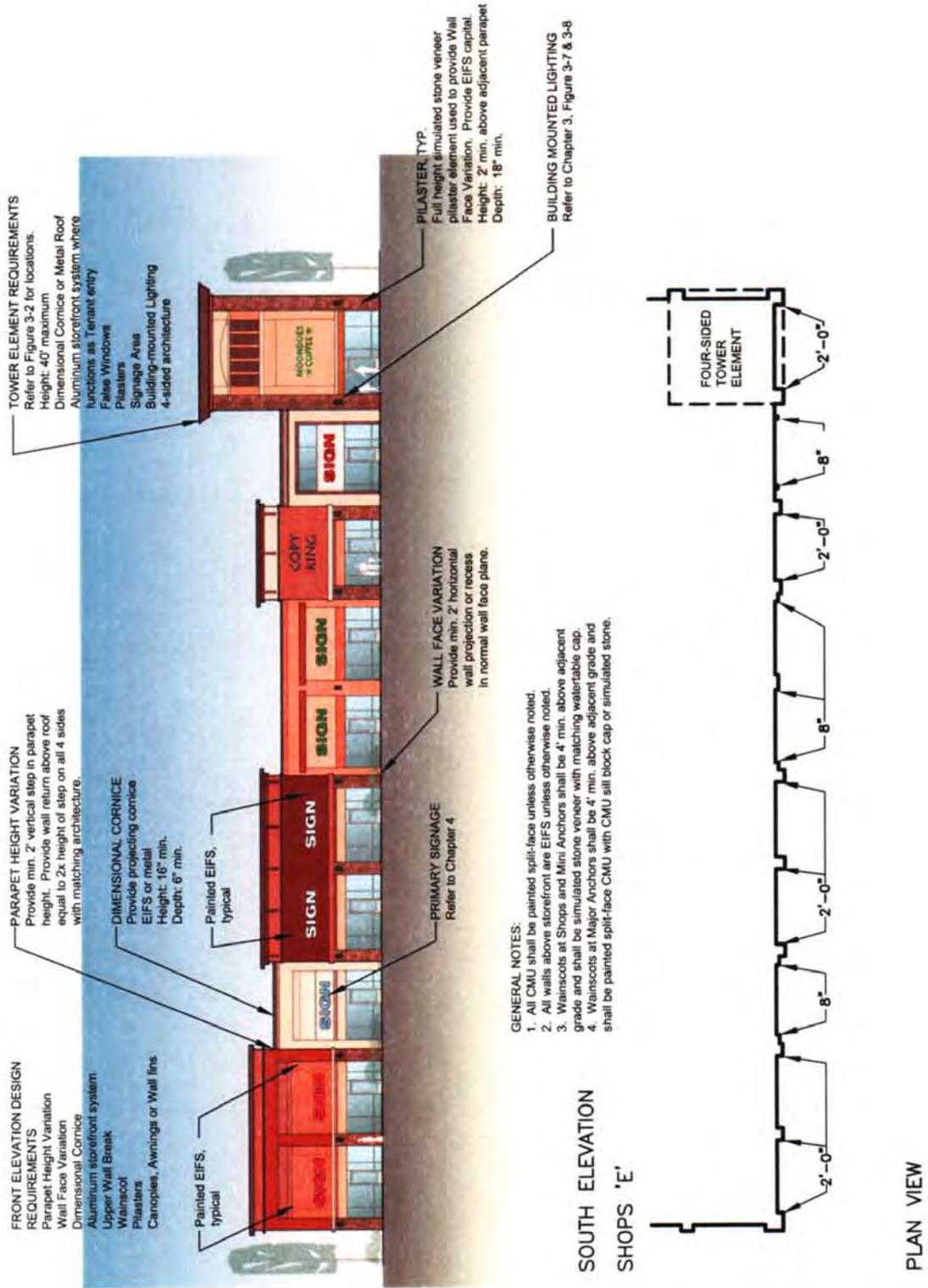


Figure 3-51 – Colored Elevations

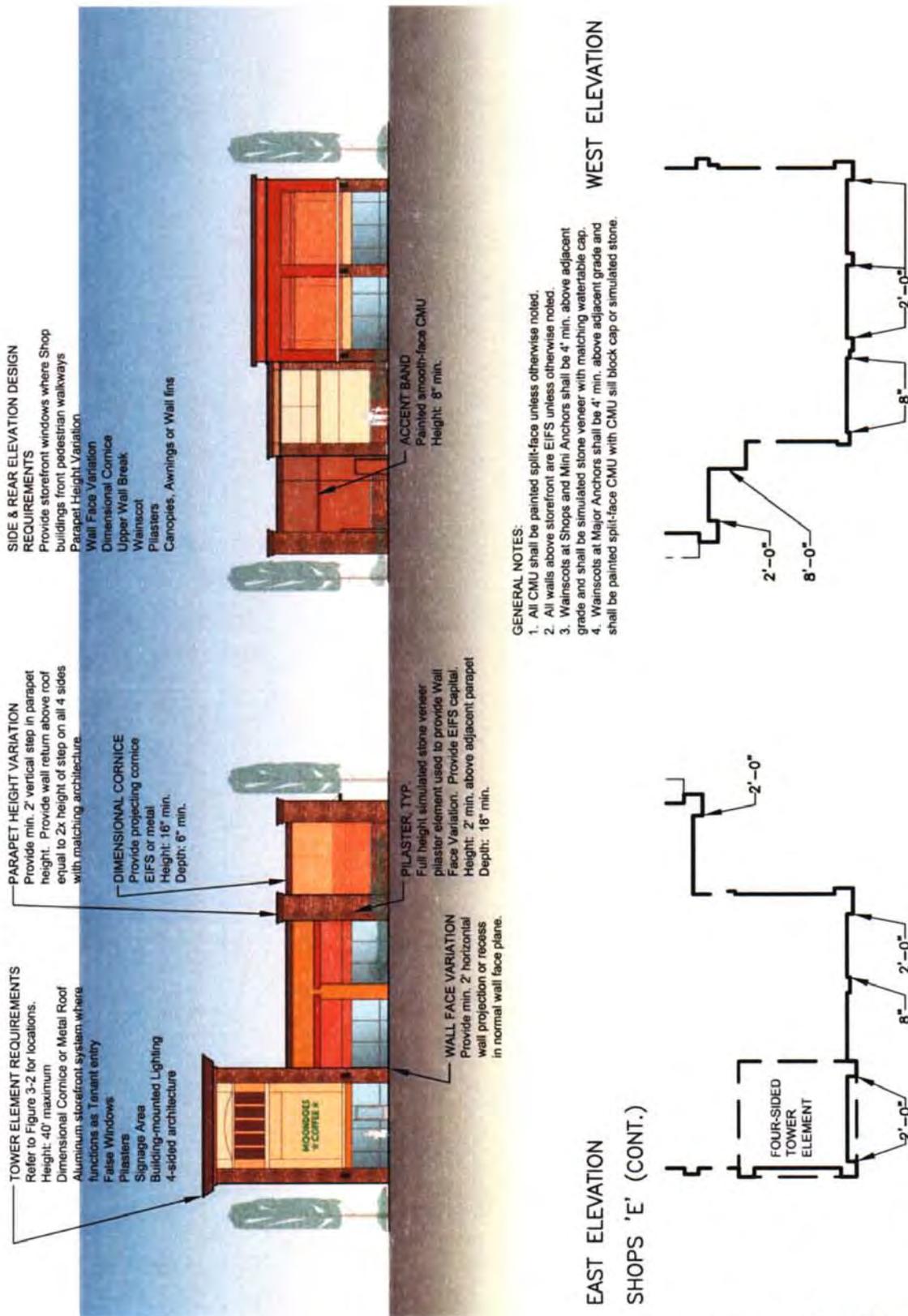


Figure 3-52 – Colored Elevations

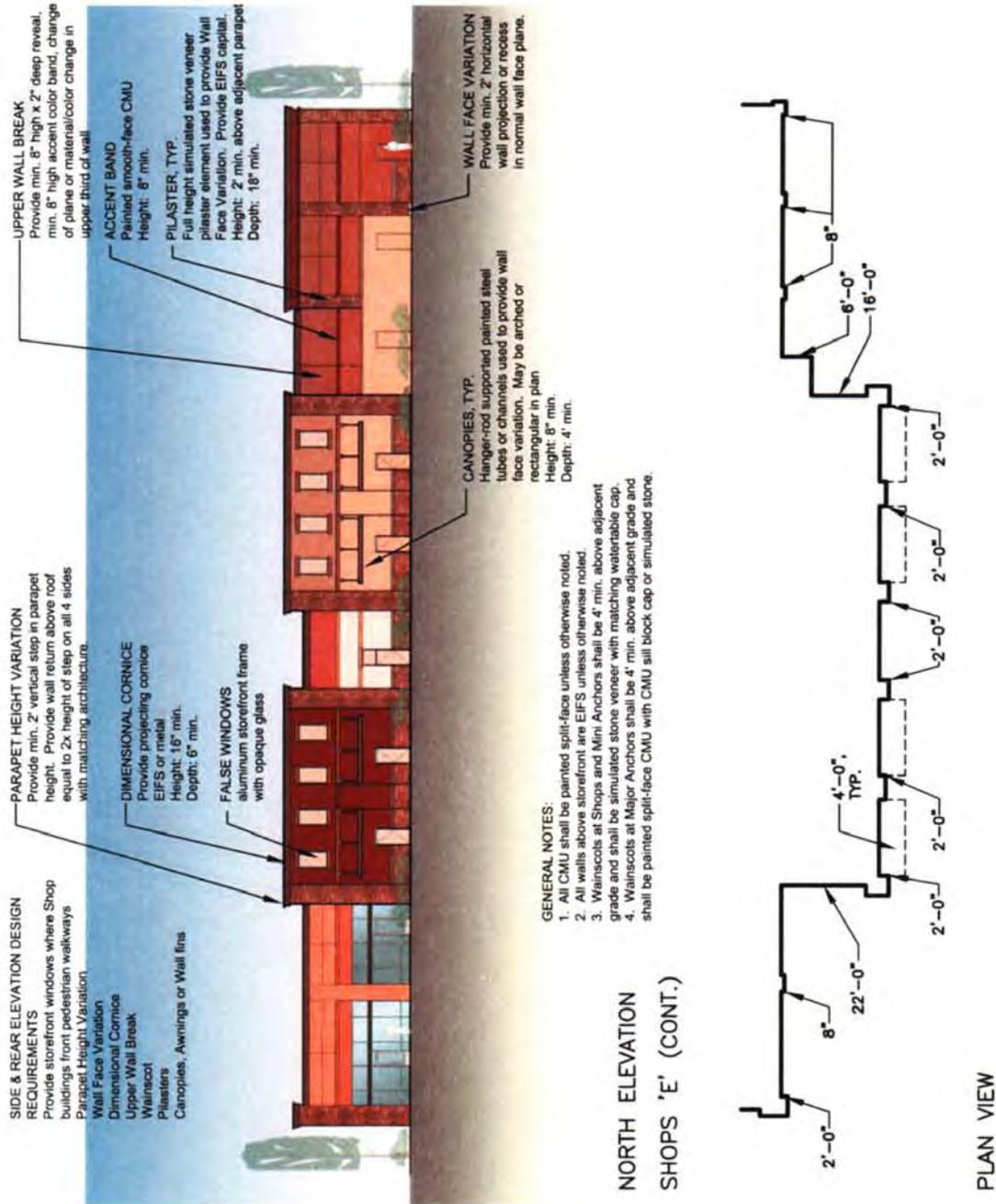


Figure 3-53 – Colored Elevations

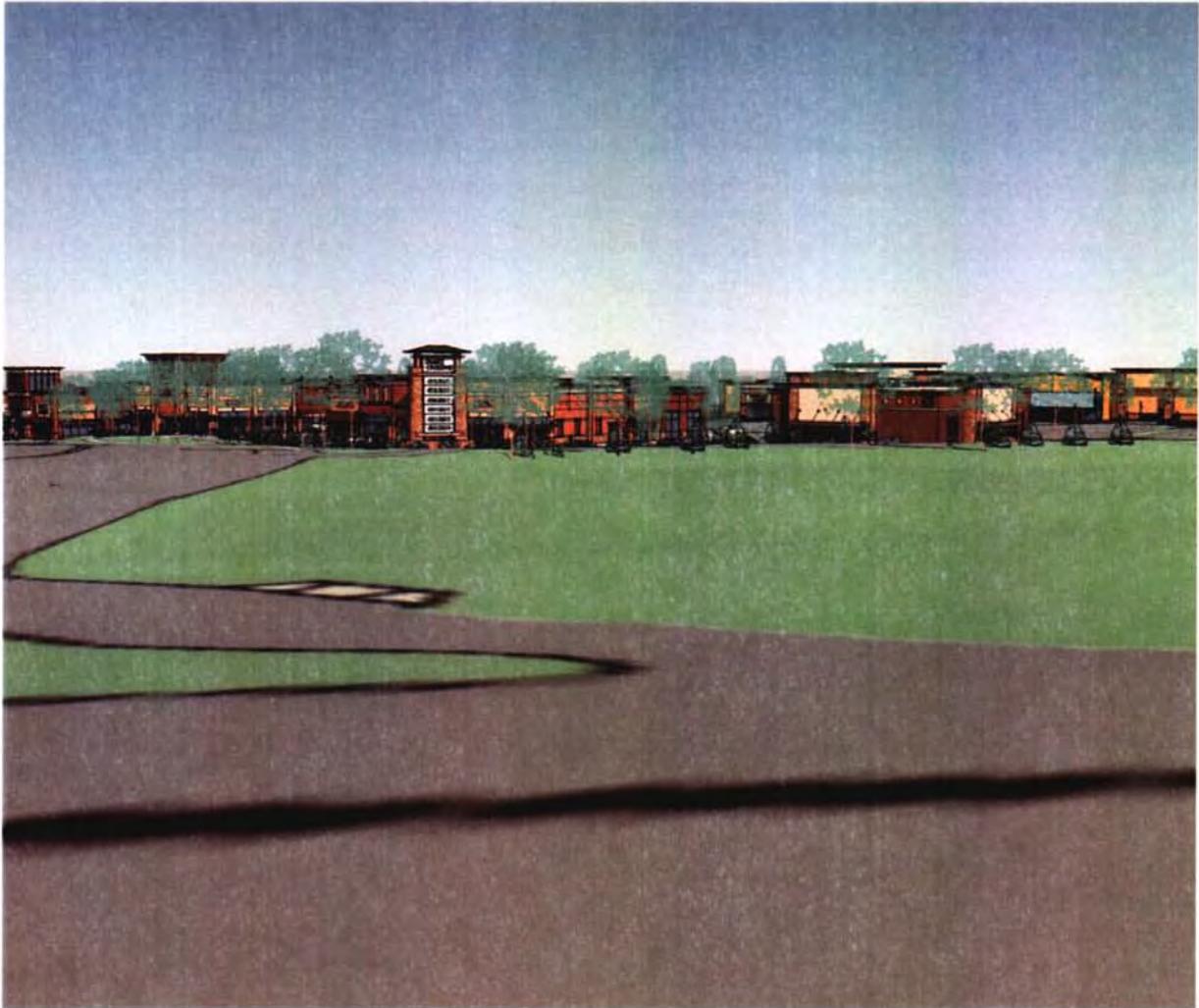


Figure 3-54 – View from Los Altos Parkway



Figure 3-55 – View from Los Altos Parkway



Figure 3-56 – View to Entry from Los Altos



Figure 3-57 – View from Los Altos Entry

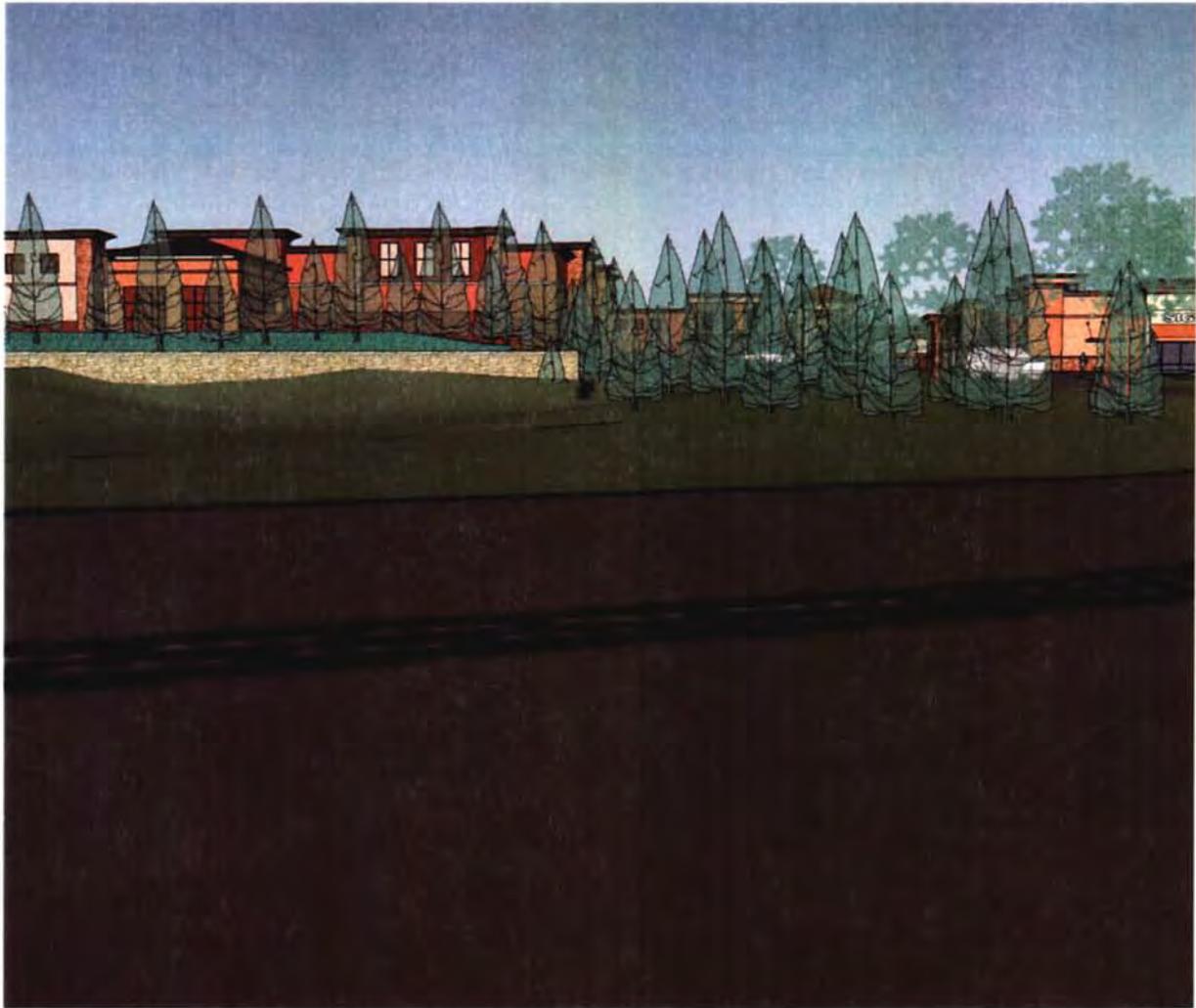


Figure 3-58 – View from Galleria Parkway



Figure 3-59 – View from Galleria Parkway



Figure 3-60 – View from Los Altos Entrance

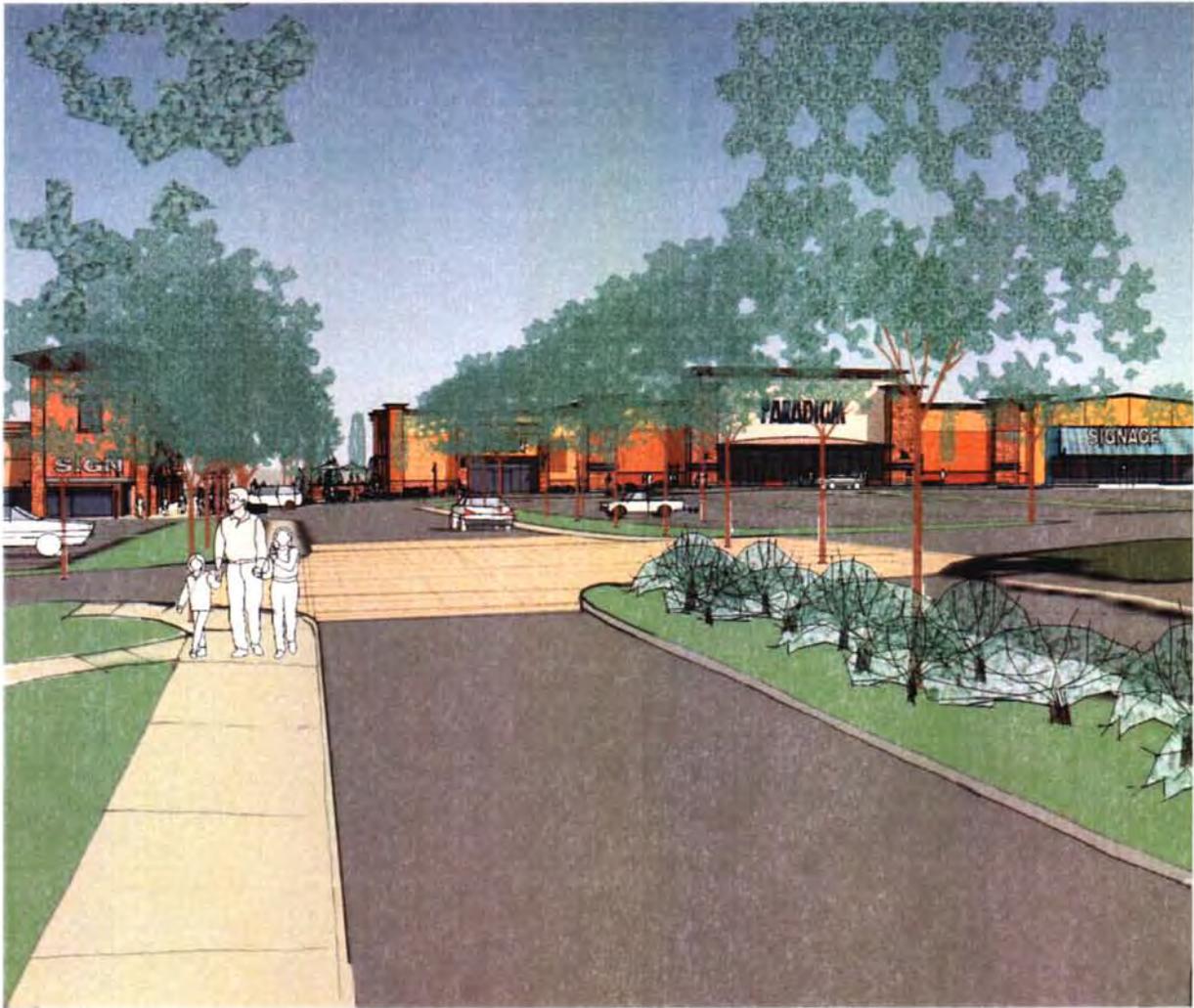


Figure 3-61 – View Toward Center Plaza



Figure 3-62 – View of Center Plaza



Figure 3-63 – View of Anchor Buildings



Figure 3-64 – View of Anchor Buildings



Figure 3-65 – View of West Plaza



Figure 3-66 – View of Shop Building



Figure 3-67 – View of Shop Buildings



Figure 3-68 – View to Entry From Galleria Parkway



Figure 3-69 – View from Galleria Parkway

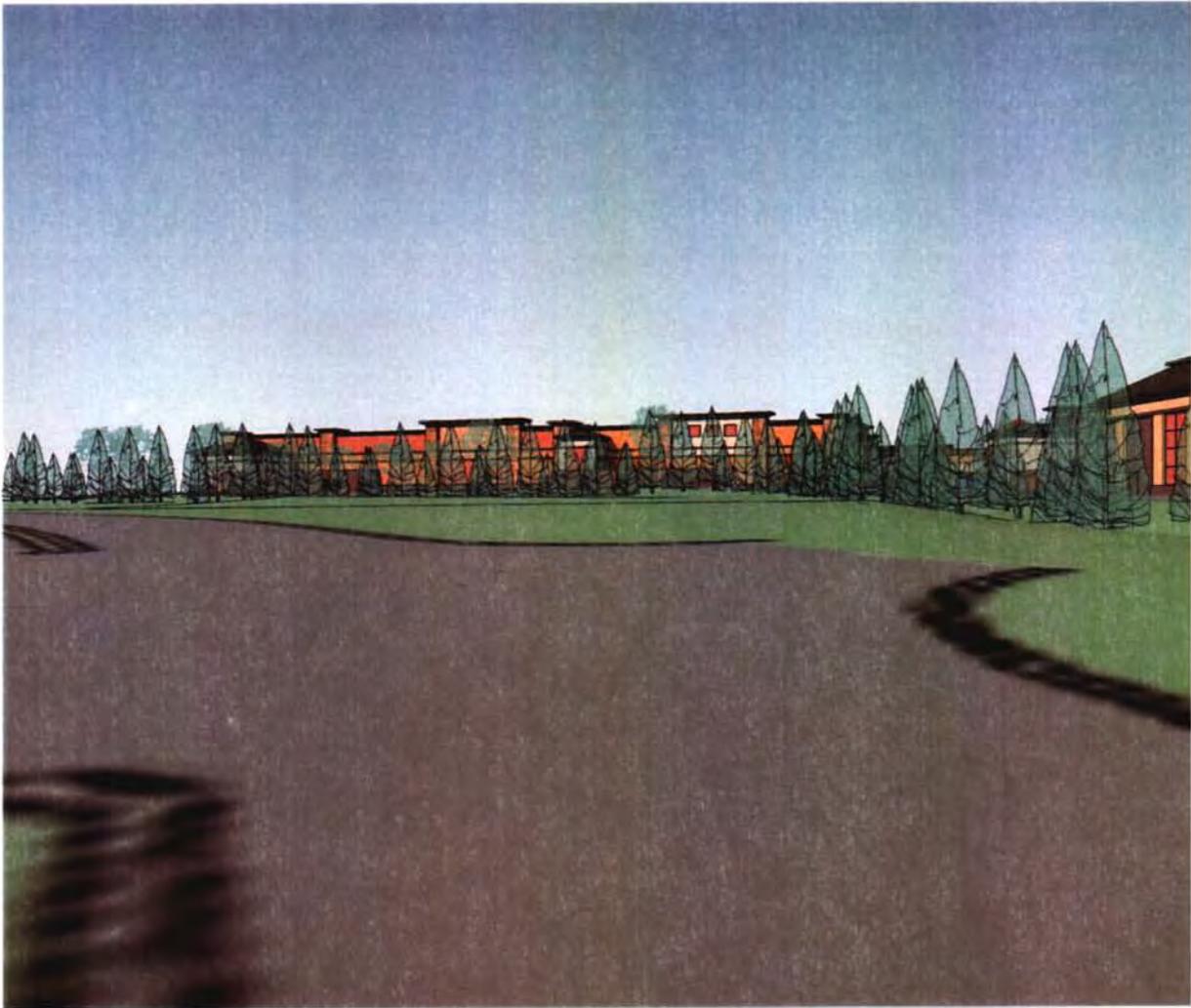


Figure 3-70 – View from Galleria Parkway

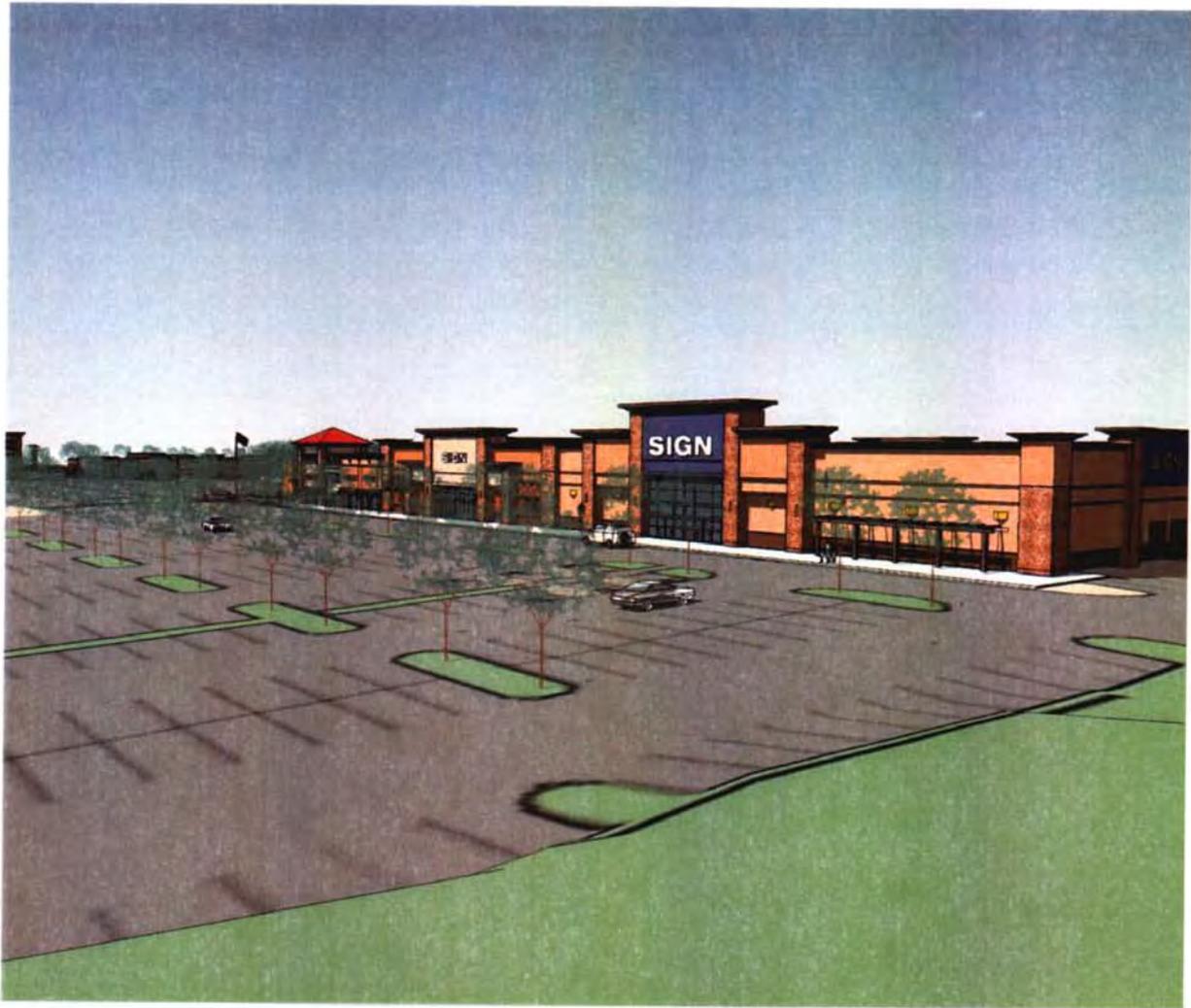


Figure 3-71 – View from Shoppers Way



Figure 3-72 – View to Center Plaza from West



Figure 3-73 – View to Shops C & D from Galleria Parkway Entry

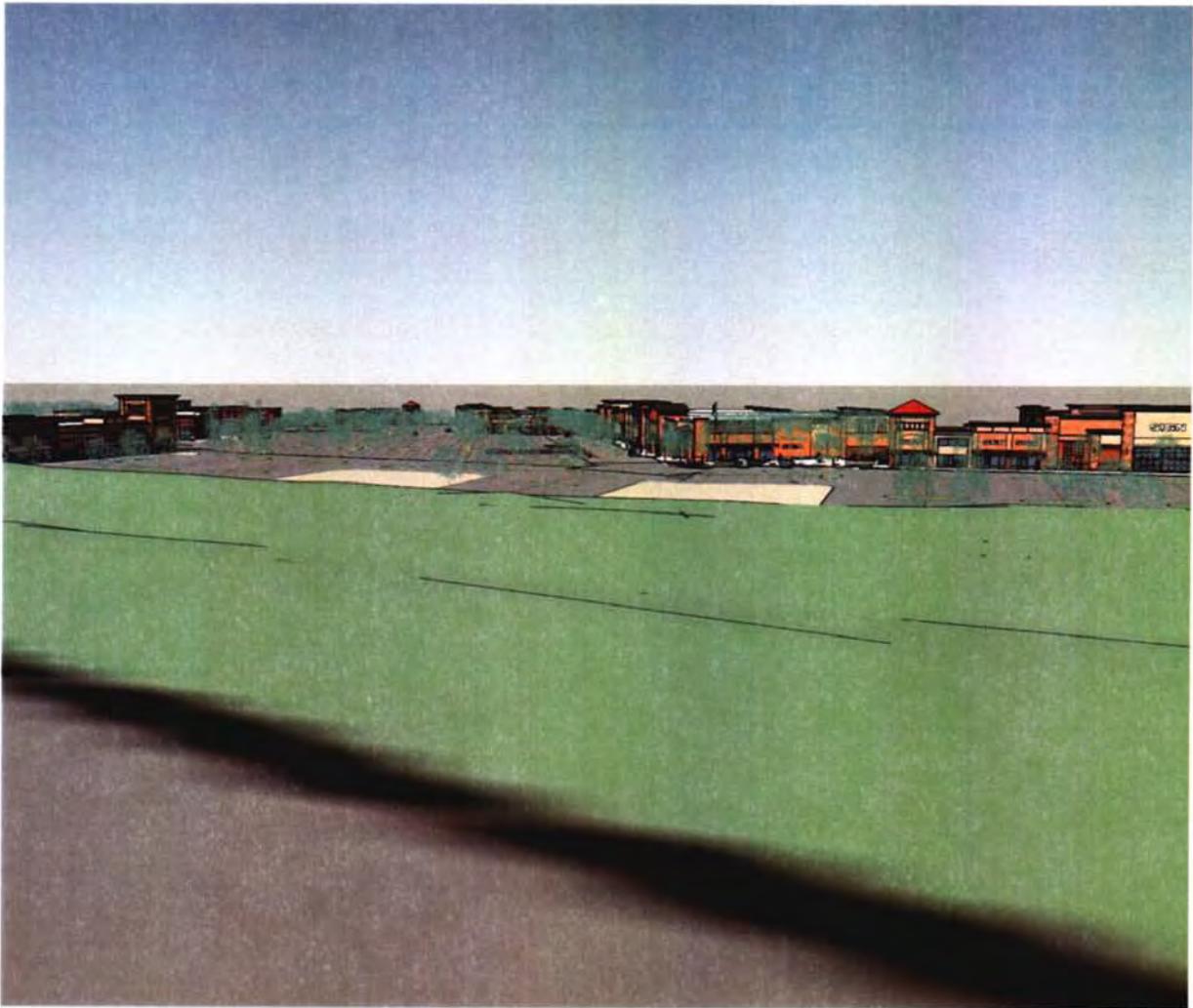


Figure 3-74 – View from Pyramid Highway

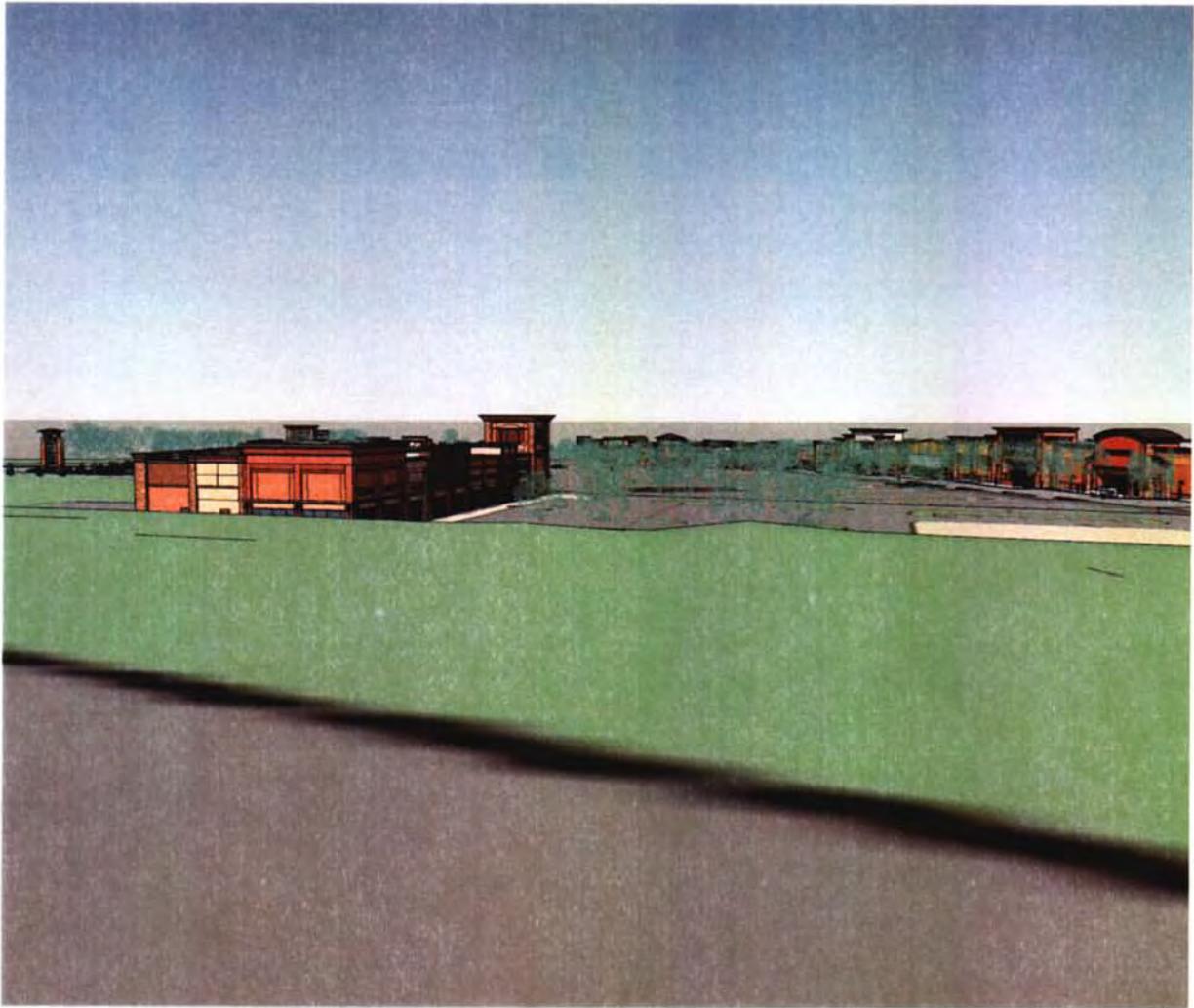


Figure 3-75 – View from Pyramid Highway



Figure 3-76 – View of Pylon Sign from Los Altos Parkway

Chapter 4 - Sign Standards

General Requirements

Definitions and Submittal Requirements:

1. Definitions of terms used herein are the same as the Sparks Municipal Code, Title 20, and Chapter 20.56 (SMC 20.56) unless specified otherwise in the body of the text.
2. For any issue or matter related to signage not covered by this standard, SMC 20.56 shall apply. In case of conflict with SMC 20.56, this standard shall apply.
3. No substitutes or other exceptions to these standards will be accepted unless first approved by both the Master Developer and the City of Sparks.
4. National Sign Program: The sign design standard of a nationally recognized tenant, restaurant chain or retail chain having 25 or more locations.
5. Pad User: A business entity using one of the free-standing pad locations as indicated on the site plan.
6. Primary frontage: The total frontage of a tenant space or pad exterior wall facing in one direction that contains the primary customer entrance for the premises.
7. Secondary Frontage: The side or rear of the premises not used for the primary customer entrance for the premises.
8. SMC 20.56: The City of Sparks Municipal Code Chapter 20.56 containing sign standards for the City of Sparks. Applies only to situations not covered in this standard.
9. Sign Height: The height of top of sign measured from average finish grade below sign.
10. It is expected that professional designers will prepare artwork for all tenants and that such artwork will include scaled schematic drawings and specifications. An elevation or photograph of the building with the sign and dimensions of signage shall be included with this submittal.

11. Submittals: shall be made first to the Master Developer and secondly to the City of Sparks for approval before any such work may begin. Submittals to the City of Sparks shall include the Master Developer's approval stamp (or approved as noted) or they will not be reviewed. Work started without written approval will be considered non-conforming and will be subject to immediate removal. In the event Tenant refuses to remove any non-conforming sign, the Master Developer shall have the right to cause said sign to be removed, at Tenant's expense, which expense shall include not only the costs associated with removing said sign, but also the costs of any repairs made by the installation or removal of the non-conforming signage.
12. Submittal Drawings to Master Developer: must indicate the following: a) The type and size of all lettering; b) The location of the sign in relation to the storefront, including an elevation of the building with the proposed sign drawn to scale; c) Section drawing(s) showing sign construction, raceway size (where required) and methods of attachment to the substrate or structure; d) Colors, finishes, material types, illumination and electrical specifications, and method of installation. Submittal drawings to City shall also include all requirements necessary for a sign permit.

Prohibited Exterior Signs:

1. Conformance to the guidelines for signs at Sparks Crossing will be strictly enforced. Any illegal non-conforming or unapproved signs are strictly forbidden and shall be removed by Tenant at Tenant expense.
2. Banners, flags (other than the Master Developer's American flag), flashing, moving, animated, blinking, or audible signs or effects are prohibited.
3. Any sign on a vehicle parked for a duration of time exceeding 24 hours, which indicates its use is for the purpose of advertising a particular tenant, service or product is prohibited.
4. Signs that are movable or transportable, painted wall signs, and printed or painted glass signs are prohibited. Signs shall not extend higher than the fascia sign band, and no sign or part of a sign shall be mounted on the roof.
5. Temporary signs other than those approved in writing by the Master Developer, and allowed by the Sparks Municipal Code are prohibited. In no case, shall any temporary banners or similar displays be attached or affixed to any permanent freestanding signs.
6. Box signs, cabinet signs, "A" frame signs, and signs that do not include individual letters are prohibited. Exception: Tenant's or Occupant's nationally recognized logo or sign program will be allowed when floor area exceeds 4,000 SF.

7. Signs on benches.
8. Roof mounted signs.
9. Official signs.
10. Signs or copy permanently affixed to the fabric of umbrellas used in tenant outdoor dining areas, except that which has been specifically pre-approved by the Master Developer.
11. Banner programs, seasonal and holiday decorations not in conformance with the SMC.
12. Any sign that the Master Developer determines is in poor taste or is otherwise objectionable as determined by the Master Developer.

Allowable Signs

Excepted signage:

The following signage is allowed as specified below and is not to be counted against area allowances specified in the following section:

1. Rear door signage to include name of tenant or business, logos and address, up to (3) three SF maximum area. All such signage, except for logos and national sign programs, shall be white die cut vinyl letters, Helvetica medium typeface. Address and suite numerals and letters are limited to 3" in height.
2. Entrance door operation hours and emergency contact information, up to 1 SF total area. All such signage shall be white die cut vinyl letter, Helvetica medium typeface only. Credit card logos are also allowed within specified area limits. Entrance door signage shall be located on or directly adjacent to the entrance door.
3. Front door address and suite signs. These shall be white die cut vinyl letter no taller than 4 inches, or as required by the local authorities. Typeface shall be Helvetica medium.
4. Drive through menu signage: Allowed up to 36 SF and six feet in height; logos and business name allowed as long as area does not exceed (2) two SF (to be included in 36 SF total).
5. Cart Corral Location signage. One Logo or Business name and/or Tenant logo or other signage is allowed on cart corrals located in the parking lot if limited to two (2) SF in total area. Advertising signage is not allowed on the cart corrals. Height of corral location signage may not exceed eight feet above grade.

6. Drive through directional signage up to (2) two SF and (4) four feet in total height.
7. Service Directional Signage: Signs locating tenant service areas or bays (i.e.: Battery installation, "Lube", or similar) may be installed on the secondary frontages of a single use freestanding tenant building, limited to (2) two SF maximum area each sign and (8) eight SF total of all signs. All such signs shall be white pin mounted metal or plastic letters. Color shall be white on dark backgrounds or black on a light background unless part of a national sign program.
8. Temporary and real estate signage is allowed as per the SMC.
9. Temporary window signage, if limited to 25% of the total glass area of the subject tenant.
10. Neon window signage is allowed if pre-approved by the Master Developer, limited to one sign per tenant space and included in the temporary window signage allowance of 25%.
11. Master Developer's seasonal banner and/or national holiday program, if any, consisting of decorative banners mounted on lighting standards in the parking lot for up to 45 days, subject to review and approval by City Staff.
12. Other signage as exempted by the SMC.

Allowable signage areas and locations:

1. Allowable building sign area: Each tenant space, Anchor, Mini Anchor or free standing pad user is allowed a total sign area allowance equal to 2 square feet of signage per linear foot of primary frontage.
2. Freestanding and monument signage, as limited by these standards, shall not count against allowable building signage. Excepted signage listed above is in addition to the sign allowance.
3. Sign area of each sign or collection of graphics or letters is to be calculated by drawing a rectangle, triangle or circle around the sign or graphic and calculating the total area thereof. A grouping of sign copy, letters and logo will be counted as one sign.
4. Each tenant space or pad user is allowed up to two wall signs. The second sign may be located on the secondary frontage of the individual tenant space only and may not exceed 1/3 the total sign allowance.
5. Sculptural elements are allowed as a part of a wall sign if part of a tenant's national sign program and the area thereof is counted within the total sign area.

6. Projecting or suspended (Blade) signs: Blade signs may be installed if design and location is pre-approved by the Master Developer and City. Such signage may be suspended from the underside of a canopy or wall mounted, with a maximum projection of 3 feet and a maximum area of 6 SF. Such signs shall count against the allowable number of signs, and shall be subtracted from the allowable sign area.
7. Permanent window signage, wall mounted plaques and logos are allowed if part of a tenant's national sign program and the sign area of each sign or graphic is subtracted from the total allowable sign area.
8. For inline tenant wall signs, the length of the sign may not exceed 75% of the tenant frontage or the sign band length, whichever is smaller. Height of tenant's wall sign may include one or two lines of copy, and total height shall not exceed 80% of the signband height. Signband height and width shall be defined as the clear flat wall area below, above or between any architectural projections, elements, or recesses.
9. No cabinet signs or fully illuminated plastic face panels shall be allowed. Signs shall be channel letters or push-out plexi-glass with metal backing.

Freestanding and Monument Sign Standards

Freestanding signage:

1. Up to three thirty foot tall freestanding signs are allowed for Sparks Crossings. Such signs may be located on the project property, at locations as determined by the Master Developer and City, except that at least one sign may be located on an adjacent or separate parcel if the Master Developer is granted an easement by the adjacent property owner. Specific location of the signs relative to the ROW and other freestanding signs shall be as per the SMC. The height of the sign does not include any architectural features that project up to 6 feet (20% of total sign height) above the top of the sign. Design style of sign, number of tenant panels, and sign area shall be as indicated in Figure 4-1.
2. Each Freestanding sign is to be two sided and allowed one project name panel and up to 6 tenant signs each side. Tenant sign areas shall be of equal size and dimensions. The Master Developer will determine which tenants are allowed on which tenant panel space. Maximum area of each tenant sign shall not exceed 3 feet by 8 feet each side. Project name panel shall not exceed 32 SF.
3. Tenant copy is limited to the sign area shown in Figure 4-1, and may be tenant's national standard typeface, but shall be push thru acrylic letters with a one inch projection and with rear illumination. All copy shall be white, exact color as specified by the Developer.

- 4. No cabinet signs or fully illuminated plastic face panels shall be allowed.

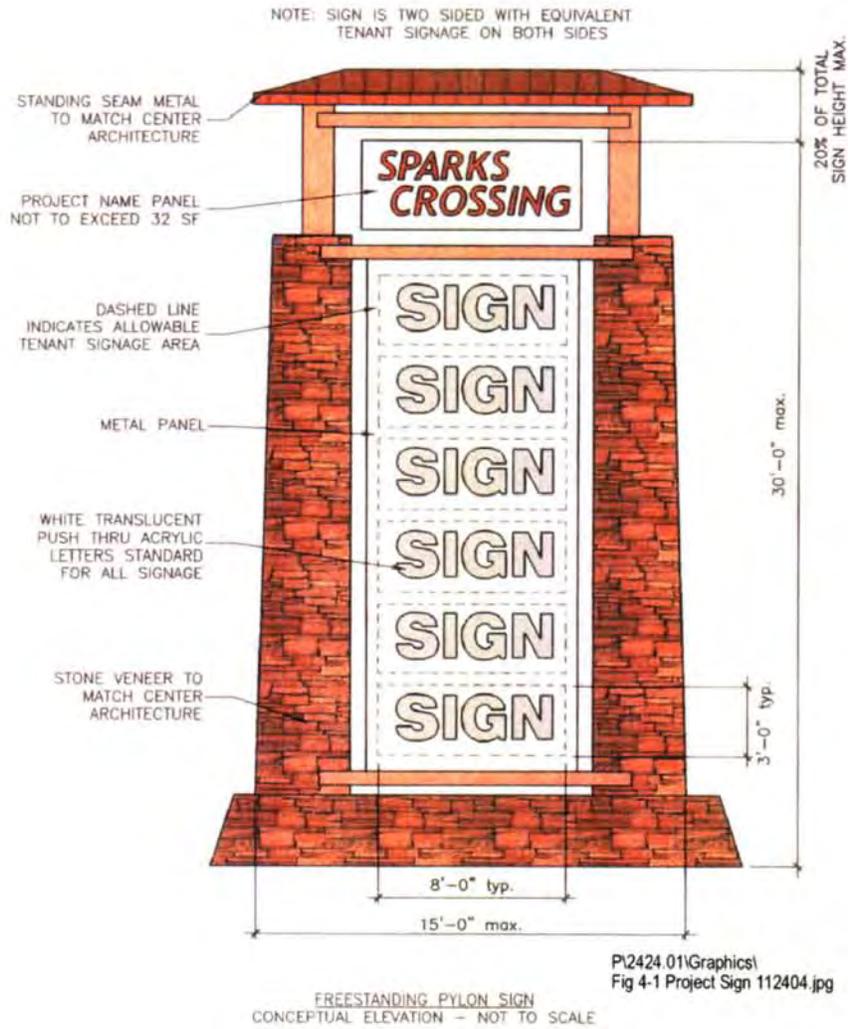


Figure 4-1 – Project Identification Sign

Monument Signage:

1. Up to two (2), six (6) foot tall, monument signs per main vehicular entrance roadway are allowed for Sparks Crossings, up to a total of ten. Such signs may be located on the project property, at locations as determined by the Master Developer and City, except that one sign may be located on an adjacent or separate parcel if the Master Developer is granted an easement by the adjacent property owner. Specific location of the signs relative to the ROW and other freestanding and monument signs shall be as per the SMC. The height of the sign does not include any architectural features that project up to 1.2 feet (10% of total sign height) above the top of the sign. Design style, number of panels, and sign area shall be as indicated in Figure 4-2.
2. Each monument sign is to be two sided and allowed up to 4 tenant signs each side. Tenant sign area shall be of equal size and dimensions. The Master Developer will determine which tenants are allowed on which tenant panel space. Maximum area of tenant signs shall not exceed 2'-2" by 4'-2" each side.
3. Tenant copy may be tenant's national standard typeface, but shall be push thru acrylic letters with a one inch projection, with rear illumination. All copy shall be white, exact color as specified by the Master Developer.
4. No cabinet signs or fully illuminated plastic face panels shall be allowed.

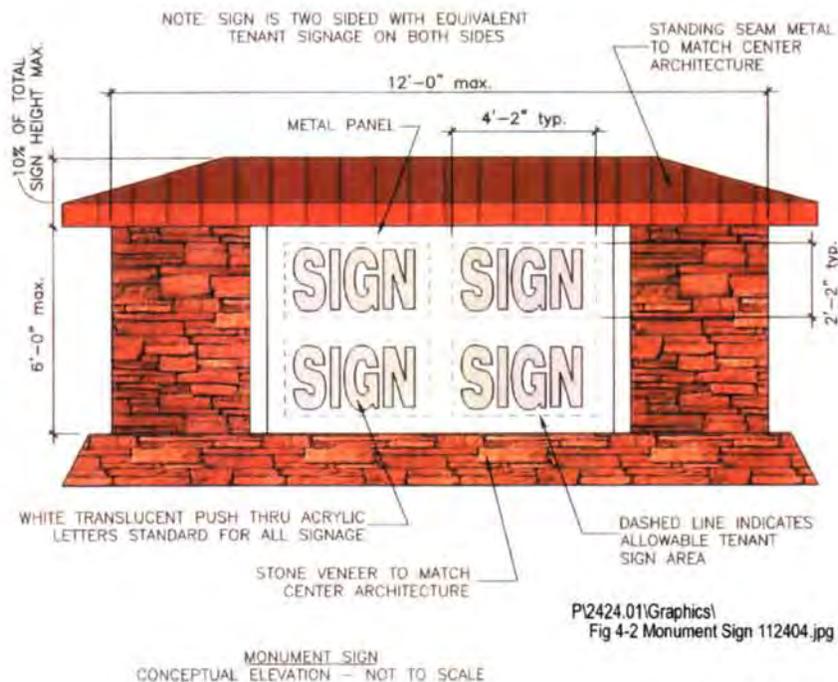


Figure 4-2 – Monument Sign

Chapter 5 – Infrastructure Phasing

Purpose

The purpose of this chapter is to address phasing and timing of six key elements of public infrastructure. Those include:

- Major Street Network – Principal Access
- Timing of Major Roadway Improvements
- Sanitary Sewer
- Storm Water Management
- Public Water System
- Orr Ditch Conveyance

Major Street Network - Principal Access

Principal access to Sparks Crossing is proposed as follows:

- Los Altos Parkway – One full movement median divided driveway restricting westbound left-turns out, and one eastbound median divided right-in/right-out driveway. Also, there is an exclusive eastbound right turn drop lane from Pyramid Highway to the driveway furthest east that serves Sparks Crossing. This was to separate the high northbound right turn volumes turning from Pyramid Highway from the eastbound through traffic on Los Altos.
- Galleria Parkway – Two full movement median divided driveways. The southernmost driveway is aligned with the primary driveway of the abutting multi-family site to the east.
- Pyramid Highway – One right-in/right-out driveway (Shoppers Way) from Pyramid Highway, with a full movement driveway from Shoppers Way.

Los Altos Parkway and Galleria Parkway are both arterial streets on the Regional Road network. Principal access to the site includes two driveways on Los Altos and two driveways on Galleria Parkway. Also, the right-in/right-out driveway proposed from the Pyramid Highway (Shoppers Way) is shared with the Sparks Galleria property to the south. That driveway requires an easement through the BLM property abutting Sparks Crossing on the west (already approved/issued), and approval by the Nevada Department of Transportation for the connection to the highway (encroachment permit process is complete). Figure 5-1, Access Plan (following page) depicts project access as well as the trip distribution percentages as estimated by Solaegui Engineers.

SPARKS CROSSING

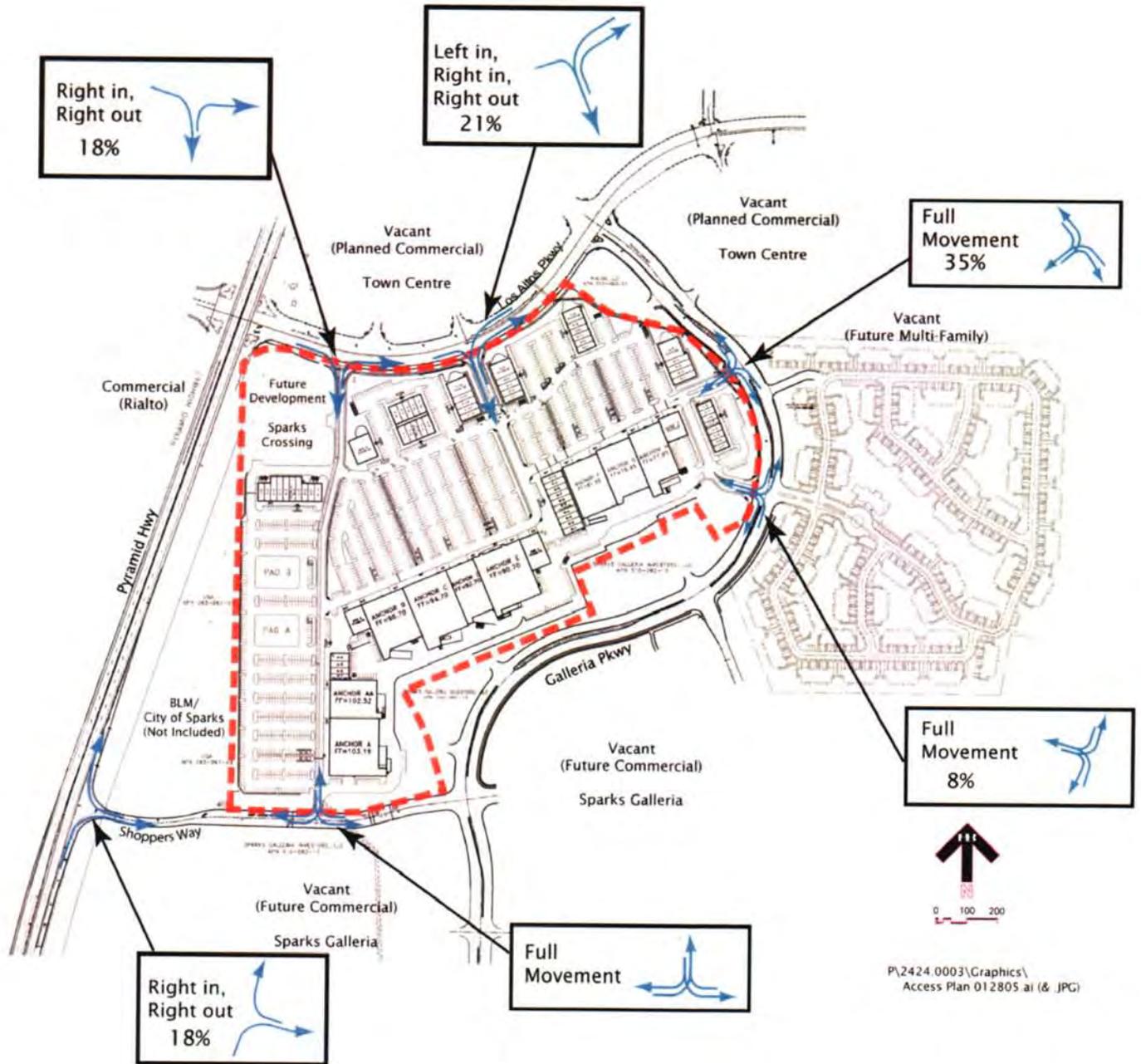


Figure 5-1 - Access Plan

Timing of Major Roadway Improvements

Los Altos Parkway was recently constructed as a full arterial street that will provide access to Sparks Crossing. AIG Baker will be responsible for the improvements to complete the driveways as needed on the south side of Los Altos Parkway that serve direct access to the site. The developer of Sparks Crossing will complete the driveway improvements for Los Altos Parkway prior to the issuance of any certificate of occupancy. The left turn lane for the primary driveway on Los Altos will need to be modified with construction of Sparks Crossing. There is no left turn pocket at this time.

Galleria Parkway between Los Altos Parkway and the south property line of the Sparks Crossing site will be constructed in accordance with the construction drawings shown in a Capital Contribution Front Ending Agreement (CCFEA). See Figure 5-2 (following page), Galleria Parkway Improvements. The CCFEA will be executed with the developer of Sparks Crossing or Galleria Station (apartment developer) abutting to the east. The roadway alignment and driveway spacing and alignments have been coordinated between the two properties. A private agreement between the developers is being created to ensure a cost sharing approach to Galleria Parkway on a fair share basis. The assurance of Galleria Parkway being constructed will occur with the CCFEA regardless of which party develops first. This segment of Galleria Parkway (Los Altos to south property line) will be constructed to RTC and City of Sparks standards prior to issuing a Certificate of Occupancy for either of the projects.

The private road connecting to Pyramid Highway (Shoppers Way) will be constructed by either the developer of Sparks Galleria or the developer of Sparks Crossing, whichever is the lead party. Again, the road is being shared by both projects for access to both sites. Necessary approvals for Shoppers Way have been secured from NDOT to construct the turn-outs and deceleration/acceleration lanes. The City of Sparks will review and approve the full width road between the NDOT right-of-way and the driveway entering into the Sparks Crossing site.

SPARKS CROSSING



Figure 5-2 - Galleria Parkway Improvements

Sanitary Sewer

The sanitary sewer alignment generally collects all wastewater at the rear of the buildings and crosses Galleria Parkway and continues through the apartment site to the east. See Figure 5-3, Sanitary Sewer Alignment (following page). The sewer main from the project will be sized to accommodate flows from the apartment site and Sparks Crossing. The entire route is a gravity sewer alignment. A 20-foot sewer easement will be granted from the abutting apartment developer to the east through that private property. This agreement will provide the legal right for the developer of Sparks Crossing to construct the sewer line through the private apartment property in the event that Sparks Crossing develops prior to the apartment site. The main line will connect directly to the interceptor located near the North Truckee Drain. Sparks staff has assured the applicant that adequate capacity exists in the interceptor to accommodate all sewer generated from land uses anticipated on the vacant property in the area.

An Army Corp crossing permit may be required to cross under the North Truckee Drain in order to tie into the sewer interceptor. The North Truckee Drain is a Water of the U.S and Army Corp regulated water. That permit will be processed by a botanical consultant. Preliminary word from the Army Corp Reno field office did not show any major concerns with the viability of approving this permit.

The Sparks Effluent project may make reclaimed water available to the site. If so, Sparks Crossing may use the reclaimed water at the appropriate time. There is discussion that the effluent line will be along Galleria Parkway.

SPARKS CROSSING



Figure 5-3 - Sanitary Sewer Alignment

Storm Water Management

A Master Hydrology Study was performed by MacKay & Soms Engineers that includes the Sparks Crossing site. That study was required by Sparks Engineering staff to evaluate the gross impacts of flow increases created by the various development proposals in the area. That study determined that detention is not necessary for the Sparks Crossing site because the capacity in the North Truckee Drain can accommodate the increase in flows from the developed condition of the Sparks Crossing project. However, the City of Sparks design criteria requires that each development project will mitigate its increase in flows for the 5 year and 100 year events. Therefore, Sparks Crossing has secured an agreement with the adjacent downstream property owner to the east to route peak flows through their site, eliminating the need for onsite detention.

The storm drain system will be routed from the east end of the Sparks Crossing site, through the apartment site to the east, en route to the North Truckee drain. A 15-foot wide storm drain easement will be granted from the developer of apartment site to the developer of Sparks Crossing to construct a public drainage system through that property. This agreement will provide the legal right for the developer of Sparks Crossing to construct the storm drain system through the apartment private property in the event that Sparks Crossing develops prior to the apartment site. The storm drain system will connect directly to the North Truckee Drain.

The open channel will be sized to include storm water from the apartment site and the Sparks Crossing project. See Figure 5-4 (following page), Storm Drain Alignment and Easement.

SPARKS CROSSING

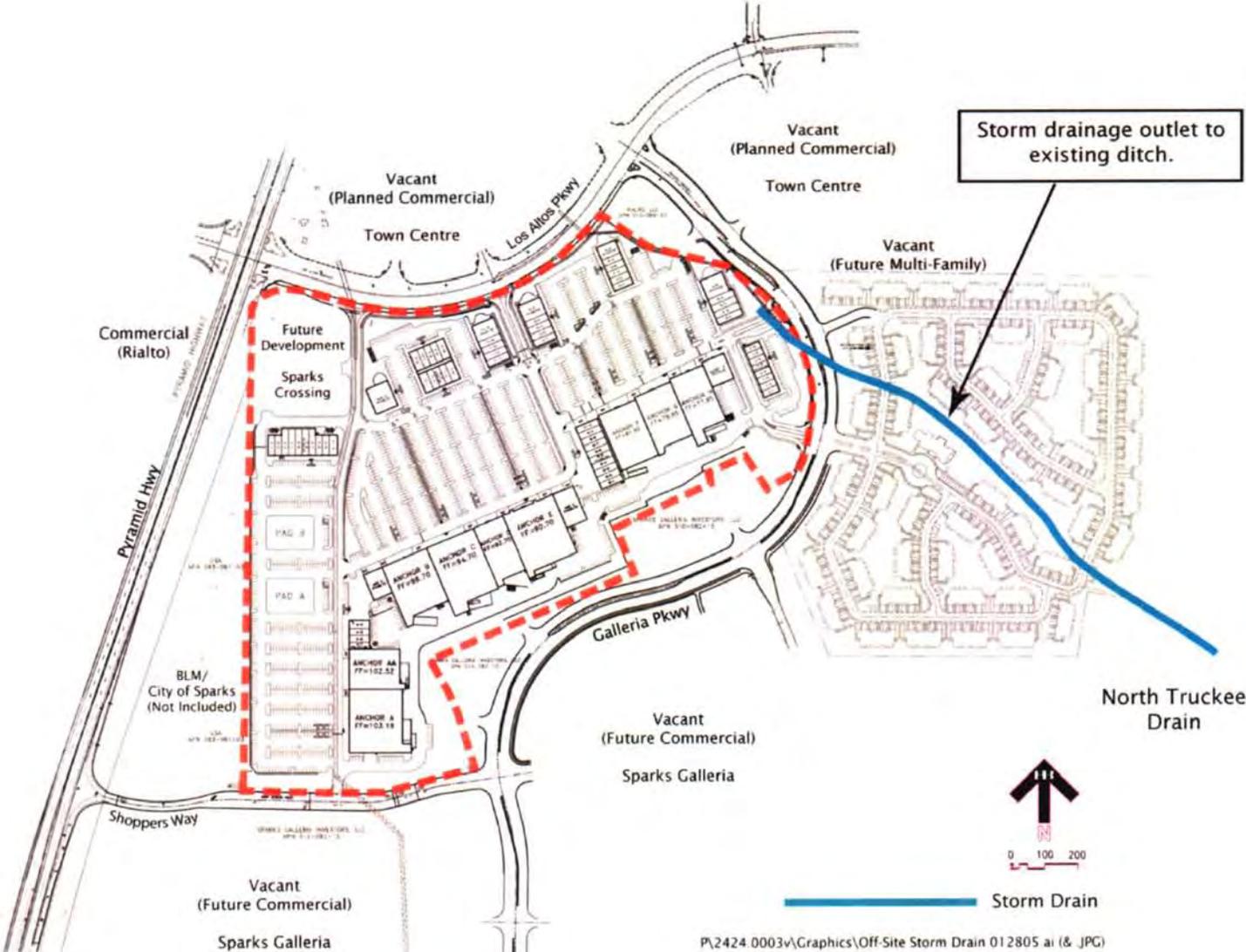


Figure 5-4 – Off-Site Storm Drain Alignment

Public Water System

Truckee Meadows Water Authority is the water purveyor for the site. The Sparks Crossing site plan has been provided to TMWA for analysis in their water modeling system. There is a major transmission line in Los Altos Boulevard that will serve as the primary source of water to the site. Also, there will be a water transmission line constructed in Galleria Parkway that Sparks Crossing will connect to. This will provide the typical looping required by TMWA and the Health Department standards. TMWA has indicated that there are sufficient water facilities in the area to serve the site and has issued a project will-serve letter.

Utilities

Gas and Electric service will be provided by Sierra Pacific Power Company (SPPCo). Both of these utilities are located in Los Altos Parkway and Galleria Parkway as typical with public street right of way design. SPPCo will evaluate specific service to the site with final design plans for Sparks Crossing. The site is located in Sierra Pacific's service area.

Cable television service will be provided by Charter Communications. SBC Nevada Bell is the telephone service purveyor.

Orr Ditch Relocation

Orr Ditch is being contained in a box culvert with construction of the project. The design includes a minor adjustment in the ditch alignment because the ditch is already placed in a culvert for both projects to the north and south. Sparks Crossing completes the final open channel link of the Orr Ditch in this area. The design team has proposed this design which includes the box culvert and obtained approval from the Orr Ditch Company officials.

Chapter 6 – Planned Development Policy Analysis

The City of Sparks has adopted policies/findings related to the development and approval of Planned Developments/New Urban Districts within the City. These policies are outlined below and are addressed in **bold face** type.

Planned Development Findings:

PD1 The plan is/is not consistent with the objective of furthering the public health, safety, morals and general welfare by providing/not providing for housing of all types and designs.

Not applicable. Sparks Crossing does not have a residential component. Instead, the project will provide much needed commercial use in support of the rapid residential growth within the Spanish Springs valley.

PD2 The plan is/is not consistent with the objective of furthering the public health, safety, morals and general welfare by providing/not providing for necessary commercial and industrial facilities conveniently located to the housing.

Sparks Crossing is in direct compliance with this finding. There is currently a lack of commercial use within northern Sparks, resulting in "leakage" of consumer spending into Reno. This project will serve to address the needs of area residents, as well as provide positive fiscal benefits to the City of Sparks.

PD3 The plan is/is not consistent with the objective of furthering the public health, safety, morals and general welfare by providing/not providing for the more efficient use of land and public or private services.

The Sparks Crossing makes efficient use of land by providing needed commercial use that is designed to properly relate to the project site, as well as adjacent uses (both existing and planned).

PD4 The plan is/is not consistent with the objective of furthering the public health, safety, morals and general welfare by providing/not providing for changes in technology of land development so that resulting economies may be available to those in need of homes.

Not applicable.

PD5 The plan is/is not consistent with the objective of furthering the public health, safety, morals and general welfare by providing/not providing for flexibility of substantive regulations over land development so that proposals for land development are disposed of without undue delay.

The Development Standards outlined in the previous chapters serve to implement this finding. The handbook developed for Sparks Crossing allows for flexibility in terms of tenant mix and final project design, while providing the City of Sparks with firm commitments related to architecture, landscaping, and overall project quality.

PD6 The plan does/does not depart from zoning and subdivision regulations otherwise applicable to the property, and these departures are/are not in the public interest for density.

Although this finding relates more to residential use, it is important to note that the Development Standards included with this handbook are much more comprehensive than the requirements of the Sparks Municipal Code. Departures from code requirements are generally more stringent, including strict architectural and sign standards, increased landscaping and streetscape requirements, etc.

PD7 The plan does/does not depart from zoning and subdivision regulations otherwise applicable to the property, and these departures are/are not in the public interest for bulk.

Sparks Crossing has been designed to properly relates to both the built and planned environment, and will complement adjacent properties.

PD8 The plan does/does not depart from zoning and subdivision regulations otherwise applicable to the property, and these departures are/are not in the public interest for use.

The Spanish Springs valley/northern Sparks currently has an imbalance in terms of land use. Sparks Crossing will provide needed commercial services for area residents, revenue for the City of Sparks, and employment opportunities in northern Sparks, a long term goal of the City and Master Plan.

PD9 The ratio of residential to nonresidential use in the planned development is:

The project is 100% nonresidential.

PD10 Common open space in the planned development exists for what purpose, is located where within the project, and comprises how many acres (or what percentage of the development site taken as a whole).

Common space within the project will include pedestrian plaza areas, landscaped entryways, buffer areas, etc. as depicted on the preliminary landscape plan. This area comprises 20% of the total site area.

PD11 The plan does/does not provide for the maintenance and conservation of the common open space by what method.

Common areas within Sparks Crossing will be maintained by the Master Developer, AIG/Baker Sparks Crossing, LLC.

PD12 Given the plan's proposed density and type of residential development, the amount and/or purpose of the common open space is determined to be adequate /inadequate.

Not applicable.

PD13 The plan does/does not provide for public services. If the plan provides for public services, then these provisions are/are not adequate.

The project plan has been designed to adequately provide for and accommodate public services.

PD14 The plan does/does not provide control over vehicular traffic.

Sparks Crossing is designed to adequately accommodate traffic flows to and from the site, as well as within the project boundaries. Driveway spacing along Los Altos Parkway and Galleria Parkway meets or exceeds City of Sparks/RTC standards, ensuring safe vehicle movements. In addition, a detailed traffic impact analysis is included in the appendix of this handbook.

PD15 The plan does/does not provide for the furtherance of access to light, air, recreation and visual enjoyment.

The project will not create negative impacts in terms of access to light, air, recreation, or visual enjoyment. The project is designed to properly relate to the surrounding environment.

PD16 The relationship of the proposed planned development to the neighborhood in which it is proposed to be established is/is not beneficial.

The project is located in the middle of a planned commercial center. Surrounding properties are approved and planned for commercial uses of similar intensities. One exception to this is the proposed apartment complex to the east. However, Sparks Crossing is designed to properly relate with the multi-family use in terms of driveway alignment, buffering, etc.

PD17 To the extent the plan proposes development over a number of years, the terms and conditions intended to protect the interests of the public, residents and owners of the planned development in the integrity of the plan are/are not sufficient.

The Development Standards included within this handbook provide long-term assurances as to how the project will develop. By incorporating the stringent design standards, the integrity of the plan will be maintained.

PD18 The project, as submitted and conditioned, is consistent with the City of Sparks Master Plan.

The project is in direct compliance with the Mixed Use Master Plan designation which allows for the type and intensity of development being proposed.

PD19 The project is consistent with the surrounding existing land uses.

Sparks Crossing directly complements all surrounding approved and planned uses which include commercial centers at similar intensities. Residents of future multi-family units (planned nearby) will benefit by being within walking distance to commercial services, promoting walkable neighborhoods. The project does not directly abut any existing or planned residential uses.

PD20 Public notice was given and a public hearing held per the requirements of the Sparks Municipal Code.

The Sparks City Council will adopt this handbook, per the requirements outlined in the Sparks Municipal Code and Nevada Revised Statutes.

APPENDICES

EXHIBIT "K"

EXTERIOR PALETTE

CMU & EIFS



Frazee #8875D
Pigeon Isle



Frazee #8365D
Colonial Brown



Frazee #7805A
Copper Gold



Frazee #8241W
Tenderfoot



Frazee #8395D
Monterey Rose



Frazee #8285A
Monk's Hood



Frazee #7785D
Heathcote



Frazee #8240W
Windrift



Frazee #8356N
Mackay Cedar



Frazee #8244D
Copper Springs



Frazee #7815A
Paprika



Frazee #8222W
Desert Fawn



Frazee #8386N
Winestain



Frazee #8245A
Ochre Rust



Frazee #8255D
Jamocha



Frazee #8763M
Pewter Works



Frazee #8376N
Deep Henna



Frazee #8355D
Burnt Copper



Frazee #7804D
Clay Um



Frazee #8262W
Peanut Shell



Frazee #8835D
Pawnee



Frazee #7774D
Desert Vista



Frazee #7784M
Nullabor Sand

STOREFRONT



Kawneer
Clear Anodized



Kawneer
Dark Bronze

SPECIALTY < 25% of wall area



Frazee #ACN076N
Harbour Blue

ROOFING



Pac-Clad
Musket Grey

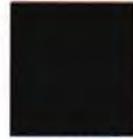


Pac-Clad
Terra Cotta

METAL PANEL / CANOPIES / WALL FINIS



Centria
Platinum 9989



Duranar UC43347
Statuary Bronze



Frazee #8835D
Pawnee



Frazee #8376N
Deep Henna

TILE



Innovative Tile &
Marble
Anchorage



Innovative Tile &
Marble
Clanfield

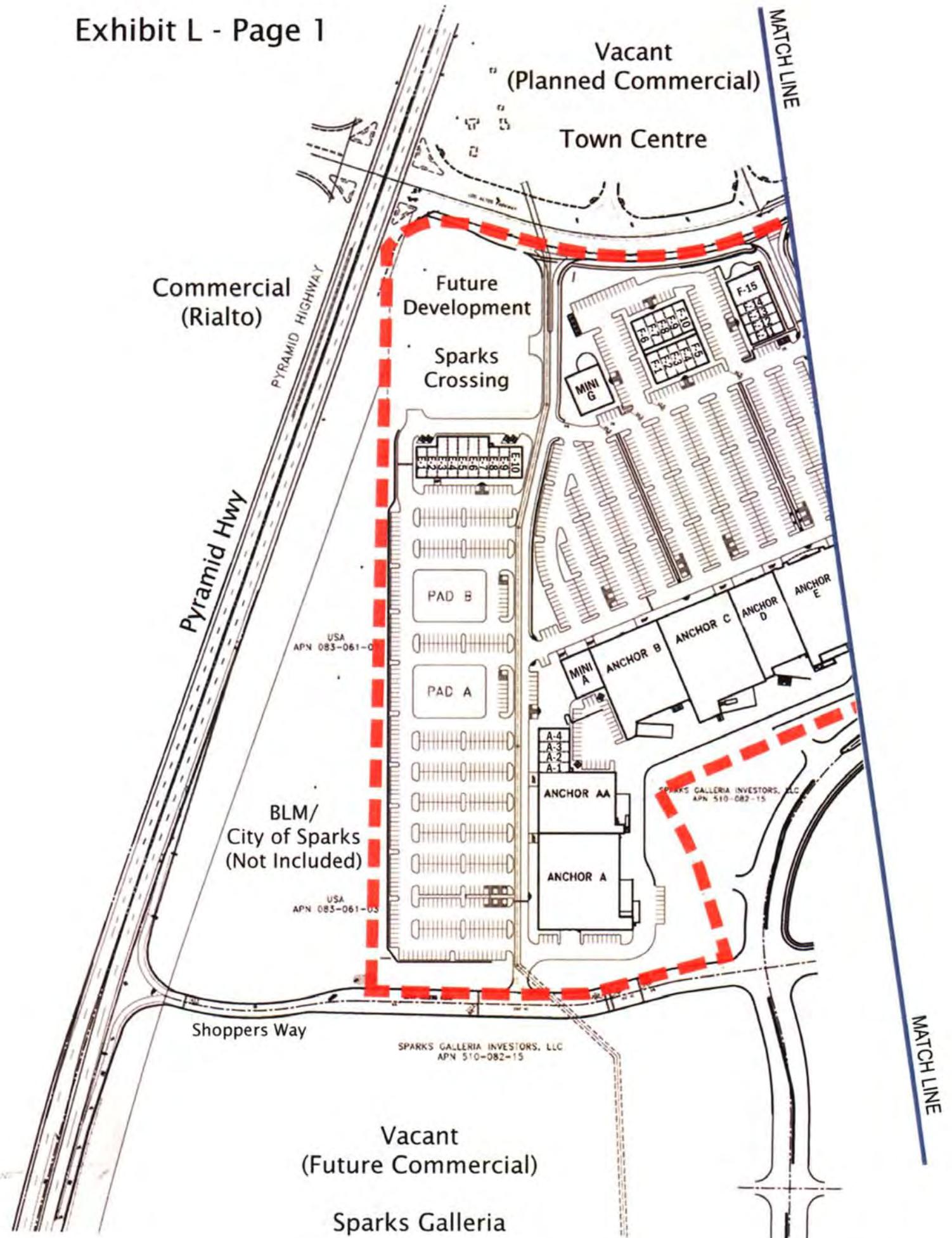


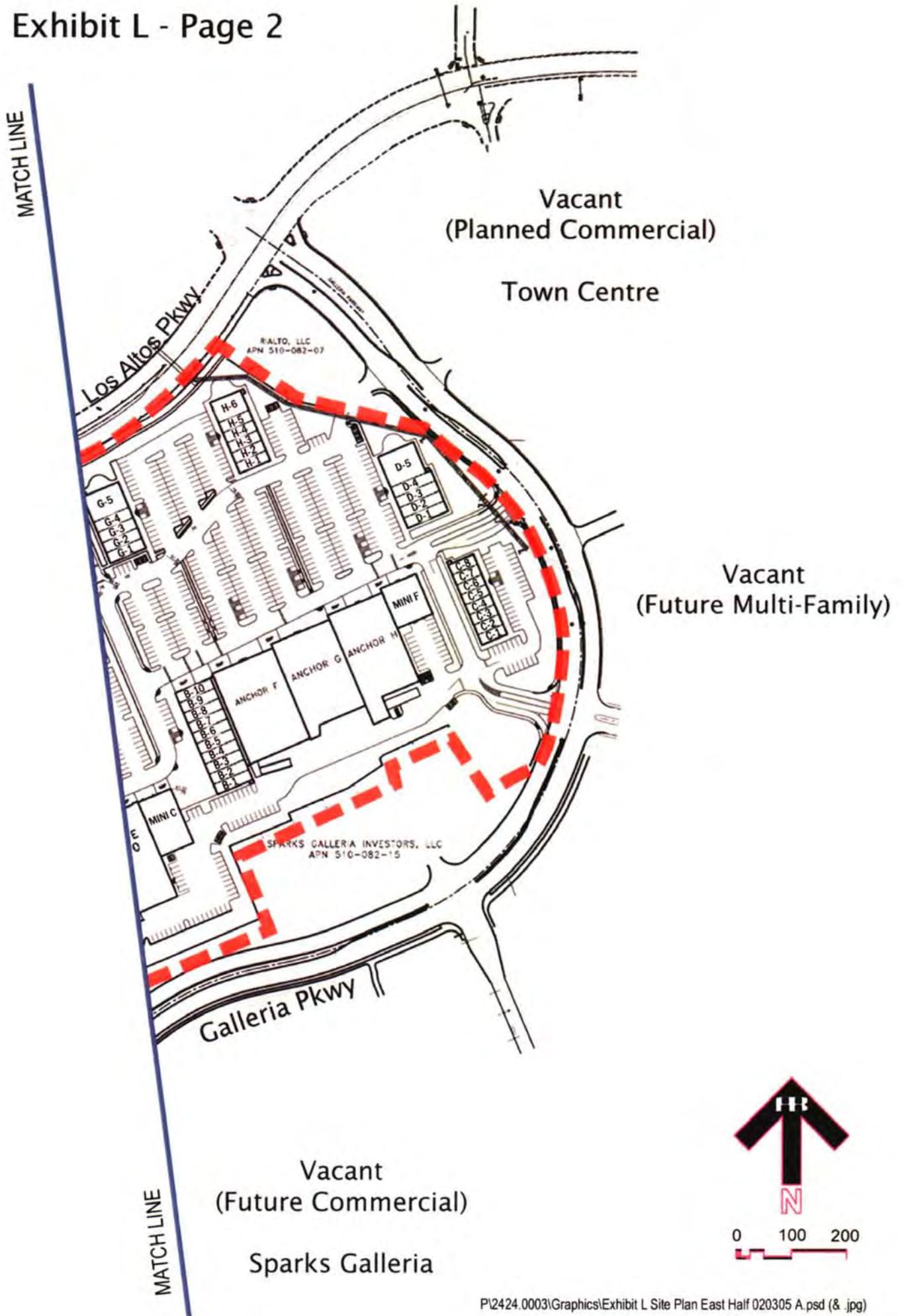
Crossville Ceramics
A890P Ebony Porcelain / Polished

STONE VENEER



Cultured Stone
Caramel Country Ledgestone





Appendices – Sign Guidelines

Master Developer Sign Design and Construction:

1. All fabrication and installation shall comply with Underwriters Laboratories requirements and specific state and local codes. All components shall bear the U.L. label indicating approval and be manufactured and installed by a U.L. certified facility.
2. All electrical connections, conduits, conductors, transformers, ballasts and other equipment shall be concealed.
3. Primary electrical service to all tenant signs shall be by tenant or occupant. Tenant shall provide time clocks and photo cells for control of tenant signage. Signage shall remain on until tenant closes for the evening, or a time specified by the Master Developer, whichever is later.
4. The illumination of all sign components shall be uniform in intensity over all of the illuminated surfaces. No noticeable hot spots or shadows will be allowed.
5. All signage shall use specified colors, except for tenants 4,000 SF and larger or Pad occupants who can demonstrate that proposed exception is consistent with tenant's National Sign Program. Exceptions will require approval stamp of the Master Developer on permit documents to be submitted to the City of Sparks by tenant for permitting.
6. All glass housings, electrodes, tube supports, glass tubing, high voltage cable and all miscellaneous components shall be of first quality industry standards and listed with the Underwriter's Laboratories. Exposed labels, other than U.L. labels if required by local officials, are prohibited.
7. Nothing herein shall apply to signs located on the interior of the premises, except window signage visible from outside the premises.

Master Developer Sign Construction:

1. Signs shall present a clean, streamlined and finished appearance. Fasteners shall be concealed from view to the extent possible. Any required bolts, fastenings, clips, conduits, etc. shall be non-corrosive stainless steel, chromium plated or hot-dipped galvanized iron or equal. Exposed conduits and wire ways are not allowed. Where externally mounted, all items shall be painted to match the exterior color of the surfaces mounted thereon.
2. All manufacturers/installers are advised that prior to final acceptance, authorized representative of the Master Developer may inspect each unit for conformance. Any signs, found not to be in conformance will be rejected and removed immediately or modified as needed by the tenant or tenant's contractor.

3. Except for tenants or pads over 4000 square feet in area, all signs shall be installed on raceways of such size as approved in writing by Landlord. All raceways shall be painted with a flat finish to match the color of the fascia.
4. Except for Tenants or pad users over 4000 SF, all signs shall consist of internally illuminated channel letters with Plexiglas faces, illuminated with neon or LED lamps, color to match sign faces. Trim caps and side returns shall match sign faces, unless otherwise noted. Tenants or pad users over 4000 SF may use their national sign program.
5. With the exception of tenants approved to use their national sign standard, color of sign faces is limited to one of the following Plexiglas brand colors or approved equivalent:

2293 RED
2447 WHITE
2114 BLUE
6. Yellow sign faces are NOT allowed unless part of the National sign standard for a tenant over 4000 SF in area, or pre approved pre lease by the Master Developer.
7. Signs shall not project more than 15" from the face of the wall upon which it is attached.

Master Developer Installation / Removals:

1. All penetrations of any building structure required for sign installation shall be completely and neatly sealed and continuously maintained in watertight condition. Penetrations of The Developer's roof structure or parapet flashing are prohibited unless otherwise approved in writing by the Master Developer.
2. All contractors for installation, removal or service must be fully licensed and must provide the Master Developer with certificate of insurance prior to commencing any work. Sign company names, labels, or stamps are not allowed, unless required by City of Sparks Municipal code.
3. All methods of attachment to any structure must be detailed on drawings and be approved by the Master Developer and City of Sparks.
4. Sign removals shall include the patching and repairing of entire work area plus the repainting of any "ghosted" areas or as directed by the Master Developer.

WLS

FVB SERIES

VERTICAL LAMP/FLAT GLASS LENS



SPECIFICATIONS

HOUSING All housings are manufactured using technologically advanced computer numerical control (CNC) machinery. Precision sheared and formed from one piece, .080 gauge, corrosion resistant aluminum, with tooled external reveal. The computerized (CNC) machinery enables all housings to be constructed quickly, efficiently, and adhering to exacting ISO 9002 standards. All external hardware is stainless steel.

LENS AND DOOR ASSEMBLY Hinged door assembly is (CNC) precision, sheared and formed from one piece, .080 gauge, corrosion resistant aluminum with captive 1/4 turn stainless steel fasteners. The standard lens is a clear tempered flat glass, secured by aluminum lens retainers and sealed with one piece extruded silicone gasketing to provide complete weather and insect protection.

OPTICAL SYSTEM Multi-faceted, segmented, highly efficient, 95% reflective formed aluminum, vertical burn reflectors, available in five distributions.

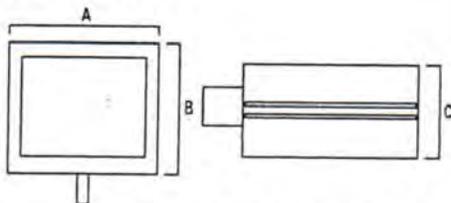
MOUNTING An extruded aluminum, bolt-on-arm, with continuous channels, securing (2) 1/2 inch zinc running bolts with stainless steel hardware is standard. Extruded aluminum arm is premounted to fixtures. Optical slip-fit arm provides ease of installation.

ELECTRICAL ASSEMBLY Ballast is factory wired and tapped to voltage specifications. All ballasts are CWA or HPF regulating auto transformers, available in Metal Halide, Pulse-Start, and High Pressure Sodium. Ballast is capable of providing consistent lamp starting down to -20 degrees Fahrenheit. Ballast is fastened to an easily accessible ballast tray for service convenience. Socket is a glazed porcelain medium or mogul base with nickel plated contacts, rated at 5K and 600V.

OPTIONS Fixtures available with a variety of options, including photo cell, photo receptacle, convex or flat glass lens, light shield, and fusing. Please consult factory for custom options.

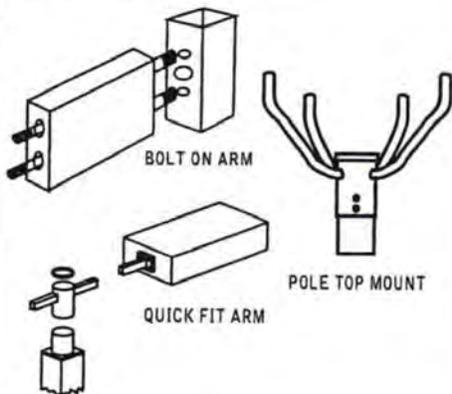


DIMENSIONS



	A	B	C	STRIPE	EPA
FVB-1	14"	14"	10"	2"	0.7
FVB-2	19"	19"	12"	2"	1.3
FVB-3	23"	23"	13"	2"	2.1
FVB-4	23"	23"	15"	2"	2.4

MOUNTING BRACKETS



Approved By: _____

Project Name: _____

Location: _____

Date: _____

WLS LIGHTING SYSTEMS

MANUFACTURING SINCE 1969

P.O. Box 100519 • Fort Worth, TX 76185
800.633.8711 • Fax: 817.735.4824 • www.wslighting.com

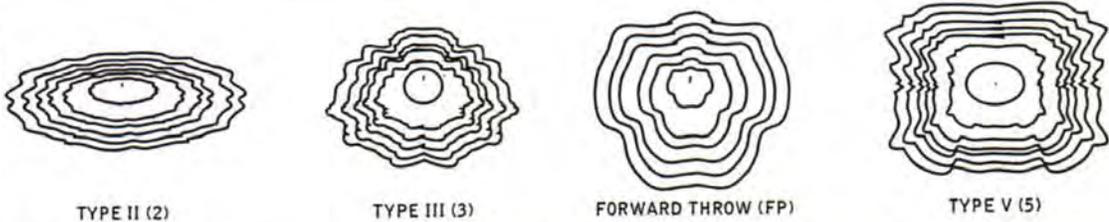
FVB SERIES

VERTICAL LAMP/FLAT GLASS LENS

FIXTURE MOUNTING CONFIGURATIONS



OPTICS



NOTE: Based on 1000 Watt MH vertical burn. Consult factory for accurate optics.

ORDERING INFORMATION							
SELECT APPROPRIATE CHOICE FROM EACH COLUMN TO FORMULATE ORDER CODE. Refer to example below.							
LUMINAIRE PREFIX	DISTRIBUTION	LAMP WATTAGE	LIGHT SOURCE	LENS	LINE VOLTAGE	LUMINAIRE FINISH	OPTIONS
FVB-1	2 - Type II	100W	PS - Pulse-Start	FG - Flat Glass	480V	BRZ - Bronze	GS - Glare Shield
	3 - Type III	150W	HPS - High Pressure Sodium (100, 150W)			BLK - Black	
FVB-2	FP - Perimeter Forward Throw	175W	MH - Metal Halide (175W)	CT - Contoured FG - Flat Glass	MT - Multi Tap ³ TT - Tri Tap ²	WHT - White	SF - Single Fusing
	5 - Type V	200W	PS - Pulse-Start			GRN - Green	DF - Double Fusing
FVB-3		250W	HPS - High Pressure Sodium (200, 250, 400W)	CT - Contoured		GY - Grey	PC - Photo Cell
		320W	MH - Metal Halide (250, 400W)			SM - Silver Metallic	
FVB-4		350W	MH - Metal Halide (250, 400W)	CT - Contoured		CC - Custom Color	
		400W	PS - Pulse-Start			ACCENT STRIPE	
FVB-4		400W	HPS - High Pressure Sodium (400W ¹)	FG - Flat Glass		BLM - Blue Metallic	
		450W	MH - Metal Halide (400, 750, 1000W)			RD - Red	
FVB-4		750W	PS - Pulse-Start	FG - Flat Glass		SM - Silver Metallic	
		1000W*	MH - Metal Halide (1000W)			GM - Gold Metallic	
FVB-4		750W	PS - Pulse-Start	FG - Flat Glass		WHT - White	
		1000W*	MH - Metal Halide (1000W)			BKM - Black Metallic	
FVB-4		750W	PS - Pulse-Start	FG - Flat Glass		CM - Charcoal Metallic	
		1000W*	MH - Metal Halide (1000W)			N - None	

FVB-2 3 400 MH FG MT BLK-N NO
(EXAMPLE ORDER)

ORDER:

WLS

NOTE:

1. Voltage for 750W High Pressure Sodium must be specified.
2. Consult factory for international voltages. (120, 277, 347 Voltage)
3. MT - Multi Tap is shipped standard unless otherwise specified.
(Multi Tap consists of 120V, 208V, 240V, and 277V. Multi Tap is pre-wired for highest voltage. Alternate voltages will require field re-wiring.)

Approved By: _____

Project Name: _____

Location: _____

Date: _____

WLS LIGHTING SYSTEMS
MANUFACTURING SINCE 1969

P.O. Box 100519 • Fort Worth, TX 76185
800.633.8711 • Fax: 817.735.4824 • www.wslighting.com