

APN: (Not required
Per NRS 111.312.1)

When recorded, mail to
City of Sparks
P.O. Box 857
Sparks, Nevada 89432-0857
ATTN: Community Services Department

DOC # 4288544

10/10/2013 01:26:27 PM
Requested By
SPARKS CITY
Washoe County Recorder
Lawrence R. Burtness - Recorder
Fee: \$199.00 RPTT: \$0.00
Page 1 of 158



(Space above for recorder's use only)

NOTICE OF ADOPTION OF DEVELOPMENT PLAN
(NRS Chapter 278A)

Name of Development: D'Andrea
Name of Plan: D'Andrea Development Standards Handbook
Date of Approval: October 15, 2009
City File No: PCN06077

A-4013
10/12/09
A.I. 8.4

NOTICE IS HEREBY GIVEN that on the above indicated date, the City Council of the City of Sparks, Nevada, finally adopted the development plan for the above named development, whose legal description is attached as **Exhibit A**.

Pursuant to NRS 278A.570 (2) after this plan is recorded, all zoning and subdivision regulations applicable to the property described in Exhibit A cease to apply and are replaced with the plan attached as **Exhibit B** hereto.

A history of prior transactions is attached hereto.

Dated this 9th day of October, 2013

8/25/08	Development Standards Handbook Amendment	PCN08039 Replaces Previously approved handbook (PCN07022)	DOC# 4084554 02/14/12
10/15/09	Development Standards Handbook Amendment	PCN06077 Replaces Previously approved handbook (PCN08039)	Recorded Herewith

EXHIBIT "A"
LEGAL DESCRIPTION
D'ANDREA PROJECT

All that property situate within the East Half of Section 35 and the West Half and Northeast Quarter of Section 36, Township 20 North, Range 20 East, MDM and the North Half of the Northeast Quarter of Section 2, Township 19 North, Range 20 East, MDM, City of Sparks, Washoe County, Nevada

EXCEPTING THEREFROM all that property within the Right-of-Way of Vista Boulevard

EXCEPTING THEREFROM all that property within the Right-of-Way of Primio Way as depicted on the Map of Dedication for Pete's Way and Primio Way, Dedication Tract Map 2845, File Number 1563292, Official Records, Washoe County, Nevada.

EXCEPTING THEREFROM all that property within the Southeast Quarter of said Section 35 and the Southwest Quarter of said Section 36 known as Marvin Moss Elementary School more particularly described as follows:

Beginning at the Southerly Section Corner common to said Sections 35 and 36;

thence with the South line of Section 35 North $89^{\circ}17'59''$ West a distance of 370.05 feet to a point on the Easterly Right-of-Way of Primio Drive;

thence departing said Section Line from a tangent which bears North $08^{\circ}33'35''$ East, along said Right-of-Way with a circular curve to the left with a radius of 327.50 feet and a central angle of $29^{\circ}25'28''$ an arc length of 168.19 feet;

thence North $20^{\circ}51'53''$ West a distance of 763.27 feet to a point on the boundary of Common Area of the D'Andrea Community;

thence with said boundary South $89^{\circ}17'59''$ East a distance of 670.84 feet to the common line between Sections 35 and 36 and also being a point on the exterior boundary of Ticino @ D'Andrea Subdivision;

thence departing said Common Area along said Section line and Ticino @ D'Andrea boundary South 00°43'33" West a distance of 835.00 feet;

thence departing said Section line South 89°09'02" East a distance of 336.60 feet;

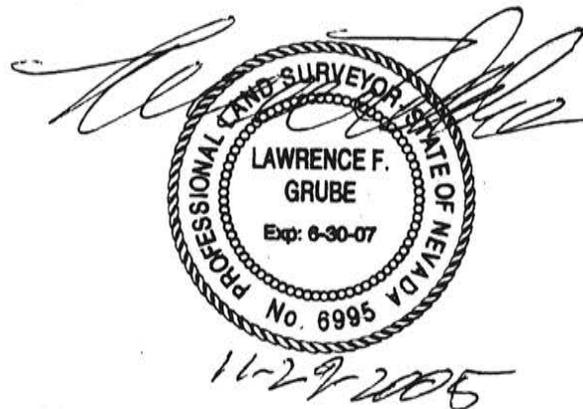
thence South 00°43'16" West a distance of 40.00 feet to a point on the South line of Section 36;

thence departing the boundary of Ticino @ D'Andrea along said Section line North 89°09'02" West a distance of 336.60 feet to the Point of Beginning.

Basis of Bearings: Nevada State Plane Coordinate System, West Zone, North American Datum 1983/94 per Record of Survey 3885.

Legal Description Prepared By:

Lawrence F. Grube
Nevada Professional Land Surveyor 6995
Summit Engineering Corp
5405 Mae Anne Avenue
Reno, Nevada 89523





DEVELOPMENT STANDARDS HANDBOOK

CREATED FOR:

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290 Gentry Way
Suite 5
Reno, NV 89502

Created By:

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Final Handbook Approval: August 25, 2008

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CHAPTER 1

CHAPTER 1 - INTRODUCTION

Statement of Plan and Purpose

D'Andrea is planned to be a quality mixed use community. Land uses include a mix of single family residential with a range of lot sizes (some of which may be active adult housing), multi-family housing, office, retail commercial, a golf course, parks and open space/trails. The handbook establishes the theme and project design standards while ensuring high quality development in D'Andrea.

Project Location

D'Andrea encompasses four parcels (APN# 030-022-01, -03, -08, 030-181-13, 402-100-08, 09, #10), and is .861 +/- acres in area. The master-planned community is located on the east side of Vista Boulevard between Prater Way and Baring Boulevard, stretching from the meadows to the foothills. The project site fronts on Vista Boulevard. Baring Boulevard and Whitewood Drive are currently "T" intersections with Vista Boulevard. These "T" intersections will become 4-way intersections with the development of D'Andrea. Adjacent land uses include an approved project, Desert Highlands to the north; vacant/BLM lands to the east; residential, Marvin Moss Elementary School and vacant land to the south; and residential, Mendive Middle School, and commercial uses to the west. Refer to Figure 1-1 Location Map.

Project Goals

- Goal 1: Establish a community with its own "sense of place" and identity that blends with the adjacent neighborhoods and environment.
- Goal 2: To cluster land uses to protect environmentally sensitive areas, and add community interest and function.
- Goal 3: To provide a variety of housing opportunities with diverse housing types and prices.
- Goal 4: To develop an infrastructure system that efficiently and effectively meets community needs.
- Goal 5: Ensure a high standard for development in D'Andrea and to establish parameters for design that encourage unique design solutions.

Requests Description

On January 26, 1998, D'Andrea received approval of the Preliminary Handbook, Annexation request, and Master Plan Amendment by the Sparks City Council. This approval was for the conceptual plan for the D'Andrea master-planned community. With the final planning, minor modifications to the master plan designations have been made due to the final routing of the golf course, demand for commercial/office uses, and the related changes in the collector roadway system.

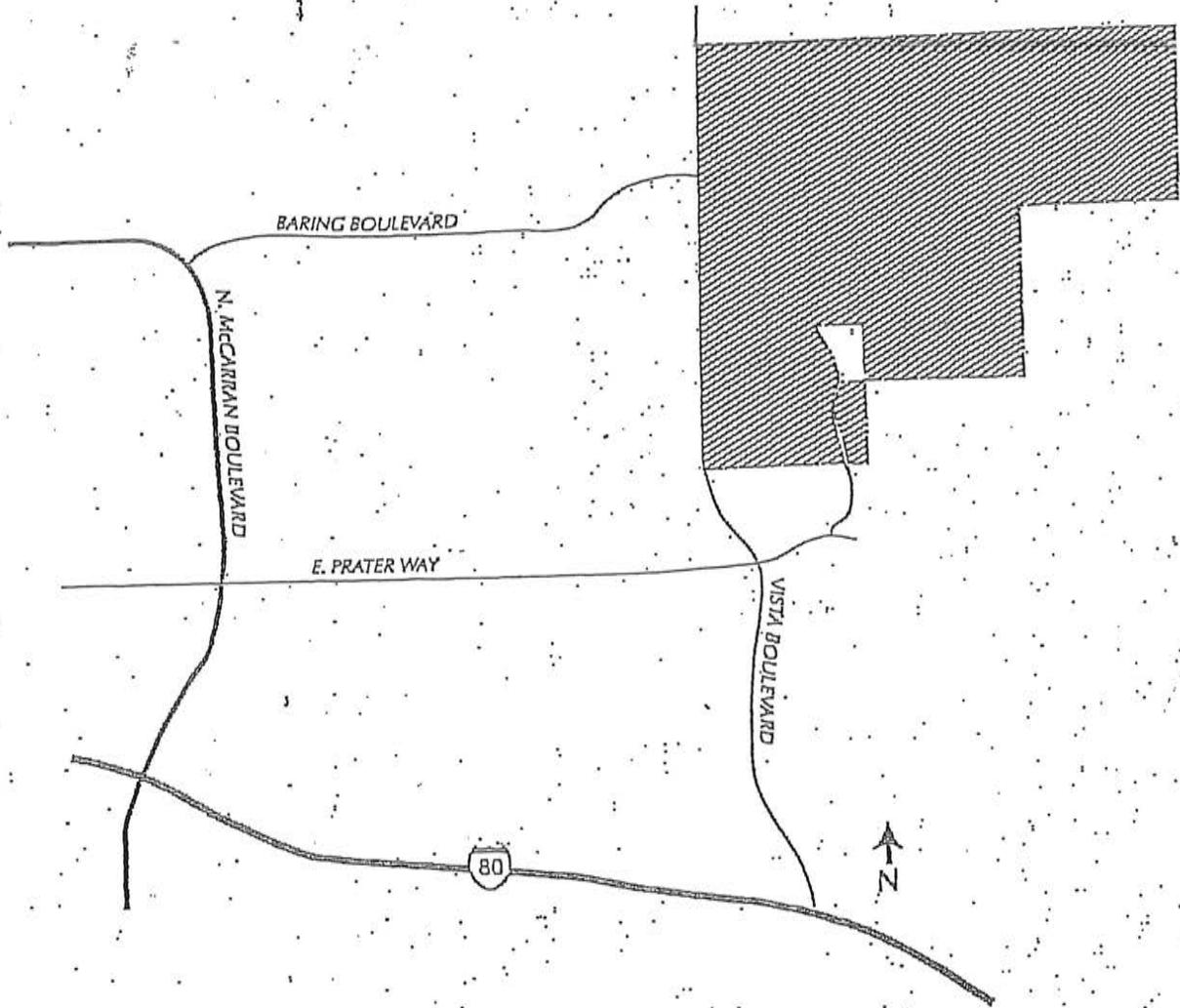


Figure I-1 Location Map

Project Concept

D'Andrea will be a unique master-planned golf course community in the City of Sparks. There will be a mixture of residential, office, retail and recreational uses. There will be two major focal points: a village center and a daily fee-public, championship golf course. The residential uses will consist of both pre-designed and custom lot neighborhoods. Another potential significant component is an active adult project with its own community recreation center. Refer to Table 1-1 Project Summary for a breakdown of land uses, acres and number of units/lots. Refer to Figure 1-2 Land Use Plan.

D'Andrea is envisioned as a family-oriented, master-planned community that combines traditional neighborhoods with active and passive recreation while remaining sensitive to the natural assets of the property. The recreational theme includes a public, championship golf course along with natural open space, trails and paths connecting the villages, parks, school(s) and village center. D'Andrea provides a wide range of housing types from apartments, cluster housing to single family homes. With the final grading and lotting there have been some changes in the acres for land uses, particularly office/commercial and apartments (MDR) from Preliminary Design Handbook approved January, 1998. With tentative and final maps, there will be more refinement in the acreages and units in villages. Therefore, it assumed there will be adjustments as the project develops. However, the overall yield and intensity shall not exceed the total number of lots/units indicated in Table 1-1. If for any reason the total number of lots/units requested by the original Planned Development Handbook (2,230 units) can not be attained with the original village configuration, any remaining residential density and or acreage may be transferred to other areas of the planned development deemed suitable for development by the Administrator, this includes the creation of new villages where appropriate. Refer to Table 1-3 General Land Use for a summary of land uses.

Table 1 - 1 Project Summary

Land Use	Acres	No. of Units/Lots	Units/Acre
Residential	441.38	2,227	5.04
Office / Commercial	21.8	--	--
Golf Course/ Recreation Center	179.19	--	--
Parks	7.66	--	--
Open Space / Streetscape	203.43	--	--
Road Right - of - Way/ Pump Stations	7.75	--	--
Total	861.21	2,227	2.58

Table 1-2 General Land Use

	Acres	Percentage
Residential	441.38	51.25%
Commercial/Office	21.8	2.5%
Golf Course/ Recreation Center	179.19	20.80%
Open Space/Streetscape/Parks	211.09	24.51%
Road Right-of-Way/ Pump Stations	7.75	.89%
Total	861.21	100%

¹ See Page 3-31 for percentage of golf course credited to public open space.

Regional Plan Amendment

The eastern 218± acres are designated on the Truckee Meadows Regional Plan as Rural Reserve. On this portion of the property, the applicant is requesting a Regional Plan Amendment to redesignate it to Rural and to remove the Joint Planning Area designation. The Regional Plan designation for Rural Reserve was based on general slope topography. With more detailed topography analysis, the steep slopes can be accurately defined and remain as open space. This request for the Regional Plan Amendment will be consistent with the intent of the Regional Plan by maintaining the steep slopes as open space and allowing large lot residential on the moderate slopes. The Regional Plan defines the gross density of Rural as one unit per acre. There are 218 units proposed in the Rural designated area which is a gross density of 1 unit per acre. The entire property is within the Sphere of Influence for the City of Sparks.

Table 1-3 Truckee Meadows Plan

Land Use Designation	Existing Plan (Acres)	Proposed Plan (Acres)
Rural Reserve	218	0
Rural	0	218
Urban	643	643
Total	861	861

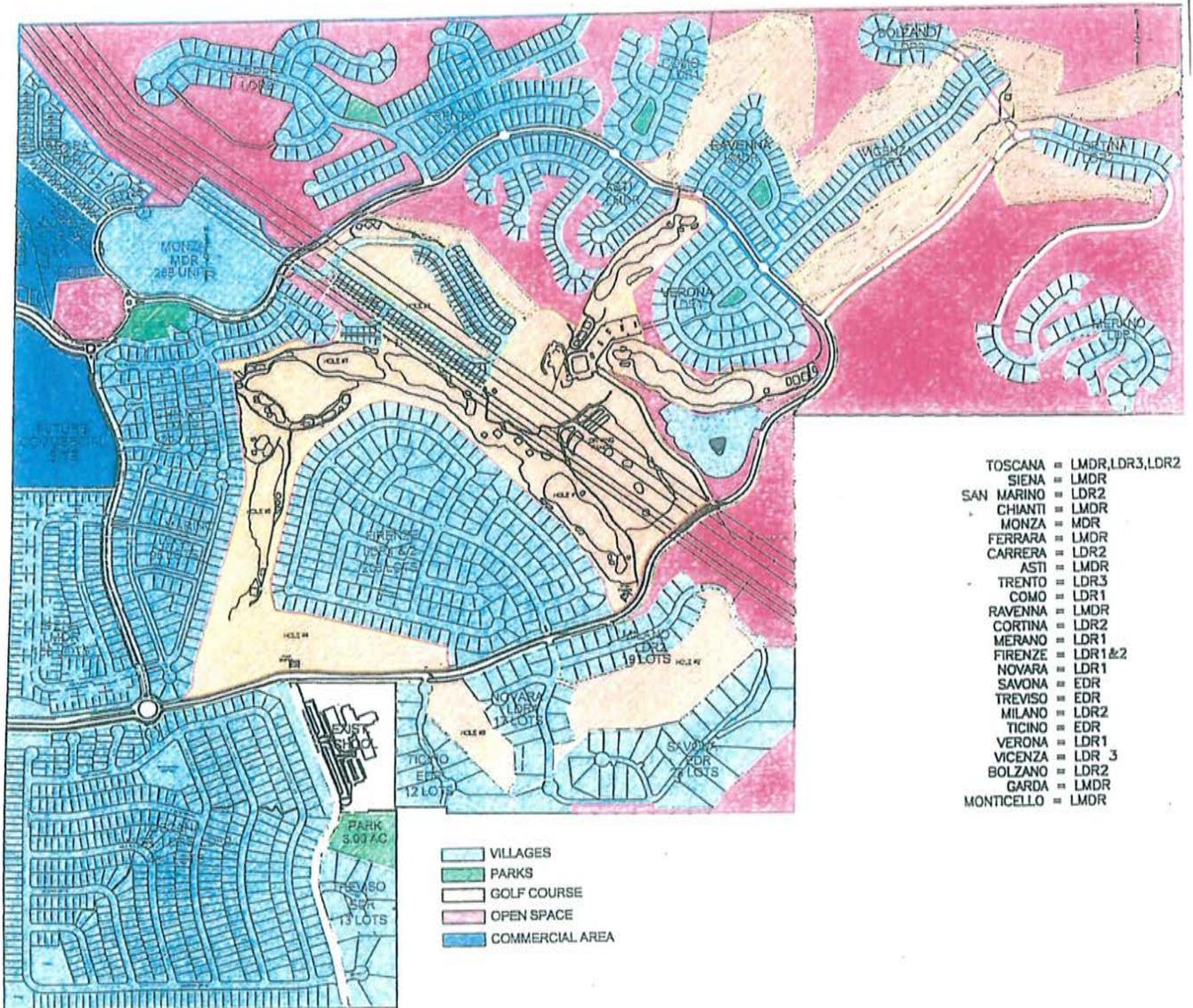
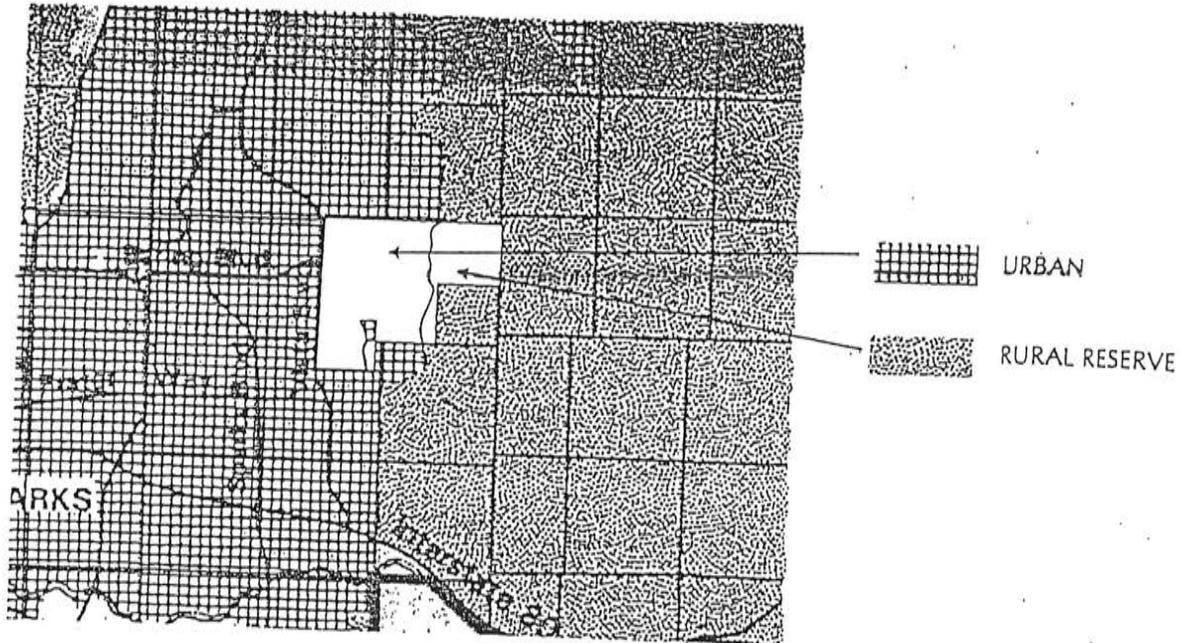
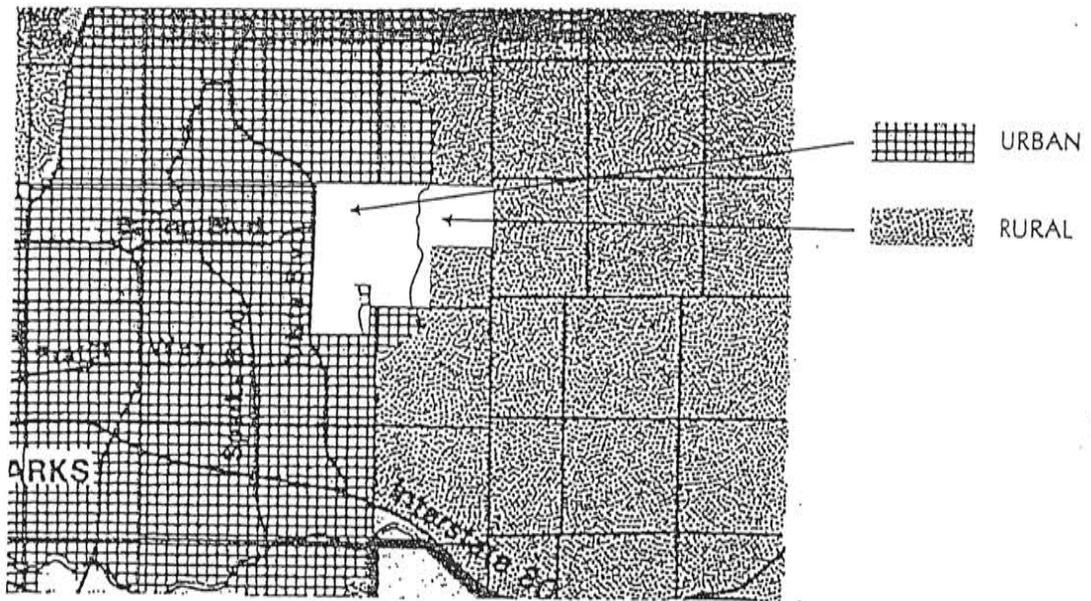


Figure 1- 2 Land Use Plan



EXISTING REGIONAL LAND USE DIAGRAM



PROPOSED REGIONAL LAND USE DIAGRAM

Figure 1 - 3 Regional Plan Amendment

Villages

Using the northern Italian theme, D'Andrea is designed to consist of 23 housing projects or products with the names of villages located in northern Italy. Within the villages, there is open space which is included in the acreage for the villages but is considered part of the D'Andrea open space. The following narrative describes each village and its characteristics. Refer to Figure 1-3 Village for location and names. The following table summarizes the lot sizes and number of homes in each Village. Refer to Table 1-4 Housing Mix.

Table 1-4 Housing Mix

Village	Acreage	Density	Housing Type	Total Dwelling Units	Lot Size per Village in S.F.
*Toscana	92.27	5.0	Veranda, Villaggio, and Vicinato	511.00	4,500; 5,200 & 6,200
*Siena	24.00	7.00	Veranda	169.00	4,000
*San Marino	22.30	4.50	Vicinato	100.00	6,000
*Chianti	29.20	4.30	Veranda	127.00	4,500
*Monza	17.50	17.70	Mezzo	310.00	NA
*Ferrara	10.73	4.70	Veranda	50.00	4,000
Carrera	19.31	3.52	Vicinato	68.00	7,000
Asti	16.38	5.25	Veranda	86.00	3,500
Trento	19.15	4.49	Villaggio	86.00	5,000
Como	10.60	2.83	Villa	30.00	8,000
Ravenna	10.85	4.79	Veranda	52.00	3,500
Bolzano	3.67	3.81	Vicinato	14.00	7,000
Cortina	5.74	3.83	Vicinato	22.00	7,000
Vicenza	13.07	5.04	Villaggio	66.00	5,000
Merano	14.46	2.97	Villa	43.00	8,000

					D'ANDREA
Verona	17.36	3.11	Villa	54.00	8,000
Garda	4.98	5.02	Veranda	25.00	3,500
*Firenze	58.50	4.40	Vicinato & Villa	257.00	Villa = 7350 Vicinato = 6,825
*Novara	5.80	5.30	Villa	17.00	7,000
Milano	9.80	1.94	Vicinato	19.00	8,050
*Savona	25.00	1.08	Bella Vista	27.00	1/3 ac - 1ac
*Ticino	9.30	1.30	Bella Vista	12.00	10,000 - 1ac
*Treviso	9.50	1.30	Bella Vista	13.00	10,000 - 1ac
**Monticello	12.25	5.87	Veranda	72.00	4,700
Total	±441.38			2,227	

* These numbers represent the original handbook totals, actual acreages and unit numbers may vary.
 ** In accordance with the Project Concept section, the village of Monticello was created by transferring existing residential density and acreage to a previously disturbed area deemed appropriate for development by the Administrator.

Toscana

This product is planned to be active adult community (with at least one resident of 55 years of age in compliance with Federal Standards) with its own recreation facility. It will be a gated community with private streets with entrances off South D Andrea Parkway at the roundabout and Primio Way. It will consist of five hundred eleven (511) lots on 92 acres. There will be 3 types of housing; Veranda, Villaggio and Vicinato. Toscana contains a variety of lot sizes and housing products including 45' x 100', 52' x 100' lots and 62' x 100' lots. There will be a minimum 30 foot wide buffer along the south boundary which will be landscaped to buffer this village from the neighboring houses to the south.

Siena

Siena is Veranda style housing. It will consist of one hundred sixty-nine (169) approximately 4,000 square foot lots on 24 acres. This village takes access from San Marino Drive. Located at the southern entrance of the project, this village will be located adjacent to the commercial/office center. The project will contain its own internalized open space. The lots will be situated on private driveways and will be designed for singles, empty-nesters, and first-time home buyers.

San Marino

San Marino is Vicinato style housing with one hundred (100) 6,000 square foot lots on 22.3 acres. It will have access off San Marino Drive. A portion of the lots will be fronting on the 5th fairway.

Chianti

Chianti is a Veranda housing type that fronts on the 5th green, the 6th and 7th fairways of the golf course. This village will contain 27 lots on 29.2 acres with a minimum lot size of 4,500 square feet with access from San Marino Drive and North D Andrea Parkway. Chianti is adjacent to the San Marino Park to the west. The trail network traverses this village to San Marino Park.

Monza

Monza is the only Mezzo housing type planned for D'Andrea. The site takes access from North D Andrea Parkway and Ferrara Drive. The property is approximately 17 acres and it is anticipated that there will be 310 apartment units. Monza is located northeast of the rock outcropping and adjacent commercial /office uses.

Ferrara

Ferrara is Veranda housing style. This site will contain cluster type housing or duplexes. It is estimated to be 50 lots on 10.7 acres. The minimum lot size will be 4,000 square feet. The site is located at the northwest boundary adjacent to Vista Boulevard, the utility easement, Monza and the commercial /office uses on Ferrara Drive.

Carrera

The village of Carrera consists of 68 single-family lots on 19.31 acres. The housing type located in Carrera is the Vicinato with a minimum lot size of 7,000 square feet. The site has spectacular views of the Sierra Nevada, Truckee Meadows and D'Andrea Golf Course. Access to Carrera is from North D'Andrea Parkway.

Asti

The village of Asti is located between the 9th and 10th holes of D'Andrea Golf Course. Asti consists of 86 single-family lots on 16.38 acres. The Veranda housing type is located in Asti with an average lot size of 3,500 square feet.

Trento

The village of Trento consists of 86 single-family lots on 19.15 acres with the Villaggio housing type. Like Carrera, Trento is located on the northern boundary and accessed by North D'Andrea Parkway.

Como

The village of Como consists of 30 single-family lots on 10.60 acres with the Villa housing type. Como is located northwest of the 11th hole of the D'Andrea Golf Course.

Ravenna

The village of Ravenna consists of 52 single-family lots on 10.85 acres with the Veranda housing type. Ravenna is located in the heart of phase three, just south holes 12 and 13 of the D'Andrea Golf Course.

Bolzano

The village of Bolzano consists of 14 single-family lots on 3.67 acres with the Vicinato housing type. Bolzano is located on the northern boundary, adjacent to the 13th hole of D'Andrea Golf Course.

Cortina

The village of Cortina consists of 22 single-family lots on 5.74 acres with the Vicinato housing type. Cortina is located in the northwest corner with views of the Truckee Meadows and the Sierra Nevada.

Vicenza

The village of Vicenza consists of 66 single-family lots on 13.07 acres with the Villaggio housing type. Vicenza is located adjacent to the 17th hole of D'Andrea Golf Course.

Merano

The village of Merano consists of 43 single-family lots on 14.46 acres with the Villa housing type. Merano is located atop a hill overlooking the entire subdivision and the Truckee Meadows.

Verona

The village of Verona consists of 54 single-family lots on 17.36 acres with the Villa housing type. Verona is located directly north of the 18th hole of the D'Andrea Golf Course.

Garda

The village of Garda consists of 25 single-family lots on 4.98 acres with the Veranda housing type. Garda is located directly south of the 18th hole of D'Andrea Golf Course.

Firenze

Firenze is a village of mixed housing. This village is located in the core of the golf course surrounded by the 1st, 4th and 5th fairway. This village consists of Villa and Vicinato style of housing. There will be approximately 90 Villa lots with dimensions of 70' x 105' and 167 Vicinato homes with dimensions of 65' x 105' on 58.5 acres. Access to this village is from South D Andrea Parkway.

Novara

Novara consists of Villa style housing. This village is south of South D Andrea Parkway between golf holes 2 and 3. This village will be 31 lots on 5.80 acres with dimensions of 70' x 100'.

Milano

Milano is an intimate village of 19 lots on 9.8 acres with dimensions of 70' x 115' or Vicinato housing style. Access is from South D Andrea Parkway. This village fronts on the 2nd fairway.

Savona

Savona is (Bella Vista) on the slopes of the southeastern corner of D'Andrea. This area is adjacent to the existing custom homes located in Washoe County and is designed to have lot sizes of one acre when adjoining the neighboring homes. These lots will look down on golf holes 2 and 3. These are 27 Bella Vista lots on 25.0 acres ranging from one third acre to one acre. Access to this village is from South D Andrea Parkway.

Ticino

Ticino is 12 custom lots (Bella Vista) on 9.3 acres located on South D Andrea Parkway adjacent to the 3rd fairway and Marvin Moss Elementary School. This village is adjacent to the existing custom homes located to the south and the housing will be similar to the neighboring homes. These lots are approximately one acre when adjoining neighboring homes. An emergency access connecting to Westview Drive will be provided from the proposed cul-de-sac.

Treviso

Treviso is custom (Bella Vista) housing south of the park and existing Marvin Moss Elementary School. The lots range in size from one third to one acre. The lots will be one acre when adjoining neighboring homes. This village incorporates Treviso Park. There will be 13 lots on 9.5 acres. To the east of this village are existing custom homes. The lot sizes will be similar to the neighboring homes. Access to this village is from Primio Way.

Monticello

Monticello is a private-gated community with Veranda style housing that is nestled around the 8th and 9th holes of the D'Andrea Golf Course. There will be 72 lots with an average lot size of 4,700 square feet all of which will enjoy the views of the Reno/Sparks area as well as the surrounding Sierra Nevada. In accordance with the Project Concept section, the village of Monticello was created by transferring existing residential density and acreage to a previously disturbed area deemed appropriate for development by the Administrator.

Policies

The following are the policies guiding development in D'Andrea:

- Smaller lots (less than 5,000 square feet) shall be located so that there are parks, open space, golf course or other recreational facilities including the trail network provided with direct access.
- The streets shall be aligned when feasible on a north/south access minimizing winter hazardous driving conditions.
- The street names shall be within the Italian Theme.

Site Analysis

Views

One of the most significant assets of this property is the views. The site rests on the eastern foothills defining the Truckee Meadows basin. Subsequently, this affords the majority of the property with spectacular views of the entire Truckee Meadows, the Sierra Nevada Mountain range and the other mountains surrounding the Truckee Meadows. Refer to Figure 1-5 Site Analysis.

In reverse, D'Andrea will be visible from a distance across the Truckee Meadows. Keeping this in mind, D'Andrea is designed to work with the land. The focus of D'Andrea is the golf course, centrally located providing a green core. The master grading is designed to blend the cut and fill slopes into surrounding terrain.

Topography

D'Andrea's topography has elevations ranging from 4402 to 5145 feet above sea level. The property provides an assortment of terraces, knolls and gently sloping areas. The majority (65%) of the property consists of topography ranging from 0% to 15% slope with 50% of the property ranging from 0% to 10% slope. The meadows or pastures along Vista Boulevard are essentially flat. Rising up from the meadows are a series of terraces with a gently sloping area located centrally in the eastern area of the property. This provides opportunities for a variety of housing surrounding the golf course. Residential development on slopes will be reviewed by the City of Sparks through tentative maps and special use permits. Table 1-5 Slope Analysis summarizes the slope categories by acreages. Refer to Figure 1-5 Site Analysis and Figure 1-6 Slope Analysis. The steep slopes identified on the site analysis are generally slopes exceeding 25% slope.

Table 1-5 Slope Analysis

Slope Category	Area	Percent of Total
0 - 10 %	432.3 acres	50.2%
10 - 15 %	130.3 acres	15.1%
15 - 20 %	108.3 acres	12.6%
20 - 25 %	78.2 acres	9.1%
25 - 30%	46.0 acres	5.3%
30+%	65.9 acres	7.7%
Total	861 acres	100%

There are six identified significant protected ridgelines located on this property per SMC 20.99. The project to limit disturbance of these ridgelines. The proposed lot layout will place structures below these ridgelines where possible. Refer to Figure 1-5 Site Analysis. The section titled Site Grading Principles (page 3-40) describes the process D'Andrea will utilize to secure review and approval by the City to ensure compliance with the City's requirements for development on hillsides, ridgelines and slopes. Any structures accessed by a public road, private road, driveway or fire department emergency access roads with a grade of 12%+ will be required to be fire sprinklered to the satisfaction of the Fire Chief.

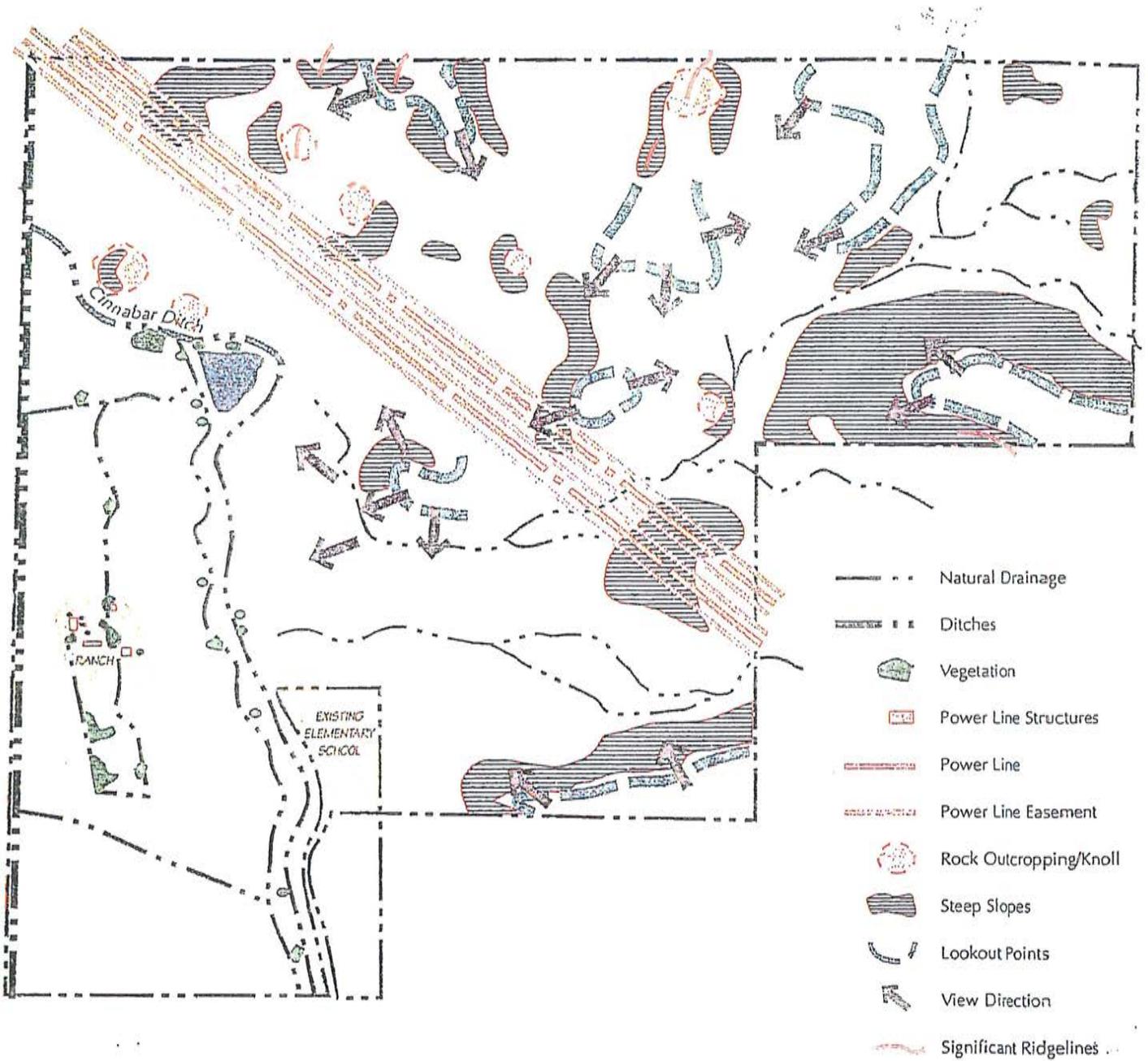


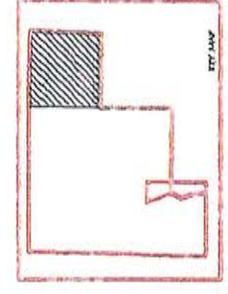
Figure 1 - 5 Site Analysis

D'Andrea Ranch SLOPE ANALYSIS

SHEET 2 OF 3



JEFF CODECA
CONSULTANTS, INC.
SLOPE ANALYSIS • LANDSCAPE ARCHITECTURE • ENGINEERING
15000 S. GARDEN • SUITE 100 • DENVER, CO 80232 • 303.755.1100
JAN 14, 2020



Range Statistics

Range	Beg.	End.	Area	Percent
1	0.00	10.00	1159876.04	16.2
2	10.00	15.00	1397554.84	18.5
3	15.00	20.00	1430138.96	19.9
4	20.00	25.00	1015555.44	14.2
5	25.00	30.00	748489.12	10.4
6	30.00	999.90	1424231.54	19.8

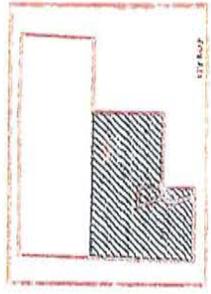


D'Andrea Ranch SLOPE ANALYSIS

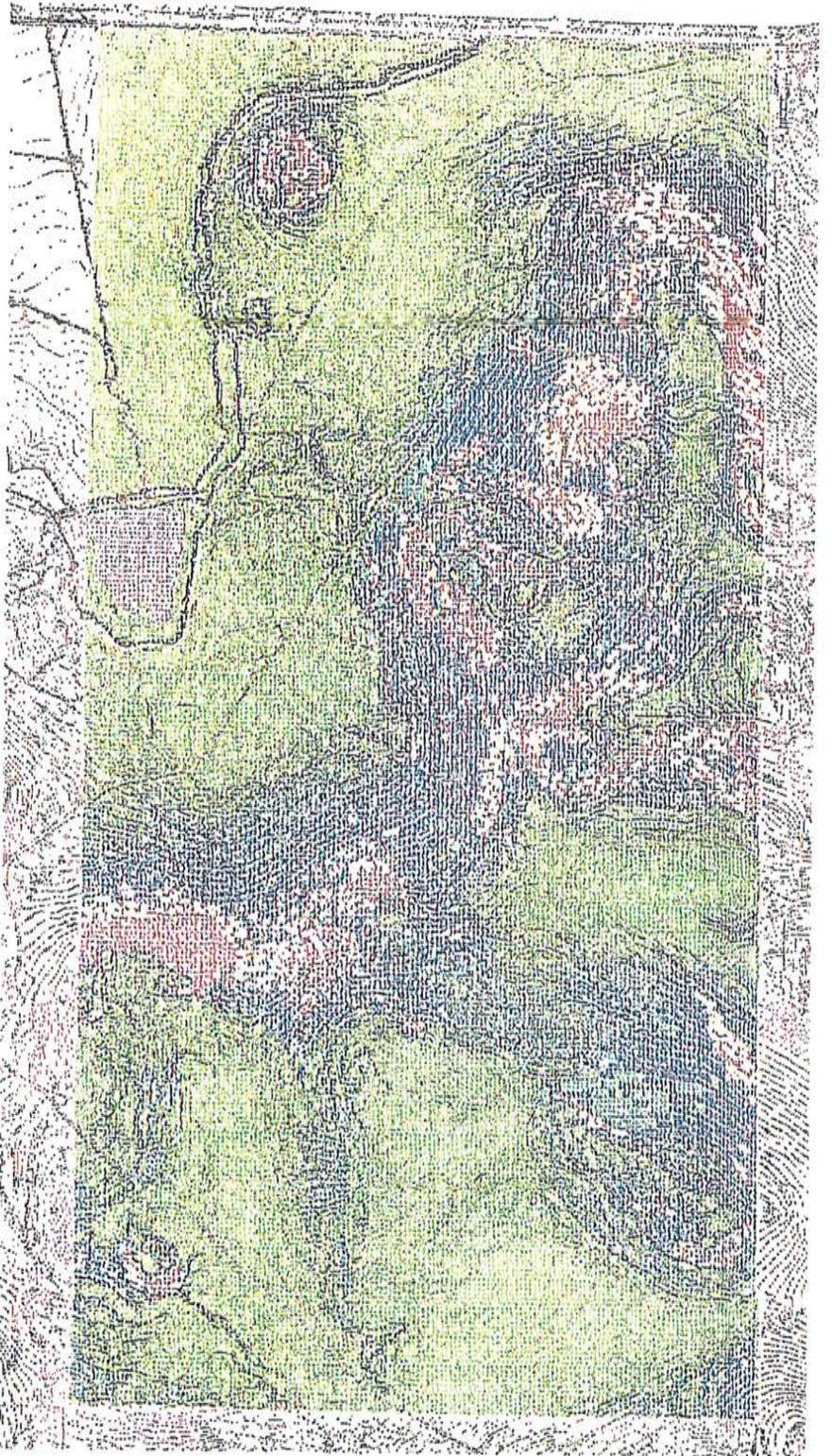
SHEET 3 OF 3



Range	Deg.	End.	Ared	Percent
1	0.00	10.00	11687459.33	68.1
2	10.00	15.00	1895571.58	11.0
3	15.00	20.00	1221865.73	7.1
4	20.00	25.00	1007282.43	5.9
5	25.00	30.00	561082.73	3.3
6	30.00	999.90	786533.09	4.6



JN 10/03

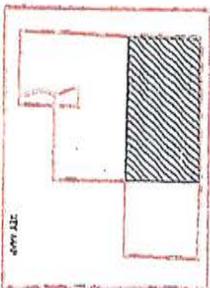


Project Range Statistics

Range	Req.	End.	Area	Percent
1	0.00	10.00	19331.646,46	43.9
2	10.00	15.00	56761.60,12	13.9
3	15.00	20.00	47138.23,35	14.0
4	20.00	25.00	34064.50,18	10.0
5	25.00	30.00	19934.79,89	6.3
6	30.00	999.90	29275.66,35	9.9

Range Statistics

Range	Req.	End.	Area	Percent
1	0.00	10.00	64843.08,29	47.4
2	10.00	15.00	23830.33,70	17.2
3	15.00	20.00	20638.17,66	15.0
4	20.00	25.00	13838.12,31	10.1
5	25.00	30.00	68330.08,04	5.1
6	30.00	999.90	716800.72	5.2



D'Andrea Ranch SLOPE ANALYSIS

SHEET 1 OF 3


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JAN 12, 2000

General Hydrology

There are two natural drainage basins from the mountains to the east that run through D'Andrea. Naturally, these drainageways route runoff from the eastern mountains into the pasture areas. Refer to Figure 1-5 Site Analysis. The D'Andrea Master Drainage Study located in the supplemental appendices provides strategies to manage any potential increase in discharge as a result of development. The existing and enhanced drainageways and detention basins will serve as part of the open space and golf course features. This will reduce the fully developed 100-year peak discharge to levels that are at or below the existing condition discharges. The Master Drainage Study also provides strategies to improve the existing drainage problems at Vista Boulevard and Prater Way.

None of the site is located within a Zone A as identified on FEMA Floodplain Maps.

In the pasture areas, there is a series of ditches that were used to irrigate the ranch. Portions of these ditches along with the existing pond will be incorporated into the golf course. The remaining portions will be discontinued or used as part of the site drainage.

Vegetation

The lower portions which served as the pasture for the D'Andrea Ranch are grasses with some native vegetation growing along the irrigation ditches. The upland areas are covered by sagebrush, rabbitbrush and native grasses. Along the utility corridor, grasses have been seeded which are well established. Existing trees located in the vicinity of the ranch house and one tree adjacent to the irrigation storage pond are primarily cottonwoods.

Potential Wetlands

A wetlands delineation has been completed by Gibson and Skordahl and reviewed by the Corps of Engineers. No jurisdictional wetlands are located on the property. Refer to letter from the Army Corps. of Engineers dated February 9, 1998 in the Supplemental Appendices.

Utility Corridor

Traversing D'Andrea are two electrical power line corridors. The northerly power line is located in an easement that is 140 feet wide, while the southern power line easement ranges from 0 to 125 feet width. The distance between power pole structures averages over 1,000 feet, and the distance between power lines averages 240 feet. There is a gap between the two power line easements. D'Andrea incorporates these corridors into the plan by using this area as open space, golf course recreation and part of the connecting trail system. D'Andrea will have no buildings located within the easements nor is the relocation of any of the power pole structures planned.

Project Phasing

Phasing strategy for D'Andrea provides a balanced approach to the buildout of the project. The phasing plan is a statement of the developer's intentions related to the pattern and timing of construction. The phasing plan described is not "cast in concrete" rather it presents a likely and logical sequence for development for

D'Andrea. Factors that can affect phasing plans include changes in interest rates, market demands for various types of housing, the pace of individual developers, the availability of infrastructure, and legal limitations such as filing of final maps. However, any substantive change in the phasing plan as shown in Figure 1-7 Phasing Plan shall require review and approval by the Planning Commission and City Council.

The backbone infrastructure, superpads and golf course will be constructed in 3 phases by the D'Andrea Nevada, LLC. There will be parcel maps recorded to create the superpads (projects) for sales to the homebuilders. The phasing of the villages will be associated with the tentative maps for each project. It is anticipated that D'Andrea will be built out in about ten years.

Phase I includes the backbone infrastructure and mass grading for the area generally west of the utility corridor. It is anticipated that construction of this phase will commence in the summer of 1998. The development of the golf course will commence in this phase although parts of the golf course extend through later phases. This will be done in order to accommodate public play by the Year 2000. Generally, Phase II is the area east of the utility corridor and west of the spine road to the public lands east of the property. The third phase is the area on the upper terraces generally along the Val Gardena Parkway to the public lands. Refer to Figure 1 - 7 Phasing Plan.

Process Section

These standards will be used in conjunction with all applicable city ordinances, maps and use permit conditions of approval, as well as any other applicable requirements. The required submittals to the City of Sparks will be per Sparks Municipal Code and Nevada Revised Statutes. Unless expressly provided for where the final Design Handbook Standards are in conflict with the Sparks Municipal Code Requirements, the stricter of the two documents shall apply.

Subdivision of land within D'Andrea will be via parcel maps, tentative maps and final maps per Nevada Revised Statutes and City of Sparks Municipal Code. Tentative map submittals for Veranda, Villaggio, Vicinato, Villa and Bella Vista Villages shall include conceptual building elevations approved by the DCC unless determined otherwise by the Administrator. Final building elevations and building set backs are reviewed and approved by the DCC, Planning Commission and City Council, prior to the submittal for the final map(s) for those villages.

Applications for Design Review approval shall be submitted to the D'Andrea Community Association (DCA) along with the applicable application fee prior to application to the City for any entitlement (Special Use Permit, Site Plan Review and Building/Sign permits). The applicant is encouraged to request a Conceptual Review with the DCC of preliminary plans to determine if the proposed design is consistent with the D'Andrea design intent. Design Review and Approval shall consist of the following steps:

1. Conceptual Design Review (Optional)

In order to save the applicant unnecessary costs associated with design, upon request the DCA Design Control Committee (DCC) of DCA will review a conceptual design for general compliance with the standards prior to a preliminary Design Review of project.

2. Preliminary Design Review

The applicant shall submit the materials shown in the Project Submittal Checklist to the DCC. The DCC will review and approve or disapprove the design, or recommend changes based on the project's compliance with the standards.

3. Construction Document Review

The applicant shall submit 5 copies of the full project construction documents to the DCC for review for compliance with the Preliminary Design Review.

4. City Review

A letter or stamped plans from the DCA approving the project shall be attached to the set of building plans,

parcel map, tentative map, final map improvement plan, special use permit, or Site Plan Review application and Submittal Material to be reviewed by the City of Sparks. The applicant shall receive the required permitting from the City of Sparks prior to commencing construction or implementation.

5. Site Inspection

The DCC will inspect the project upon completion to review for compliance with approved submittal prior to, or in conjunction with the City's review for compliance with approved submittal.

The following list is the submittal requirements to the DCA. Submittals to the City of Sparks will be per City requirements in effect at the time.

The following is required for Preliminary Design Review submittal:

- Location Map - Indicating location of property within the development, assessor's parcel number, and name, address, and telephone and fax number for both the owner and project designer/design team.
- Site/Grading Plan - 1" = 50' minimum scale, showing property lines, dimensions, setback requirements, and existing topography. Indicate proposed grading and drainage, including finish contours. Show proposed driveways and structures with grade elevations.
- Landscape Plan - 1" = 50' minimum scale. Showing all proposed plant material, plant size, groundcover, finish grading, hardscape, retaining walls, fence design and locations, landscaping calculations and other information as required per Title 20 of the Sparks Municipal Code.
- Irrigation Plan - 1" = 50' minimum scale. Showing irrigation information as required per Sparks Municipal Code Title 20.
- Conceptual Floor Plans - 1/4" = 1' scale preferred. Include plans for all levels.
- Calculation of Square Footage.
- Conceptual Roof Plan - Show roof pitch and direction.
- Exterior Elevations - Show all elevations with existing and finished grades clearly indicated. Note all exterior materials, roof slopes, and heights above finish grade.
- Materials Samples Board - Provide actual samples of all primary exterior materials and paint colors applied to the actual exterior materials.
- Entry signage - showing dimensions, materials, colors, lighting, and location.

The following is required for submittal to the City for review of tentative maps:

- Tentative Map - lotting plan per SMC
- Location Map - Indicating location of property within the development, assessor's parcel number, and name, address, and telephone and fax number for both the owner and project designer/design team.
- Site/Grading Plan - 1" = 50' minimum scale, showing property lines, dimensions, and existing topography. Indicate proposed grading and drainage, including finish contours. Show proposed driveways and building pads with grade elevations.
- Preliminary Landscape Plan - 1" = 50' minimum scale. Showing conceptual design and plant list, including proposed plant names, specific plant material and sizes, at time of planting and street tree per the Final Design Handbook Standards. Accompanied by DCC approval stamp or signed DCC approval letter.
- Entry signage - showing dimensions, materials, colors, lighting, and location.
- Slope Analysis Map - 1" = 50' minimum scale to indicate if the site's existing topography requires review and approval as a Hillside Development under Sparks Municipal Code standards. If the subject site has slopes of 10% or greater over 25% or more of the development site, additional submittal material requirements under Sparks Municipal Code 20.99.050 are required with the tentative subdivision map review.
- Conceptual Building Elevations - for the Veranda, Villaggio, Villa and Vicinato villages, a DCC-approved set of conceptual building elevations are required illustrating how the proposed development complies with the architectural standards as listed in the Final Design Handbook.
- Setback Map - 1" = 50' minimum scale. Showing the street layout(s), driveway locations, lot dimensions,

proposed building/structure locations and the required building setbacks per the approved Final Design Handbook. Accompanied with a DCC-stamp of approval or signed DCC-approval letter.

Plan Amendment

Any amendments to this Handbook and/or standards will require review by the City of Sparks Administrator for determination of compliance to the Handbook. If the determination that the amendment will constitute a Plan Amendment, then D'Andrea Community Association (DCA) will submit the proposed amendment to the City of Sparks for public review. The amendment will follow application reviewing procedures as outlined in Sparks Municipal Code 20.19.030 including being reviewed in a public hearing before the Planning Commission and City Council. D'Andrea Villages will be developed using parcel maps and tentative maps per City of Sparks Codes and Nevada Revised Statutes.

Minor Adjustment

The Administrator of the D'Andrea Handbook shall be allowed to permit minor adjustments as long as the proposed adjustments from the standards are established in this Final Design Handbook for D'Andrea is not more than a 20% variance. The applicant for the minor adjustments shall submit the proposed minor adjustments before the D'Andrea Design Control Committee for Review and Approval prior to submittal to the Administrator. The submittal to the administrator shall include a letter describing the proposed adjustments, a letter of approval or stamped plans indicating the approval of the DCC, and a complete set of dimensional plans illustrating the adjustments and potential effect on surrounding properties. The administrator shall deny, approve or conditionally approve the minor adjustments and convey the decision in a letter to the applicant. The sole exception is that the Veranda and Villaggio Villages are specifically excluded from the minor deviation adjustment process.

Appeal Process

The decision of the administrator may be appealed to the Planning Commission and City Council with City Council as the final arbitrator. The Planning Commission and City Council shall hear the appeal and either affirm, modify or reverse the decision of the administrator. The appeal request must be submitted to the Planning and Community Development Department within ten (10) days from the date of the decision of the administrator.

Applicability

These Development Standards are applicable to new construction projects as well as to proposed additions and modifications to structures and landscape elements by homeowners/developers/homebuilders.

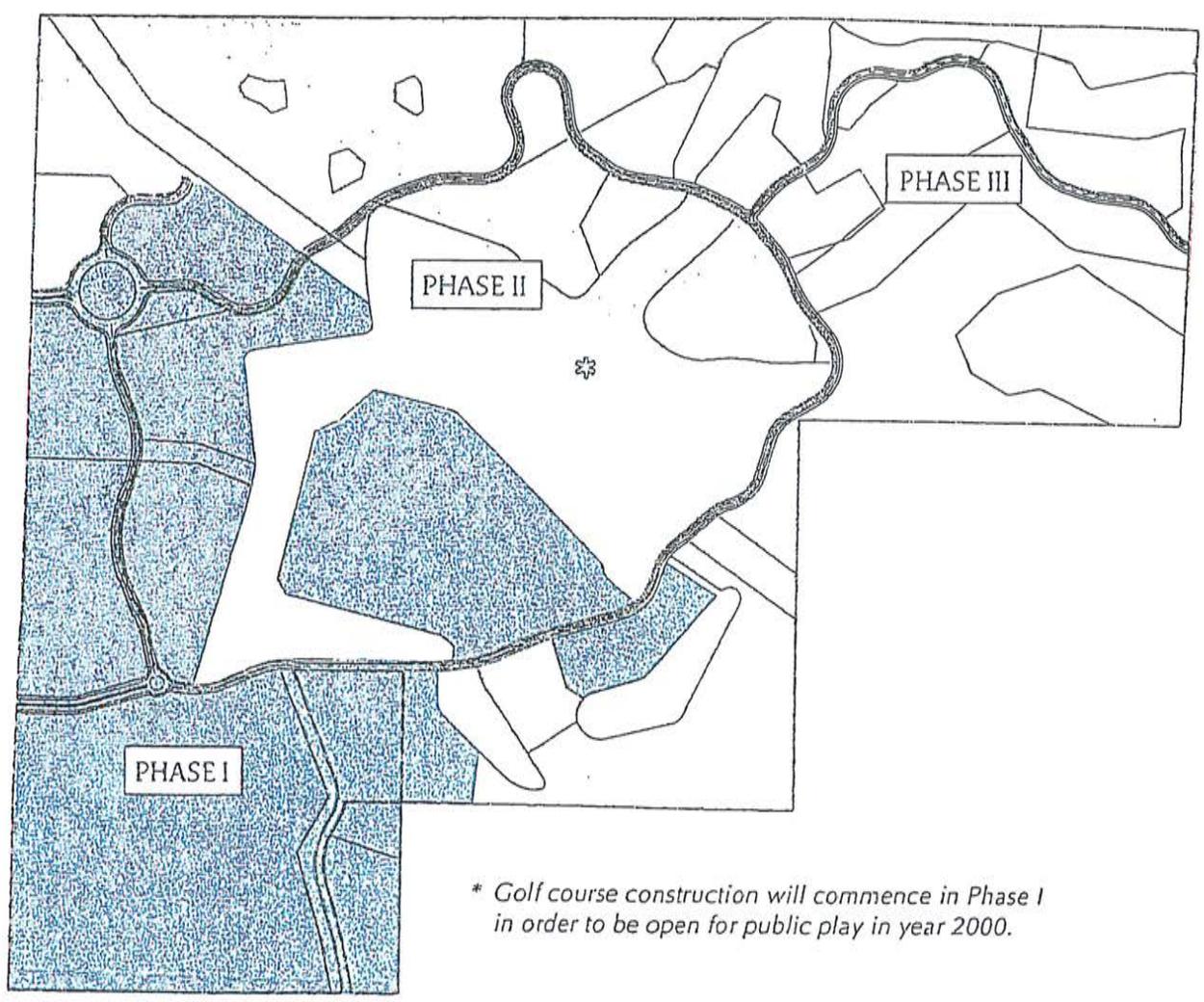


Figure 1- 7 Phasing Plan

CHAPTER 2

CHAPTER 2 - INFRASTRUCTURE AND PUBLIC SERVICES

Access and Circulation

The main access to D'Andrea is provided by Vista Boulevard. The Vista Interchange off Interstate 80 is approximately 2 miles to the south, providing D'Andrea excellent access to Interstate 80. Vista Boulevard is a four lane major arterial serving the east side of Sparks. The Regional Transportation Commission plans to widen Vista Boulevard to 8 lanes when warranted.

Streets

Both of the entrances to the project will feed parkways designed to facilitate the traffic flow to and from Vista Boulevard. There will be a looped entrance roadway connecting the north and south entrances to D'Andrea. This looped roadway, D Andrea Parkway will function as the main traffic spine. There will be collectors or connector roads which will access off D Andrea Parkway. There will be no direct access off these roadways and no parking allowed. Traffic within the residential project will be internalized with limited access on collector roads. Refer to Figure 2-2 Circulation Plan. There will be two types of collector streets through D'Andrea. Street design and right-of-way will be to the approval of the Public Works Director. Refer to Figure 2-1 Street Cross Sections.

Vista Boulevard - The developer will dedicate right-of-way for the future widening of Vista Boulevard. A sidewalk will be provided along Vista Boulevard which will eliminate the need for the school zone.

Community Collector - The community collectors: South D Andrea Parkway, North D Andrea Parkway, and San Marino Drive will serve as the backbone circulation for D'Andrea. At the entrance to the project, North and South D'Andrea Parkway will have a 76 foot right-of-way narrowing down to 31 feet past the intersection of San Marino Drive. San Marino Drive will have a 31 foot right-of-way with landscaped common area. There will be no parking along the sides of these streets. Significant landscaping within a common area will flank both sides of the street. Pedestrian access adjacent to both the parkways and community collector street sections shall include a 5 - foot wide concrete sidewalk on one side and 10 - foot wide asphalt meandering path, located on the inside of the general curvature of the road, with finished edges (metal/aluminum or recycled plastic edging) on the other side of the street section. Refer to Figure 2-1 Street Cross Sections.

The developer will provide emergency vehicular access to Westview Boulevard per City standards.

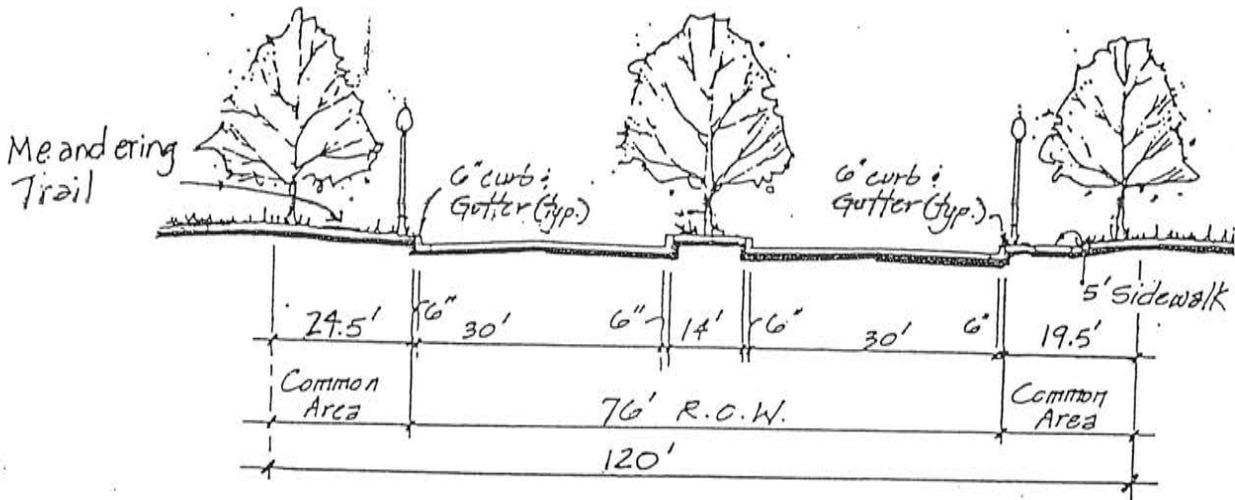
Residential Streets - The roadways within the subdivisions (villages) will be designed to City of Sparks standards with the possible exception of the roads within the Active Adult, Bella Vista and cluster (Veranda) projects. Public streets within the villages will have sidewalks provided on both sides.

Private Street - The D'Andrea project will have the option to propose to the Public Works Director the use of private streets with unmanned gates in active adult, Veranda and Bella Vista subdivisions and Mezzo housing. The entry access of the gates will be to the approval of the Public Works Director, Police Chief, and Fire Chief. Advanced detection systems for gates and vehicles will be provided by developer. Gate operating systems shall be required to provide non-delayed entrance by Fire, Police, and Emergency Vehicles. Gated communities shall be openable by a device that senses strobe lights. The sensing device must be compatible with equipment used by the City of

Sparks Traffic Division. A keypad entry system shall be provided for police entry. The roadway Security gates shall also be openable manually if power failure should occur. Systems shall be submitted for approval by the Fire and Police Chiefs and the Public Works Director. Approval will be required prior to final map submittal. Street sections are depicted in Figures 2 - 1 and 3 - 7 Street Cross Sections. Street lights, storm drain and sanitary sewer systems located within a private street shall be constructed to City of Sparks standards but shall be privately owned and maintained. Maintenance will be provided by homeowner's association for the villages approved for the use of private streets. The private streets and driveways, particularly those associated with cluster type developments shall be designed utilizing sound engineering practices to approval of Public Works Director. Design shall demonstrate that safe vehicular and pedestrian interaction and movements can occur to approval of Public Works Director, Fire Chief, and Police Chief.

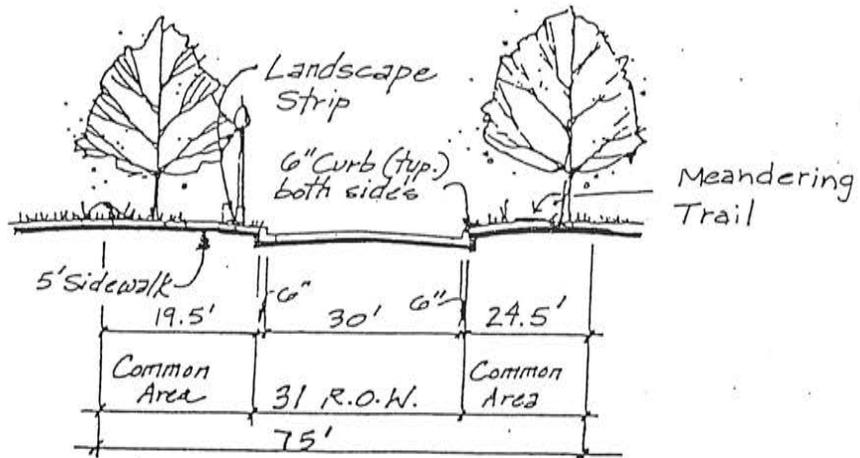
Developments proposing to utilize private streets shall submit for review and approval by the Public Works Director a maintenance schedule for the proposed private roads, showing the estimated costs associated with the maintenance of the development's private roads to a level equivalent to the city's street maintenance schedule for public streets. The DCA shall update the maintenance schedule on an annual basis to the satisfaction of the Public Works Director.

- Roundabouts - The minimum circumscribe diameter for the roundabouts shall be 95 feet. The traffic flow shall be one - way counter clockwise. There shall be 2 traffic lanes. There will be no pedestrian access to the roundabout island unless approved by Public Works Director. Roundabouts shall meet the turning radius for the longest fire department fire apparatus to the satisfaction of the Fire Chief.
- Pedestrian Access - Full, continuous pedestrian access shall be provided throughout the entire D'Andrea Development. As a minimum, sidewalks and connecting ramps for the Physically Disabled shall be provided on all Public Streets within the right of way and/or the common areas. Likewise, as a minimum, sidewalk and connecting ramps for the disabled shall be provided on one side of all private streets. All sidewalks throughout the development shall be constructed of concrete and be in accordance the Standard Detail for Public Works Construction. Sidewalk and path widths will vary as indicated elsewhere in this handbook. The developer may submit an alternative pedestrian access plan for review and approval by the Public Works Director, Administrator and the Parks and Recreation Director. The alternative pedestrian access plan shall be submitted as a part of the tentative map process.



PARKWAY ENTRY

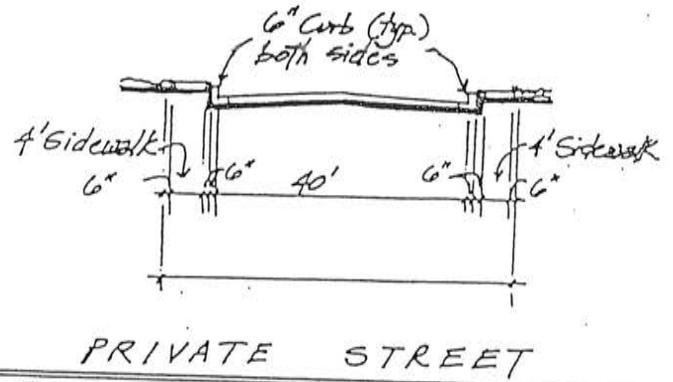
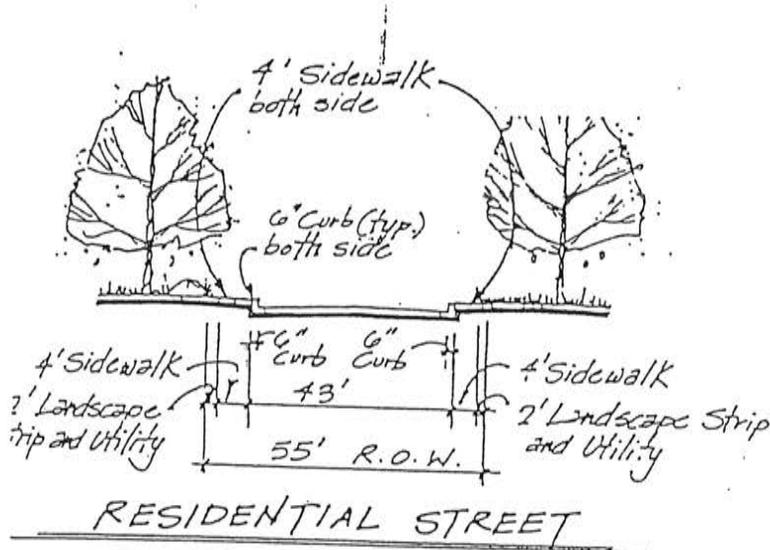
Note: Refer to Figure 2-3 Primary Trails/Sidewalks for location.



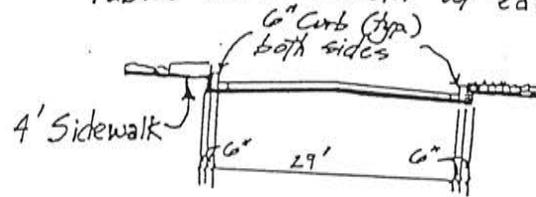
COMMUNITY COLLECTOR STREET

Note: Refer to Figure 2-3 Primary Trails/Sidewalks for location.

Figure 2-1 a Street Cross Sections



(Note: Active Adult Mezzo, Veranda and Bella Vista projects only Subject to review and approval by the Public Works Director w/ each project.)



No On Street Parking - There will be 90° parking pockets at 0.5 spaces/unit.
(Note: only for Mezzo, Veranda, and Bella Vista projects, subject to review and approval by the Public Works Director w/ each project.)

Figure 2-1b Street Cross Sections

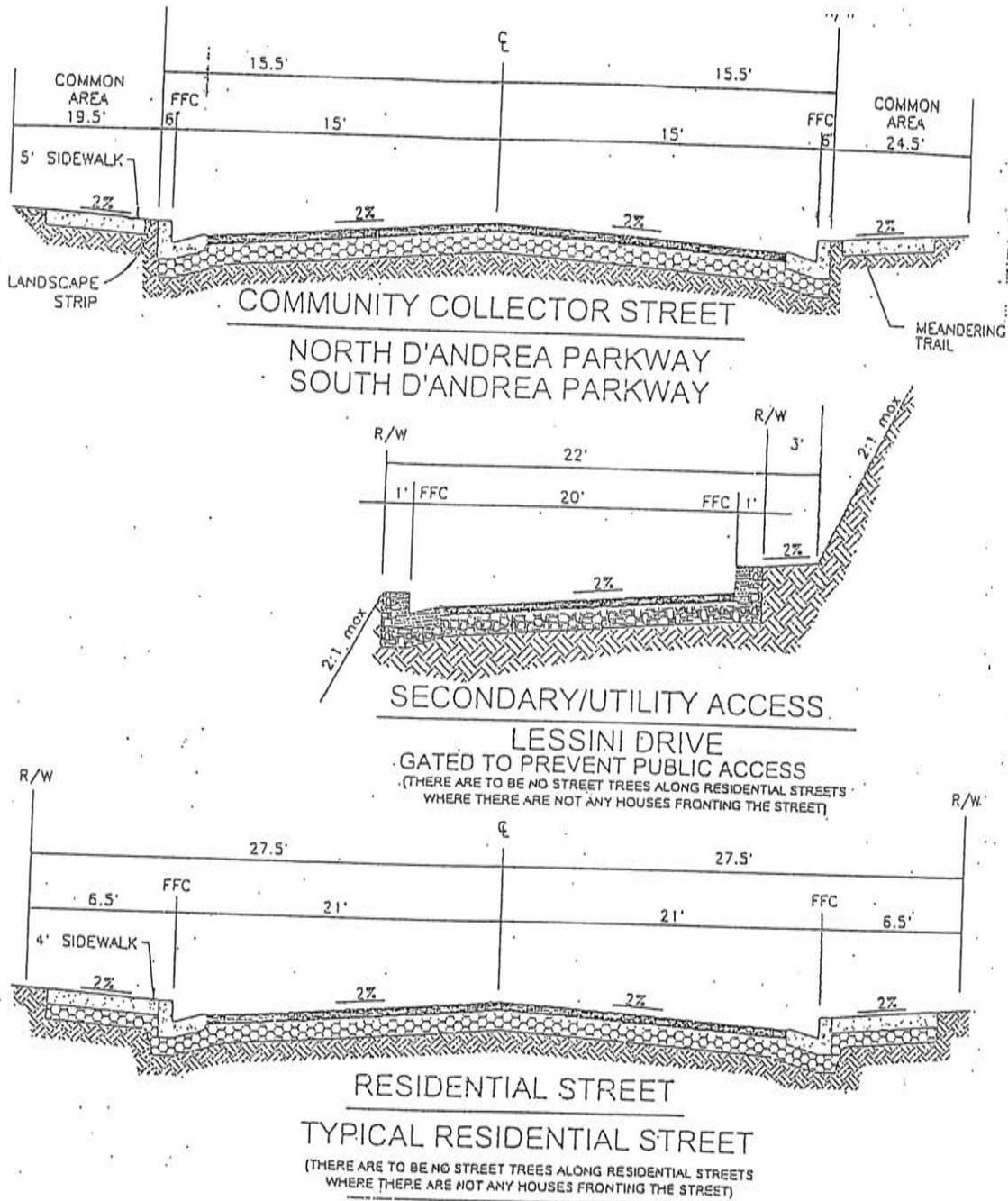
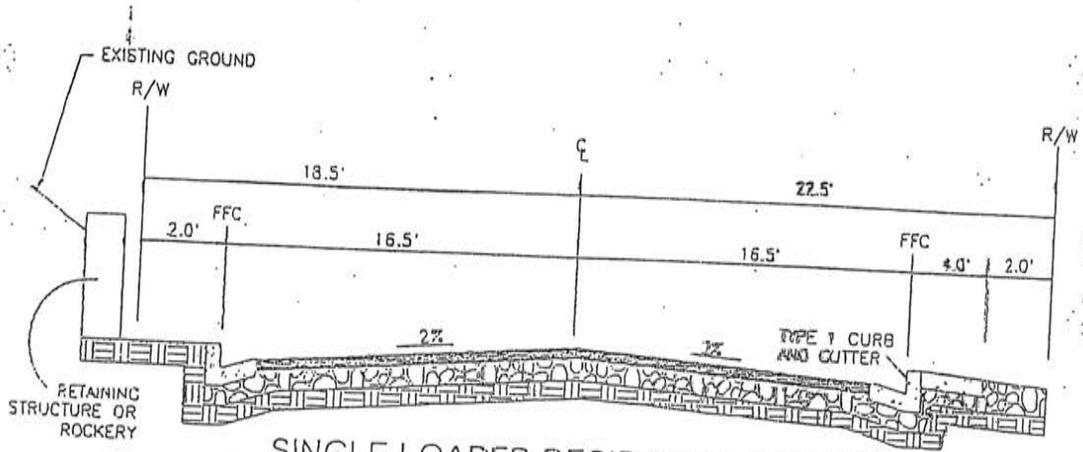
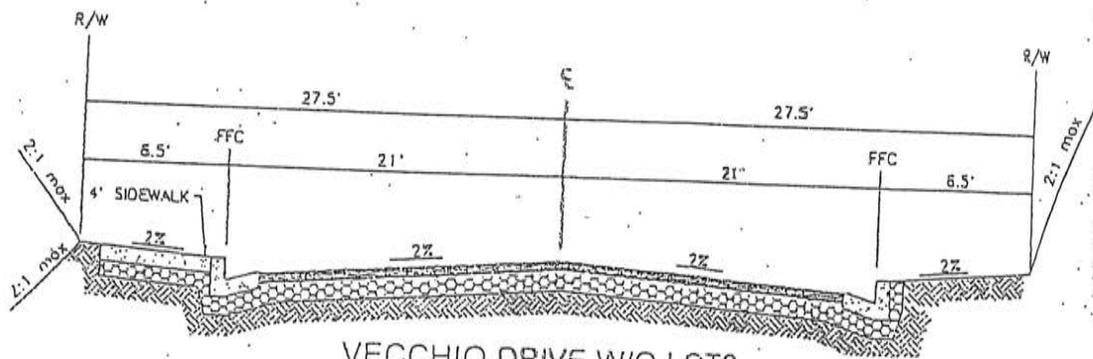


Figure 2-1 c Engineered Cross Sections



SINGLE LOADED RESIDENTIAL STREETS

(THERE ARE TO BE NO STREET TREES ALONG RESIDENTIAL STREETS WHERE THERE ARE NOT ANY HOUSES FRONTING THE STREET)



VECCHIO DRIVE W/O LOTS

FROM THE LAST LOT IN CORTINA TO THE FIRST LOT IN MERANO

(THERE ARE TO BE NO STREET TREES ALONG RESIDENTIAL STREETS WHERE THERE ARE NOT ANY HOUSES FRONTING THE STREET)

Figure 2-1 d Engineered Cross Sections

Traffic Analysis

This summary will give an overview of the expected traffic impacts. Detailed analysis will be provided in a separate traffic impact study enclosed in the Supplemental Appendices.

Trip Generation - The number of trips generated from a development is determined by identifying the type and size of each land use being developed, and applying trip rates from accepted sources. Trip generation for this project was estimated using Trip Generation, 6th edition, published by the Institute of Transportation Engineers.

The total number of trips generated consists of primary trips and pass-by trips. Primary trips are those made specifically to this project site. Pass-by trips are trips already on the nearby street network, but are attracted to the site after construction. All trips are considered to be primary trips, except trips destined for the shopping center. Data from Trip Generation, 5th edition, indicate that approximately 40% of the total shopping center trips will be drawn from traffic already using surrounding streets. The following table displays the estimated trip generation for the entire project.

Table 2-1 Project Trip Generation

	Units	Weekday	AM Peak	PM Peak
Single-Family	1,890 units	15,900	1,370	1,560
Multi-Family	310 units	1,900	140	170
Commercial	145,000 sf	8,700	200	800
Office	75,000 sf	1,100	150	160
Golf course	18 holes	650	50	60
Elementary school	600 students	650	170	150
Day care	275 children	1,000	150	180
Recreation center	7,500 sf	240	10	30
Total Trips		30,140	2,240	3,110
Less 40% pass-by trips (shopping center)		3,500	80	320
TOTAL ADDED TRIPS		26,640	2,160	2,790

Regional Street Network - Project traffic will use the internal street network to connect to Vista Boulevard, which is designated as a minor arterial. One project entrance will form the fourth leg of the intersection of Vista & Baring Boulevards, which will need a traffic signal in the future. As the D'Andrea Project develops, the south project entrance will form the fourth leg of the intersection of Vista Boulevard & Whitewood Drive. There is an existing traffic signal operating at this intersection which may need upgrading with the project.

Future traffic volumes have been estimated by the Regional Transportation Commission (RTC) staff using a computerized prediction model. This land use proposal has been incorporated into the model and the results

have been included in the traffic report appendix submitted as part of the appendices in this handbook. The modeling helps determine how the development will integrate into the regional street system, and what improvements will be necessary to accommodate the increase in vehicular traffic. This analysis has shown that Vista Boulevard is designated by RTC to be widened to eight lanes to accommodate future traffic at Level of Service (LOS) "C".

Using the current regional road impact fee schedule, D'Andrea is expected to generate more than \$4.7 million towards regional street capacity projects. These funds are administered by the RTC and must be used in this benefit area.

Speed Humps - Speed humps will be provided on residential streets in accordance with City's Speed Hump Policy. Design will be provided such that the requirement for speed humps will be minimized to the full extent possible.

Alternative Transportation Modes - Transit demand is not expected to be high in the residential portion of this project, due to its low density, nor for the golf course. However, the commercial/office area may warrant some transit service. There will likely be a need for school bus service to the elementary school. Travel by bicycle and on foot will be strongly encouraged through site design and provision of bike, golf cart - designated trails, and pedestrian facilities. This will help reduce internal vehicular trips, but will not significantly reduce regional traffic generation.

Unsignalized Intersection Analysis - Vista and Baring as an unsignalized intersection under existing conditions. The overall level of service is "A" (average delay 6 second per vehicle); however, the east-to-north left turn movement, although relatively low volume, experiences a level of service "F" due to delay caused from opposing through movements.

Segment Analysis - With the adoption of the Regional Road Impact Fee program, future street improvements have been defined by average weekday traffic levels. Although this type of analysis does not accurately reflect true traffic conditions, it an expedient method to evaluate conditions area-wide.

Table 2-2 displays results of this type of analysis. Numbers in parentheses indicate number of lanes needed for level of service "C" based on average lane capacity figures originally developed by the City of Reno for major arterials. Due to excellent control of access on Vista, values are conservative.

Summary - Future level of service is reduced with the addition of traffic from D'Andrea. The effect of the D'Andrea traffic is isolated by adding project traffic to existing traffic. With project traffic added, level of service at all intersections does not drop below "C", with suggested intersection improvement. However, this does not reflect the effect of other future developments which may contribute traffic to these intersections. Traffic improvements will be provided in accordance with the approved traffic report, RTC recommendations and Public Works Director. Traffic improvements are required to meet RTC minimum design standards in order to be eligible to receive impact fee credits in accordance with the Regional Road Impact Fee System General Administrative Manual.

Traffic Impact Analysis has been accepted and subject to provisions, recommendations, and amendments approved by the Public Works Director.

Table 2-2 Estimated Future Traffic Volume

Segment	2007		2015		Build Out	
	without D'Andrea	with D'Andrea	without D'Andrea	with D'Andrea	without D'Andrea	with D'Andrea
Vista n/o Baring	24,000 (4)	28,000 (6)	37,000 (8)	39,000 (8)	50,000 (8)	52,000 (10)
Vista s/o Baring	21,000 (4)	26,000 (6)	30,000 (6)	39,000 (8)	48,000 (8)	48,000 (8)
Vista n/o Prater	20,000 (4)	28,000 (6)	30,000 (6)	41,000 (8)	50,000 (10)	50,000 (10)
Vista s/o Prater	22,000 (4)	27,000 (6)	32,000 (6)	39,000 (8)	46,000 (8)	47,000 (8)
Vista n/o I-80	31,000 (6)	35,000 (6)	41,000 (8)	45,000 (8)	53,000 (10)	54,000 (10)
Baring w/o Vista	14,000 (4)	17,000 (4)	16,000 (4)	21,000 (4)	20,000 (4)	22,000 (4)
Baring w/o Sparks	26,000 (6)	30,000 (6)	34,000 (6)	38,000 (8)	48,000 (8)	48,000 (8)
Baring e/o McCarran	32,000 (6)	35,000 (6)	40,000 (8)	43,000 (8)	53,000 (10)	54,000 (10)
Baring e/o Vista	0	4,000 (2)	0	7,000 (2)	0	7,000 (2)
South Andrea Pkwy e/o Vista	0	7,000 (2)	0	10,000 (2)	0	10,000 (2)
Prater w/o Vista	7,000 (4)	8,000 (4)	8,000 (4)	9,000 (4)	10,000 (4)	11,000 (4)
Prater e/o Vista	3,000 (2)	3,000 (2)	4,000 (2)	3,000 (2)	NA	NA
Prater e/o McCarran	10,000 (4)	10,000 (4)	13,000 (4)	14,000 (4)	16,000 (4)	16,000 (4)
McCarran n/o Baring	18,000 (4)	20,000 (4)	19,000 (4)	22,000 (4)	25,000 (6)	26,000 (6)
McCarran n/o Prater	27,000 (6)	28,000 (6)	33,000 (6)	33,000 (6)	45,000 (8)	45,000 (8)
McCarran s/o Prater	30,000 (6)	31,000 (6)	38,000 (8)	36,000 (6)	46,000 (8)	45,000 (8)

Pedestrian Paths and Bikeways

There will be a pathway system throughout the project that will connect the individual projects, school(s), commercial uses, the golf course, and parks providing recreational opportunities for walking and bicycling. The trail/pathway system will be integrated with the sidewalk system throughout D'Andrea. Refer to Figure 2-3 Primary Trails/Pathways. The utility corridor will be used where meaningful as part of the trail system. The trails/paths will be used for pedestrians and bicycles, although some trails/paths may not be suitable for "bikeway designation" due to gradient or other factors. In some pathway sections, the trails/pathway may be designated for joint use with golf cart trail limited to areas outside of the golf course boundaries. The trail system will provide an alternative to the automobile for some travel. At the eastern edge of D'Andrea, there will be a staging area for access to the public lands. In Chapter 3, there are more details on the trails/pathway standards and trail head. Vista Boulevard pedestrian access/concrete sidewalk will be provided for the entire property frontage. Connection to Country Ridge Subdivision may allow the City to provide connection to the entrance which could eliminate the need for the existing school zone, crossing and medians at the intersection of Finco Way and Vista Boulevard.

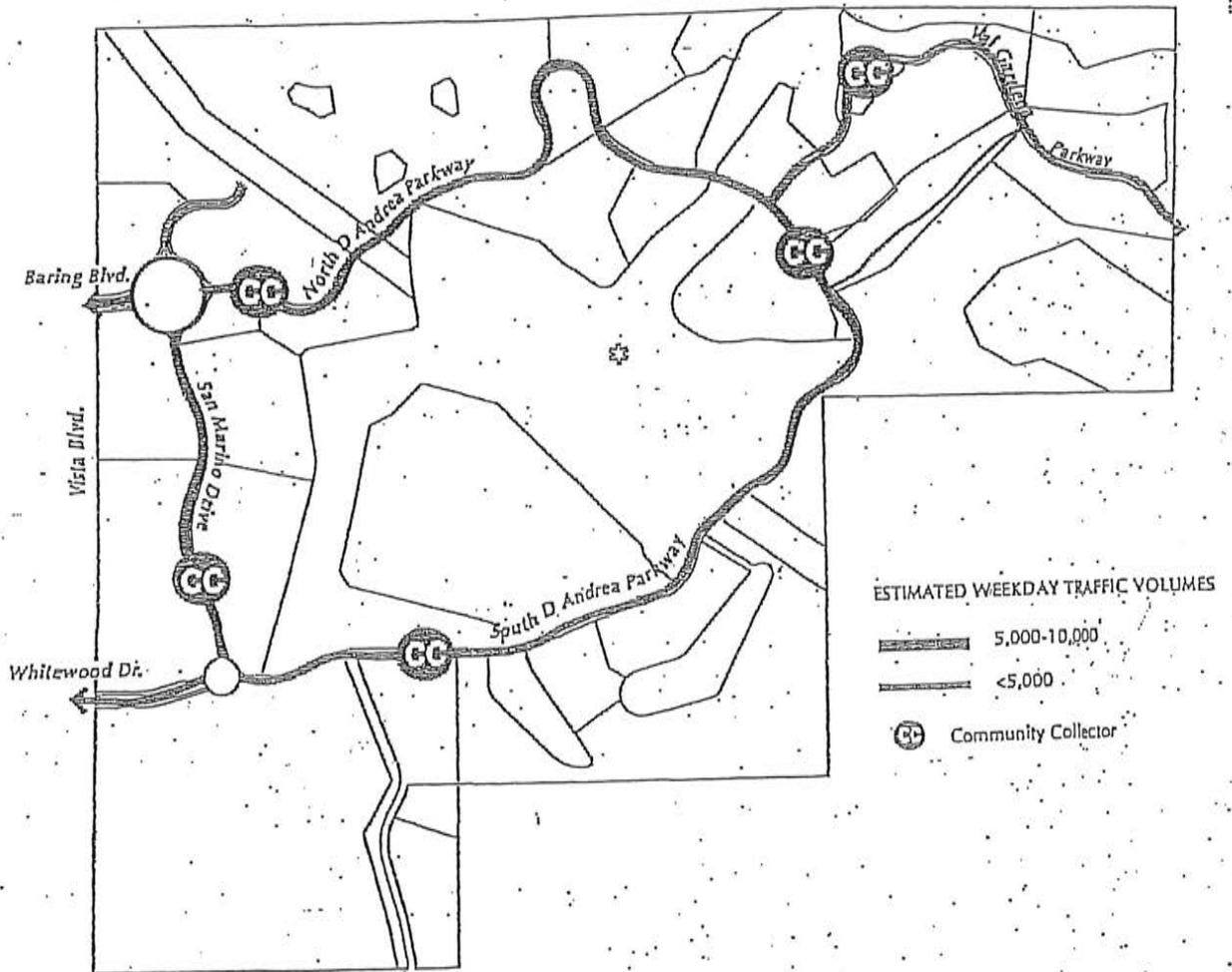


Figure 2-2 Circulation Plan

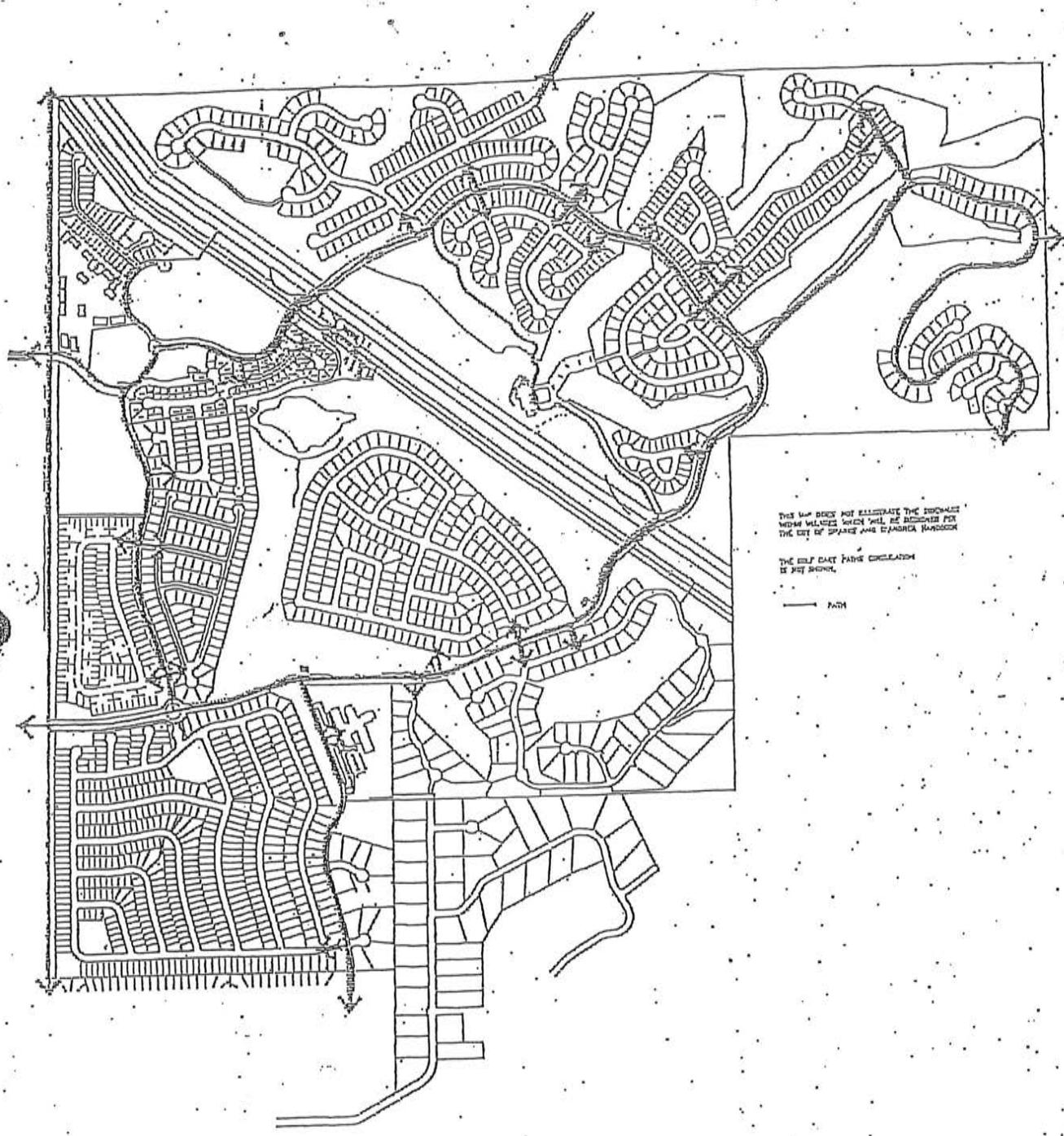


Figure 2-3 Primary Trails/Sidewalks

Utilities

Water

A portion of the property is located outside of the service district of Sierra Pacific Power Company. In discussions with Sierra Pacific Power Company, there will be no difficulty annexing into the service district. Upon annexation, D'Andrea will be served for water by Truckee Meadows Water Authority. Water rights will be transferred to the City of Sparks and service provided by TMWA as required by TMWA Rule #17. A 16" water line exists on Vista Boulevard. There will be an effluent line extension to D'Andrea for the watering of the golf course, streetscapes, trails and parks. The details of this line extension will be incorporated into the golf course design, which are part of the Landscape Section of this Handbook and a separate Development Agreement.

Table 2-3 Estimated Water Demand

Land Use	Demand (AFY)
Single Family Lots	767
Multi-Family Units	83
Commercial / Office	19
Open Space / Streetscape / Trails / Parks ¹	50
Golf Course ¹	350

¹ Note: These areas will be watered with reclaimed water.

Truckee Meadows Water Authority will serve the project's water needs by the 16" water main in Vista Boulevard. A series of 4 pump stations will be built on site at specific locations to serve all phases of development. Water tanks in various locations will be required to back up the system. Water tanks and pump stations to meet the approval of the Fire Chief. Water for fire protection shall meet the approval of the Fire Chief prior to any combustibles allowed on site.

Reclaimed Water

D'Andrea Nevada LLC will incorporate the use of reclaimed water for Golf Course and main infrastructure-landscaping uses. D'Andrea Nevada LLC has agreed with the City of Sparks Public Works Department to enter into an agreement to buy reclaimed water from the Truckee Meadows Water Reclamation Facility under a 25 year contract. The City of Sparks or D'Andrea Nevada LLC, under coordination with the City will complete a new line east down Shadow Lane starting at Sparks Boulevard and continuing to Pah Rah Park and going south down Vista Lane to the existing pond on the D'Andrea. Total water usage acre feet figures are being determined and will be incorporated in the agreement between D'Andrea and the City of Sparks.

CES, a consultant of the City of Sparks and Builder of the Sparks Sewer Effluent Plant will coordinate and complete the engineering for the D'Andrea proposed route.

Cinnabar Mountain Ditch

The Cinnabar Mountain Ditch/Ore Ditch Extension is an irrigation channel that has served northeast Sparks residents for many years. This ditch extends onto the D'Andrea Ranch property. Irrigation flows pass through an existing pond and eventually are captured in an existing drainage inlet at the southwest corner of the site. In the initial phase of this development, the means by which the irrigation and drainage requirements of this ditch and its stakeholders will be established, designed, and constructed to the satisfaction of the Public Works Director. The approval will be based upon a clear demonstration that there will be no adverse impacts to upstream users or downstream facilities. The Cinnabar Mountain Ditch provides water for pasture irrigation to the D'Andrea property through a series of ditches and underground pipes, traveling down Shadow Mountain Lane. D'Andrea Nevada, the developer, proposes to use the ditch water to irrigate the 18 hole golf course and the DCA landscaping. The irrigation water will be combined with the proposed effluent pipeline. The Cinnabar Mountain Ditch water will be stored in the existing pond which will be modified to increase the storage capacity.

Gas/Electric

Sierra Pacific Power Company has determined that sufficient capacity can be provided to serve this site via existing Vista Boulevard infrastructure. Sierra Pacific Power Company will provide the designs and improvement plans for these utilities.

Sewer/Sewerage

Introduction

The project will be divided into 3 phases. The development will connect to the existing sewer system at several locations, depending on available capacity. Refer to Figure 2-4 Sewerage for the phasing diagram, the onsite backbone sewer alignment and projected flows in the system. For the details refer to the Sewerage Report in the Supplemental Appendices.

Existing Sewerage Facilities

There are no existing sewage facilities within the site. There are several existing trunk lines that are near the proposed D'Andrea development all of which flow westerly and southerly to the Sparks Boulevard interceptor. These are described below. The ability of the development to use these existing lines is dependent upon the current capacity of those lines. The existing capacities in those mains have been metered with flow meters by Hydro Tech, Inc and by the City of Sparks. They were monitored near the Sparks interceptor to get the total existing contribution.

Baring Boulevard has a 15" main which collects flows and conveys sewage to the west in Baring Boulevard. There is a manhole in the intersection of Baring and Vista which could accept approximately 125 single family residential (SFR) units from the site directly from the east. The remainder of the site would convey to other systems. The proposed Desert Highlands project may use this line also, containing approximately 330 residential units, 520 apartments, and 9 acres of commercial property.

Del Rosa Way has a 10" main which can be accessed approximately 150 feet west of the Vista Boulevard intersection. This line conveys sewage to a lift station which pumps it north and into the Baring Boulevard trunk line. The commercial area, the 310 apartments and approximately 589 residences can gravity flow to

this location.

Whitewood Drive has an 8" main which can be accessed approximately 600 feet west of the Vista Boulevard intersection. All of the site except the southernmost 523 units can gravity flow to this location. A connection to this main would require paving repair over the 600 foot distance to reach the site. A proposed 8" sanitary sewer main for the Autumnwood development could supplement or replace this Whitewood Drive main. The terminus of this Autumnwood main would be within 100 feet of Vista Boulevard. Use of this main by the D'Andrea development would require agreements between the developers.

East Prater Way has a 12" main which can be accessed at the Vista Boulevard intersection. The entire site can gravity flow to this location.

Vista Boulevard south of Prater Way has an 8" main which can be accessed in Vista Boulevard approximately 900 feet south of the Prater Way/Vista Boulevard intersection. The entire site can gravity flow to this location. The contributors to this main are predominantly industrial users. An advantage to connecting to this main is that the residential peak flows from the D'Andrea development would occur at different times than the industrial peak flows.

Proposed Onsite Sewer System

The proposed onsite sanitary sewer collection system within the development will consist of 8" diameter minimum SDR 35 pipes sized at minimum slopes of 0.40 percent. Most of the pipe slopes will be greater than 0.40 percent. Larger diameter pipes may be required depending on the alignment and flows. The City of Sparks Public Works Department indicated that the sewage flows should be derived from the procedure outlined in the City of Reno Public Works Design Manual dated March 2, 1992. The 8" sewer mains in the local streets of the development will be based on 350 gallons per capita per day peak flow. The trunk lines (10" and larger) will be based on 250 gallons per capita per day peak flow. Capacities will be based on 1/2 depth of flow for all mains and trunk lines. Each phase of construction will need to provide the offsite connections to serve the entire phase and sized to carry the flows from the ultimate development using that particular route. Tables 2-4 and 2-5 below show the onsite sewage discharge for each phase of the development. All sewer not on public property shall have access easement and road provided to the satisfaction of the Public Works Director.

Table No. 2-4 Estimated Project Design Discharge - 8" mains

Phase	Single-family units ¹	Multi-family units ²	Commercial Development (acres) ³	Peak Design Discharge (gpd)
Phase 1	1,224	310	20	1,702,200
Phase 2	522	0	0	548,100
Phase 3	174	0	0	182,700
Total	1,920	310	20	2,433,000

Table No. 2-5 Estimated Project Design Discharge - Trunk lines (10" and larger)

Phase	Single-family units ¹	Multi-family units ²	Commercial Development (acres) ³	Peak Design Discharge (gpd)
Phase 1	1,224	310	20	1,273,000
Phase 2	522	0	0	391,500
Phase 3	174	0	0	130,500
Total	1,920	310	20	1,795,000

Occupancy rates ¹ 3.0 capita / dwelling unit² 2.0 capita / dwelling unit³ 10,000 gal / acre / day

Phase 3 could have a 10-acre school site to replace 30 residential lots. A school site would generate 3,200 gpd/acre so a 10 acre site would generate 32,000 gpd. Thirty residential lots would generate 31,500 gpd. Therefore there would be no significant difference in sewage flows if the school option is implemented.

Proposed Offsite Sewer System

Table No. 2-6 below shows the half-depth capacities for each of the existing lines under consideration. Pipe sizes and slopes are from drawings on file at the City of Sparks. The capacities remaining in each line have been determined by flow tests.

Table No. 2-6 Half-depth Capacity

Line	Size (inches)	Slope(%)	Half-depth capacity (gpd)
Baring Blvd.	15"	0.3%	1,062,000
Del Rosa Way	10"	0.6%	510,000
Whitewood Drive	8"	0.4%	229,000
Autumnwood	8"	0.4%*	229,000
Prater Way	12"	0.264%	550,000
Prater Way	15"	0.1%	613,000
Prater Way	18"	0.1%	996,000
Vista south of Prater	8"	0.3%	199,000
Vista south of Prater	18"	0.118%	1,083,000
New main in Prater	18"	0.1%	996,000

* Assumed slope

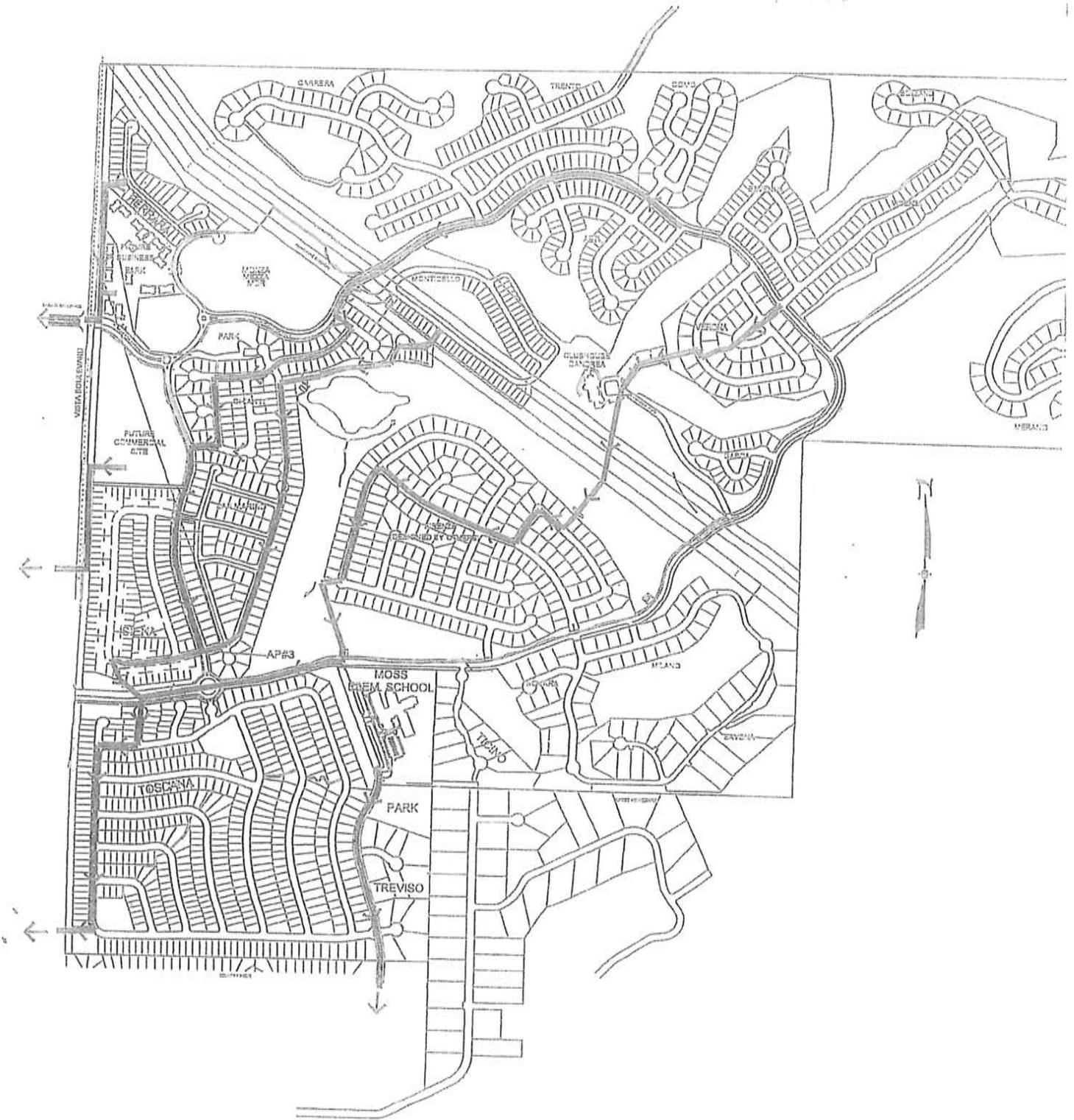


Figure 2-4 Sewerage

Existing Sanitary Sewer System Metering

Metering of the existing sanitary sewer system was performed by Hydro Tech, Inc and by the City of Sparks to determine the available capacity in the offsite trunk lines being considered. Table No. 2-6 shows the results of the metering.

The Baring Boulevard sewer system was metered by the City of Sparks in 2 locations. One test was done approximately 50 feet east of the eastern most Reed High School parking lot entrance, the other approximately 100 feet west of the western most Reed High School parking lot entrance. The peaks occurred between 7:00 am and 8:00 am on the weekdays and between 10:00 am and 11:00 am on the weekends. Table No. 2-6 shows the testing results.

The Whitewood Drive sewer system was metered in the cul-de-sac at Candlewood Street west of the intersection of Tangerine Street. The sewer is a 10" diameter main at this location. The peaks occurred at 7:00 am on the weekdays and between 9:00 am and 10:00 am on the weekends. The connection point for the D'Andrea development is at the eastern terminus of the 8" line near the existing school. This 8" line only has the existing school contributing to it at this location. A second access point for this system is through the proposed Autumnwood development. The Autumnwood development is proposing an 8" sanitary sewer which extends to within 100 feet of Vista Boulevard, across from the D'Andrea site. If proper agreements are in place this line may replace or supplement the Whitewood Drive access point. Table No. 2-6 shows the testing results.

The East Prater Way sewer system was metered in East Prater Way at the first manhole east of Sparks Boulevard. The sewer is an 18" diameter main at this location. The peaks occurred between 7:00 am and 8:00 am on the weekdays and at 10:00 am on the weekends. The flows measured during the non-peak hours did not drop significantly, ranging from 124 gpm to 138 gpm, indicating possible infiltration. The hospital which is contributing to this system could also account for the high off-peak flows. Table No. 2-7 shows the testing results.

The Vista Boulevard south of Prater Way sewer system was metered in Vista Boulevard near the signalized driveway for the industrial buildings, approximately 3,300 feet south of Prater Way. The sewer is an 8" diameter main at this location. The weekday peaks occurred at different times throughout the day; 11:00 am, 3:00 pm and 9:00 am. Table No. 2-7 below shows the hydraulic analysis of this system.

Additional metering was performed for the Vista Boulevard south of Prater Way sewer system at a location near the Sparks interceptor. The sewer is an 18" diameter main at this location. The peaks occurred between 12:00 and 1:00 pm on weekdays and at 10:00 am and 5:00 pm on weekends. The flows measured during non-peak hours did not drop significantly, ranging from 12.6 gpm to 21.9 gpm, indicating possible infiltration.

A new trunk line along Prater Way to the Sparks Boulevard interceptor will need to be constructed for any sewage that exceeds the capacity remaining in the available existing trunk lines. The impact of this development on the Prater Way interceptor will be assessed by the City of Sparks who will then determine if any mitigation measures will be needed. Southernmost connections will have less of an impact on the Sparks Boulevard interceptor.

Table No. 2-7 System Metering

Line	Size (inches)	Testing Period	Measured Peak Daily Flows (gpm)
Baring Blvd. (eastern location)	15"	3/13 to 3/17 (Fr to Tue)	Weekday 317.7 to 338.8 Weekend 359.8 to 393.9
Baring Blvd. (western location)	15"	3/17 to 3/24 (Tue to Tue)	Weekday 320.8 to 410.4 Weekend 339.4 to 348.0
Candlewood Street	10"	3/12 to 3/16 (Thurs to Mon)	Weekday 45.9 to 47.0 Weekend 44.8 to 44.8
East Prater Way	18"	3/19 to 3/23 (Thurs to Mon)	Weekday 270.0 to 366.5 Weekend 354.9 to 386.1
Vista Boulevard	8"	3/10 to 3/12 (Tue to Thurs)	Weekday 23.8 to 29.3
Boxington	18"	3/17 to 3/23 (Tue to Mon)	Weekday 58.7 to 59.6 Weekend 21.8 to 26.5

Based on the metering results the available capacity in each line is shown in Table No. 2-8. For conservative purposes the highest daily peak value of each test was used to determine available capacity.

Table No. 2-8 Available Capacity for Existing Sewer Mains

Line	Size of Line (dia)	Half-depth Capacity (mgd)	Existing Peak Flow as Metered (gpd)	Estimated Peak Flow at Connection (gpd)	Available Capacity (gpd)
Baring Blvd. east of Reed High School	15"	1,062,000	567,000	86,000	495,000
Baring Blvd. west of Reed High School	15"	1,062,000	645,000	86,000	417,000
Del Rosa Way	10"	510,000	see Baring Blvd.		
Whitewood Drive	8"	229,000	33,000	12,000	217,000
Autumnwood	8"	229,000	33,000	12,000	217,000
Prater Way	12"	550,000	556,000	211,000	339,000
Prater Way	15"	613,000	556,000	287,000	326,000

Prater Way	18"	996,000	556,000	556,000	440,000
Vista south of Prater	8"	199,000	42,000	0	157,000
Vista Blvd south of Prater at Interceptor	18"	1,083,000	86,000	0	997,000
New trunk line in Prater Way	18"	996,000	0	0	996,000

Recommendations

The recommended distribution of the sewage from the proposed development are summarized in Table No. 2-9. The new 18" trunk line will be slightly more than half depth (11.24") at peak flow however this line could be considered interceptor status so an 18" line should be adequate.

Table No. 2-9 Recommended Distribution Into the Existing Systems

Line	Size of Line (dia)	Available Capacity (mgd)	Recommended contribution from D'Andrea (units)
Banning Blvd.	15"	417,000	125 SFR from Phase 2
Whitewood Drive	8"	217,000	None
Autumnwood	8"	217,000	207 SFR from Phase 1
Prater Way	12"	326,000	434 SFR from Phase 1(temp)
Vista south of Prater	8"	157,000	150 SFR from Phase 1(temp)
New trunk line in Prater Way	18"	1,546,000*	1,588 SFR from Phases 1,2,3 310 Apartments Phase 1 20 Acres Commercial Ph. 1

* 62% depth

Conclusion

The D'Andrea development will utilize any available capacity in the existing sewage system lying westerly of the proposed project. A new offsite trunk line will be required when the capacity of the existing system has been maximized. The D'Andrea development should have no significant impact on the existing sanitary sewer system if the recommended measures, or equivalent measures, are implemented. Refer to Supplemental Appendices for full report.

The intent of this preliminary report is to indicate that sewer capacity is available in this area to serve the capacity requirements of this development. The final phasing plan, temporary and permanent connection points, design and construction shall be to the approval of the Public Works Director.

Stormwater Management And Grading

The storm waters will be channeled through the property through a series of detention basins. These basins will be incorporated into the design of the golf course and open space. The green space of detention areas will be maintained via the DCA. The final storm drainage and detention system will be designed with the golf course design. The grading of the property will be designed to be contour grading. This will minimize the use of cuts/fills and retaining walls with no slopes 2H: 1V, except for areas located entirely within the golf course, residential lots and/ or the DCA common areas subject to the approval of the Public Works Director and Administrator. The hillside analysis includes the protection of the significant hilltop ridgelines per the Sparks Municipal Code 20.99.

There is historic flooding at the intersection of Vista Boulevard and Prater Way, flooding entire sections of the street above the top of curb. This flooding has been identified as an area problem and is scheduled as a capital program improvement. The D'Andrea has agreed to participate in the capital improvements needed to remedy the flooding problem.

Storm drainage design criteria shall be in accordance with the Washoe County Draft Hydrologic Criteria and Drainage Design Manual as developed by W.R.C. Engineering, Inc., including amendments, revisions as well as modifications as deemed necessary by the Public Works Director.

Based upon this criteria, the discharge from the site and areas contributing to the southwest corner site in the existing condition is 287 cubic feet per second in the 5-year, 24 hour event and 1369 cubic feet per second in the 100-year 24 hour event. The increase in peak discharge due to the development of the site will be offset down to the capacity of the down stream storm drain system with the application of onsite detention basins, which is approximately 31 cfs.

Those items that are to be addressed, which may not be specifically addressed in the Drainage Design Manual, include the following:

Stormwater Master Plan and Phasing

A Stormwater Master Plan prepared by WRC Nevada, Inc. (May, 1990) is provided in the Supplemental Appendices of the Final Planned Development Handbook. The Master Drainage Plan identifies the existing condition, impacts to the existing condition, and mitigation with sufficient detail so as to identify the requirements of each individual unit.

As part of the Stormwater Master Plan, a Phasing Plan is provided which will identify the sequence of the construction of on-site and off-site storm drainage improvements as related to the overall site development. The approved phasing plan will be adhered to such that improvement plans will not be submitted for approval until key elements of the drainage system to the satisfaction of the Public Works Director are constructed. Development Agreement regarding storm drainage improvements that resolve hydrologic problems beyond boundaries of D'Andrea may receive credits for "regional improvements" that benefit the area, subject to the approval of City Council.

Temporary Drainage Facilities

Temporary drainage facilities that may be proposed shall be designed and constructed in accordance with the same criteria as the permanent facilities.

Drainage Modeling

Should the storm drainage capacity of the existing system be evaluated, to the extent possible, it shall be done following the same criteria. The goal is to limit stormwater discharge such that available capacity is not exceeded.

The minimum parameters of the capacity analysis of the existing storm water drain system are to include the North Truckee Drain and extend to the project area.

This will be accomplished through onsite detention to decrease 100-year discharges from the site to a flow rate that is equal to pipe capacity at the storm drain inlet currently located at the southwest corner of the project site. The detained flows will also arrive at a point in time that is later than the other contributing areas to the storm drain.

The drainage system will be designed such that there will be no adverse impacts to the surrounding area.

Pah Rah Canyon Drainage System

The northwestern portion of the D'Andrea site contributes to the Pah Rah Canyon storm drainage system. The master drainage study considers the effects of existing and proposed drainage on this system. The drainage master plan identifies approximately 35 acres of the Pah Rah watershed that will be diverted to the south through the detention basins in D'Andrea to reduce the existing flows into the Pah Rah Drainage.

Telephone/Cable Television/Wireless Communication

The project will be served by Nevada Bell for telephone service and by a cable service company. Services will be provided via Vista Boulevard.

Refuse Collection

D'Andrea will be served by Sparks Sanitation Company. Will serve letters will be required as part of the building permit process. All streets and parking areas shall be designed and constructed to accommodate refuse collection trucks. All trash dumpster enclosures shall be designed and constructed to the approval of the DCC, Sparks Sanitation Company and Administrator.

Public Facilities/Services

Schools

At present schools serving D'Andrea are Marvin Moss Elementary School, Mendive Middle School and Reed High School. Moss Elementary School and Mendive Middle School are contiguous to D'Andrea. Reed High School is located on Banning Boulevard approximately a mile from the property.

Table 2-10 Capacities and Enrollments

East Sparks	School Enrollment '97 - '98	School Capacity	Extra Capacity	With Multi-Track
Marvin Moss E.S.	525	624	99	830
Mendive Middle	906	950	44	1,264
Reed High	2,345	2,100	-245	N/A

Washoe County School District planning standards for an elementary school suggest that 9.4% of the household population will be elementary (K-6) school age children, 3.2 % will be middle school ages, and 5.5% will be high school ages. The school district estimates that a single family household averages 2.5 persons per household. Using 2.5 persons per household and 1,689 households since 511 households of the 2200 units will be active adults without children, the site specific population with children will be 4,222 persons.

Using Washoe County student projections, it is anticipated that this project will generate the following:

Table 2-11 School Demand Based on 1701 Dwelling Units and 2.5 Persons/DU

School Type	Student Population	Student Demand
Elementary	9.4%	397
Middle	3.1%	131
High	5.5%	232

Police and Fire

Police

D'Andrea is located within the city limits and is generally an infill project. This project will become part of the routine police patrol as it becomes part of the existing established community of Sparks.

Fire

The City of Sparks Fire Department will serve D'Andrea with Emergency fire protection in accordance with the laws of Nevada. The developer shall install residential fire sprinkler systems (NFPA 13D shall be the minimum standard) within those villages/subdivisions/developments where Fire Department average response times exceeds 8.5 minutes (the green areas as shown on the Fire Department's response time from Fire Station #2 exhibit) or where slopes exceed 12% gradient. D'Andrea Nevada LLC shall provide for the installation of traffic control device improvements (the addition of Opticom systems to existing traffic signals on Baring Boulevard from Fire Station #2 to the project's major entries, inclusive) to improve current Fire Department response times. The developer shall dedicate a 1.5 acre site to the City for the purpose of a City-constructed fire station to be located adjacent to the D'Andrea golf course maintenance facility between the existing utility easements (with the parking area located under the utility lines). If the City determines through its Master Plan update efforts that the D'Andrea fire station site is no longer needed, then the fire station site shall revert back to D'Andrea Nevada LLC or its successor. Construction of a fire station on the D'Andrea fire station site

may eliminate or reduce the 8.5 minute response time trigger for residential fire sprinklers within the D'Andrea project. However, all structures shall be subject to a fire prevention ordinance(s), including fire sprinklers, in effect at the time of application for building permit. Prior to the issuance of any building permit for construction of single family residences within the eleven villages of Phase III as identified in the Final Design Handbook Phasing Plan (Figure 1-7 Phasing Plan) (i.e. Carrera, Trento, Como, Asti, Verona, Garda, Ravenna, Vicenza, Bolzano, Cortina, and Merano) verification must be provided that one (1) Type 6 apparatus fire suppression vehicle has been ordered and purchased by the developer on behalf of the City of Sparks Fire Department. In combination, the developer must provide verification that a second Type 6 apparatus fire suppression vehicle has been ordered and purchased by the developer on behalf of the City of Sparks Fire Department prior to the issuance of any building permit for construction of single family residences within the villages of Cortina or Merano. These fire suppression vehicles are necessary for primary use in the steeper portions of the D'Andrea project area where Fire Department response time exceeds City standards.

A wildland fire zone is situated to the east of D'Andrea. With development of the easterly edge of D'Andrea, fuel modification and management plans to mitigate wildland fire concerns will be developed for review and approval by the Fire Department. These plans may include weed/dry vegetation abatement, construction of permanent fire lines, properly treating cut/fill slopes to minimize burn potential, and using fire retardant materials in buildings/structures as necessary. Also, access to the wildland areas will be improved through a roadway network and trailhead. Chapter 3, Design Standards, addresses wildland fire zone.

Parks and Trails

D'Andrea's golf course, parks, open space and trails will provide a variety of recreational opportunities. To meet established standards for park service areas (1/2 mile radius from homes), there needs to be park facilities or open space within 1/2 mile of each project. There will be public parks, and also a recreational center for use by DCA. The parks will be designed to approval by Parks and Recreation Director and constructed by the Master Developer as the earliest adjacent village is under development and dedicated to the City of Sparks as public parks. It is the intent of the Master Developer to receive credit towards the Residential Construction tax (park fees). There will be one park located adjacent to the existing Marvin Moss Elementary School that is approximately 3 acres in size. This park will have a mixture of passive and active facilities such as playground, open play areas and trails. There will be another park site approximately 3 acres at the northern entrance to D'Andrea which will contain similar park facilities as the southern park. Throughout the project there will be pocket parks within the villages and open space that will provide trails and lookout points. At the eastern boundary of D'Andrea will be a trailhead to provide a staging area for access onto the adjacent BLM lands. This trailhead will be constructed by the master developer and will be situated on the eastern edge of D'Andrea or on Bureau of Land Management property. D'Andrea will be connected by a network of trails/pathways providing meaningful recreational opportunities. Chapter 3 provides the details of the park concepts, trail conceptual plan and design, and the trailhead. The following will be maintained by DCA:

- Pocket parks
- Trailhead
- Open space
- Network of trails

CHAPTER 3

CHAPTER 3 - DEVELOPMENT STANDARDS

Introduction

The purpose and intent of the D'Andrea Handbook is to establish the standards for the development within D'Andrea. The design requirements yield designs that have a distinct identity, expressing an integration of the built and the natural environments. These standards provide the City of Sparks, the D'Andrea Community Association and the residents of D'Andrea with the necessary assurances that this community will develop in accordance with the highest quality and character.

Community Character

The community character of D'Andrea draws on a mixture of influences that reflect the Northern Italian, French Normandy, and Craftsman vernacular for additional variety. These distinct, yet analogous, styles of architecture are integrated within this master planned community to create a strong sense of identity and community through the use of harmonious architectural concepts, colors and materials.

The Northern Italian architectural style will be used for all the major community buildings (golf course clubhouse, recreation center, preview center and office/retail centers) along with the entry landscaping/monumentation and streetscapes. This will create overall harmony among the various individual neighborhoods/villages. The architecture and landscape style incorporate the elements of the Northern Italian with simple lines and materials, formal plantings, water features and symmetry. Northern Italian is the overall theme of D'Andrea. The theme was selected to honor the heritage of the property and the Northern Italian pioneers who settled in Sparks over one hundred years ago. D'Andrea will provide the flavor of Alpine Northern Italy with a wide variety of housing to suit most segments of residents in the Truckee Meadows. The other styles define elements that provide housing developers flexibility and a palette of design choices for a variety of housing styles.

Land Use Designations

Single Family Homes

Veranda (LMDR) Homes

Description:

Veranda homes are attached or detached single family units located on smaller lots that are in the form of clustered housing. As a benefit, parks, open space, the golf course or other recreational facilities, including the trail network, are provided either within or adjacent to the Veranda home projects. Yet, veranda home projects appeal to a large segment of home buyers due to low maintenance requirements, including active adult housing. Veranda homes will be located on the lower sections of the property or the areas with slopes less 10% slope.

- Lot Size:
- 3,200 sq. ft. minimum per dwelling unit.
- 3,700sq.ft.minimum for corner lots.

Allowed Uses:

- Single-family dwellings of a permanent nature.

- Zero or z-lot line developments.
- Temporary sales office and model homes and temporary construction yard.
- Condominium(s).
- Duplex/Townhome(s)
- Common drive cluster homes (8 lots served by a common driveway).
- Structural additions to the original house and any other structure, upon review and written approval of the entire project by the D'Andrea Community Association, providing all D'Andrea Design Handbook Standards are met prior to submittal to City for building permits.
- Accessory structures are limited to 48 square feet with a maximum height of 6 feet.

Prohibited Uses:

- No recreational vehicle, boat or trailer storage is allowed outside of the garage. Pickup trucks with campers or vans that serve as primary transportation, are exempted from this provision.
- Garages may not be converted into living space and must retain vehicles parking space.
- No other uses are allowed.

Note: Above provisions will be included with the disclosure statement at the time of sale and resale of each house.

Parking:

- One off-street parking space will be provided per bedroom.
- Minimum lot width and depth
- Minimum lot width: 35 feet
- Minimum lot depth: 90 feet
- Minimum lot frontage: 35 feet

Setbacks for Detached Units (Refer to Figure 3-1, Yard Definitions/Setbacks):

- Front Yard: 10' from property line; 18' minimum from property line to garage/carport.
- Side Yards: 5' with a minimum 10' between structures.
 - Exterior Side Yards (corner lots) 10' from property line.
- Rear Yards: 10'
- Minimum Rear Yard: 400 square feet

Setbacks for Attached Units (Refer to Figure 3-1, Yard Definitions/Setbacks):

- Front Yard: 10' from property line; 18' minimum from property line to garage/carport.
- Side Yards: 0' with a minimum of 10' between structures.
 - Exterior side yards (corner lots) 10' from property line.
- Rear Yards: 10'
- Minimum Rear Yard: 400 square feet

Projections into Required Setbacks:

- Cornices, canopies, eaves, chimneys or similar architectural features may extend a maximum of 2 1/2 feet into a required front or rear yard setback and are not to exceed 25% of facade with architectural feature.

Variation in Elevations:

- With the detached homes, no identical (like or mirrored) front elevations may be repeated on adjacent lots facing the street. Adjacent lots may share the same floor plan, but must have different elevations.

- Variation in Setbacks:
- Each adjacent home must vary in average front yard setback as follows: 2' minimum while complying with minimum setbacks or provide sufficient architectural articulation such as varying elevation projections such as porches, side loaded garages, house forward of garage, courtyards, etc. Streetscapes must vary in perceived front yard setback by varying elevation projections such as porches, side loaded garages, house forward of garage, courtyards, etc., by landscape treatments or by curvilinear streets.
- Height Limitation: Three stories. (35 feet)
- Fencing Requirements: See Hardscape, Fences and Walls.
- Other:
- Any standards not addressed above will comply with City of Sparks, SMC Title 20.74 (R3).
- If cluster homes design restricts fire truck access, the units beyond allowable distance as determined by City of Sparks Fire Department, these units shall have an NFPA 13D fire sprinkler system installed that is locally monitored to the satisfaction of the Fire Chief.

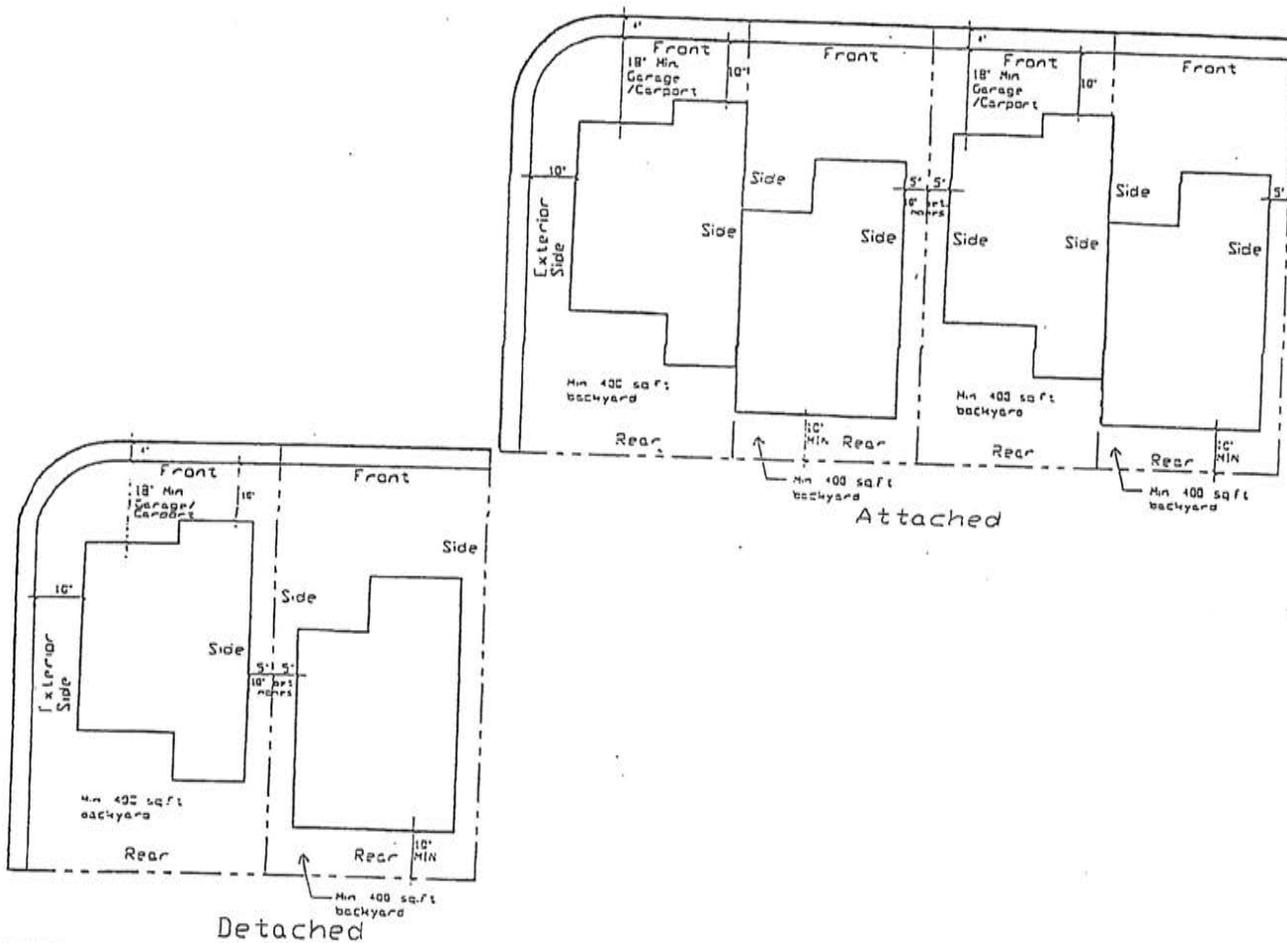


Figure 3 - 1 Yard Definitions/Setbacks for Veranda Homes

Villaggio (LDR3) Homes

Description:

Villaggio lots provide intermediate housing type between the Vicinato and the Veranda homes. These lots will be designed with proximity to parks, open space, the golf course or other recreational facilities including the trail network.

- Lot Size:
 - 4,500 sq. ft. minimum interior lots
 - 5,000 sq. ft. minimum corner lots
- Allowed Uses:
 - Single-family dwellings of a permanent nature.
 - Accessory uses customarily incident to the above uses, located on the same lot, including tool/storage shed, garden house, private workshop and private greenhouse (75 square feet maximum floor area, 8' maximum height).
 - Temporary sales office and model homes and temporary construction yard.
 - Children's playhouse (75 sq. ft. maximum floor area, 8' maximum height).
 - Structural additions to the original house and any other structure, upon review and written approval of the project by the D'Andrea Community Association (providing all D'Andrea Design Handbook Standards are met prior to submittal to City for building permits).
- Prohibited Uses:
 - No recreational vehicle, boat or trailer storage is allowed outside of the garage. Pickup trucks with campers or vans that serve as primary transportation, are exempted from this provision.
 - Garages may not be converted into living space and must retain vehicle parking space.
 - No other uses are allowed.

Note: Above provisions will be included with the disclosure statement at the time of sale and resale of each house.

- Parking:
 - One off-street parking space will be provided per bedroom.
- Minimum lot width and depth:
 - Minimum lot width: 45 feet
 - Minimum lot depth: 100 feet
 - Minimum lot frontage: 35 feet
- Setbacks Refer to Figure 3-2, Yard Definitions/Setbacks:
 - Front Yard: 15' from property line; 18' minimum from property line for garages/carports.
 - Side Yards: 5' with a minimum of 10' between structures.
 - Exterior Side Yards (corner lots) 10' from property line adjacent to street.
 - Rear Yards: 15'
 - Rear yard setbacks may be reduced to 10 feet in order to allow for a second story deck on lots that are adjacent to the golf course, public or private right of ways, or dedicated open space.
 - Minimum Rear Yard: 675 square feet
- Projections into Required Setbacks:
 - Cornices, canopies, eaves, chimneys or similar architectural features may extend a maximum of 2 1/2 feet into a required front and rear yard setback and are not to exceed 10% of facade with architectural feature.

Variation in Elevations:

No identical (like or mirrored) front elevations may be repeated on adjacent lots facing the street. Adjacent lots may share the same floor plan, but must have different elevations.

Variation in Setbacks:

Each adjacent home must vary in average front yard setback as follows: 2' minimum while complying with minimum setbacks or provide sufficient architectural articulation such as varying elevation projections such as porches, side loaded garages, house forward of garage, courtyards, etc.

Height Limitation: Two stories (30 feet).

Fencing Requirements: See Hardscape, Fences and Walls.

Other:

Any standards not addressed above will comply with City of Sparks, SMC Title 20.60 (R2).

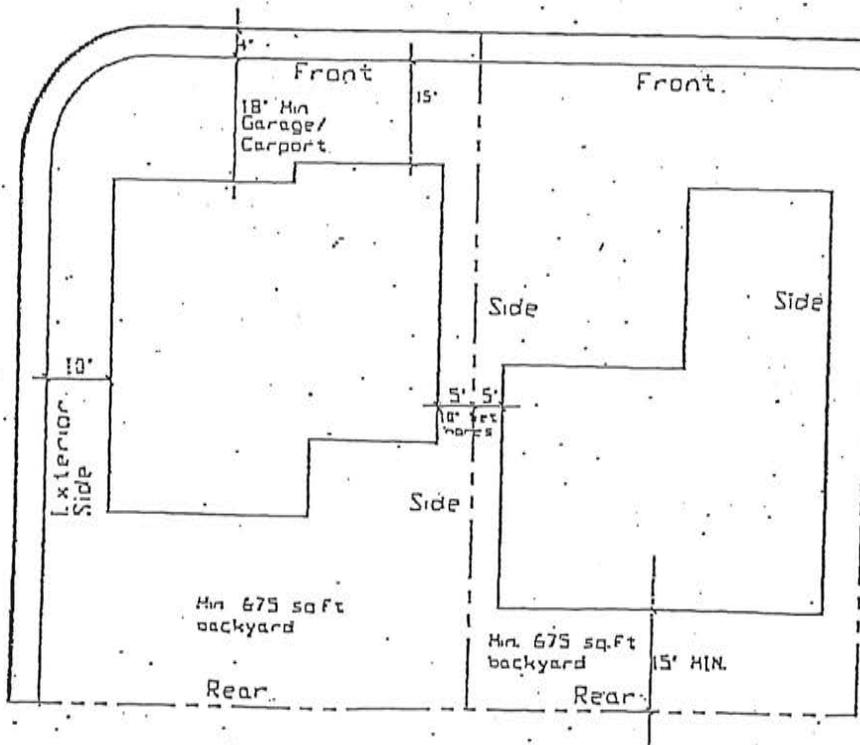


Figure 3-2 Yard Definitions/Setbacks for Villaggio Homes

Vicinato (LDR2) Homes

Description:

Vicinato lots provide intermediate housing type between the higher density Villaggio homes and the Villa lots. These lots will be designed with a proximity to public open space and recreational facilities.

Lot Size:

- 6,000 sq. ft. minimum interior lots
- 7,000 sq. ft. minimum corner lot

Allowed Uses:

- Single-family dwellings of a permanent nature.
- Accessory uses customarily incident to the above uses, located on the same lot, including tool/storage shed, garden house, private workshop and private greenhouse (75 square feet maximum floor area, 12' maximum height).
- Temporary sales office and model homes; temporary construction yards.
- Children's playhouse (75 sq. ft. maximum floor area, 8' maximum height).
- In home child care for the number of children legally permitted with one care giver.
- Structural additions to the original house and any other structure, upon review and written approval of the project by the D'Andrea Community Association providing all D'Andrea Design Handbook Standards are met prior to submittal to City for building permits.
- Home occupation in accordance with Sparks Municipal Code.

Prohibited Uses:

- No recreational vehicle, boat or trailer storage is allowed outside of garages. Pickup trucks with campers or vans that serve as primary transportation, are exempted from this provision.
- Garages will not be converted into living space and must maintain vehicle parking space.
- No other uses are allowed.

Note: Above provisions shall be included with the disclosure statement at the time of sale and resale of each house:

Parking:

- One off-street parking space will be provided per bedroom.

Minimum Lot Width and Depth:

- Minimum Lot Width: 60 feet
- Minimum Lot Depth: 100 feet
- Minimum Lot Frontage: 35 feet

Setbacks Refer to Figure 3-3 Yard Definitions/Setbacks:

- Front Yard: 15' from property line; 18' minimum from property line for garages/carports.
- Side Yards: 7.5' with a minimum of 20' between structures.
 - Special Condition: Side yard setback can be reduced to 5' with 15' between structures if approved by DCC, Planning Commission and City Council, provided that there is a variety of approved architectural styles within the subdivision.
 - Exterior Side Yards (corner lots) 15' from property line adjacent to street.

Rear Yards: 20'

- Rear yard setbacks may be reduced to 10 feet in order to allow for second story deck on lots that are adjacent to the golf course, public or private right of ways, or dedicated open space.

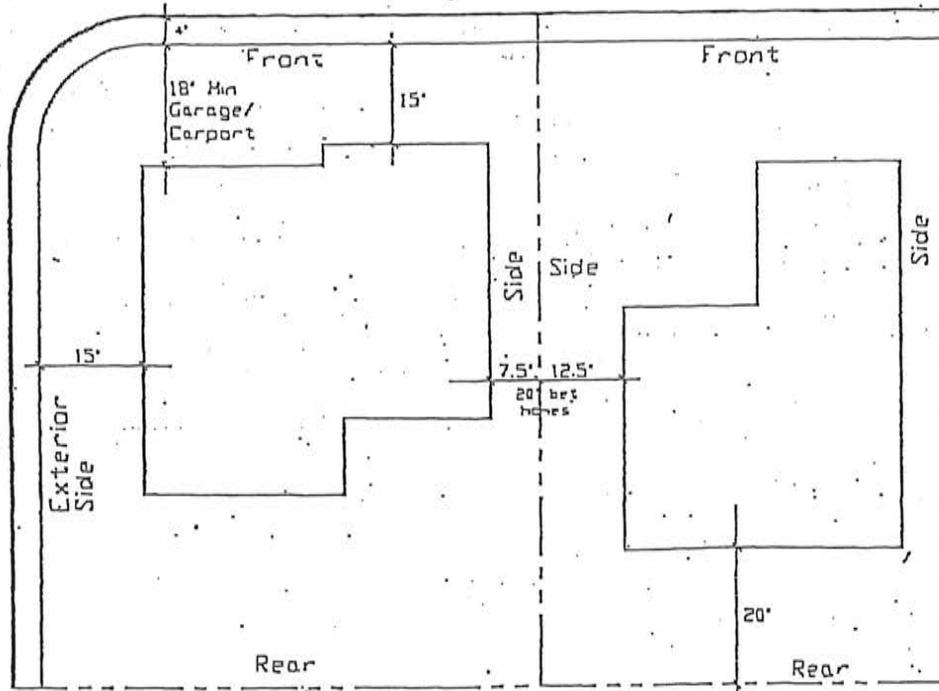
Projections into Required Setbacks:

- Cornices, canopies, eaves, chimneys or similar architectural features may extend a maximum of 2 1/2 feet into a required front, rear and 10' side yard setbacks and are not to exceed 10% of facade with architectural feature.

Accessory Buildings:

- Rear & side setback: Same as required side and rear setbacks for house.

- Maximum height: 12'
- Minimum distance from main structure: 10 feet
- Building shall be approved by D'Andrea Community Association prior to submittal of a building permit to City of Sparks.
- Variation in Elevations:
 - No identical (like or mirrored) front elevations may be repeated on adjacent lots facing the street.
 - Adjacent lots may share the same floor plan, but must have different elevations.
- Variation in Setbacks:
 - Each adjacent home must vary in average front yard setback as follows: 2' minimum while complying with minimum setbacks or provide sufficient architectural articulation such as varying elevation projections such as porches, side loaded garages, house forward of garage, courtyards, etc.
- Height Limitation: Two stories (30 feet).
- Fencing Requirements: See Hardscape, Fences and Walls.
- Other:
 - Any standards not addressed above will comply with City of Sparks, SMC Title 20.60 (R1-6).



Special condition 5' & 10' side setback with 15' bet structures permissible if approved by DCC & City Council

Figure 3-3 Yard Definitions/Setbacks for Vicinato Homes

Villa (LDRI) Homes

Description:

Villas provide a level of housing type between the Vicinato homes and the Bella Vista residences. These lots will provide a larger home and lot for families, the "move-up" housing.

Lot Size:

- 7,000 sq. ft. minimum interior lots
- 7,500 sq. ft. minimum corner lots

Allowed Uses:

- Single-family dwellings of a permanent nature.
- Accessory uses customarily incident to the above uses, located on the same lot, including tool/storage shed, garden house, private workshop and private greenhouse (150 square feet maximum floor area, 12 feet maximum height).
- Temporary sales office and model homes and temporary construction yard.
- Children's playhouse (75 sq. ft. maximum floor area, 8' maximum height).
- In home child care for the number of children legally permitted with one care giver.
- Structural additions to the original house and any structures, upon review and written approval of the project by the D'Andrea Community Association, providing all D'Andrea Design Handbook Standards are met prior to submittal to City for building permits.
- Home occupations per City requirements.

Prohibited Uses:

- No recreational vehicle, boat or trailer storage is allowed outside of the garage. Pickup trucks with campers or vans that serve as primary transportation, are exempted from this provision.
- Garages will not be converted into living space and must maintain vehicle parking space.

Note: Above provisions will be included with the disclosure statement at the time of sale and resale of each house.

Parking:

- One off-street parking space will be provided per bedroom.

Minimum Lot Width and Depth:

- Minimum Lot Width: 65 feet
- Minimum Lot Depth: 100 feet
- Minimum Lot Frontage: 35'

Setbacks Refer to Figure 3-4 Yard Definitions/Setbacks:

- Front Yard: 15' from property line; 20' minimum from property line for garages/carports.
- Side Yards: 7.5' or 20' between structures.
 - Exterior Side Yards (corner lots) 15' from property line adjacent to street.
- Rear Yards: 20'
 - Rear yard setbacks may be reduced to 10 feet in order to allow for a second story deck on lots that are adjacent to the golf course, public or private right of ways, or dedicated open space.
- Projections into Required Setbacks:
 - Cornices, canopies, eaves, chimneys or similar architectural features may extend a maximum of 2 1/2 feet into a required front, side or rear yard setback and are not to exceed 10% of facade with architectural feature.

Accessory Buildings:

Rear and side yard: Same as main structure

Maximum height: 12 feet

Minimum distance from main structure: 10 feet

Building shall be approved by D'Andrea Community Association prior to submittal to City for building permit.

Variation in Elevations:

No identical (like or mirrored) front elevations may be repeated on adjacent lots facing the street.

Adjacent lots may share the same floor plan, but must have different elevations.

Variation in Setbacks:

Each adjacent home must vary in average front yard setback as follows: 2' minimum while complying with minimum setbacks or provide sufficient architectural articulation such as varying elevation projections such as porches, side loaded garages, house forward of garage, courtyards, etc.

Height Limitation: Two stories (30 feet).

Fencing Requirements: See Hardscape, Fences and Walls.

Other:

Any standards not addressed above will comply with City of Sparks, SMC Title 20.61 (R1-7).

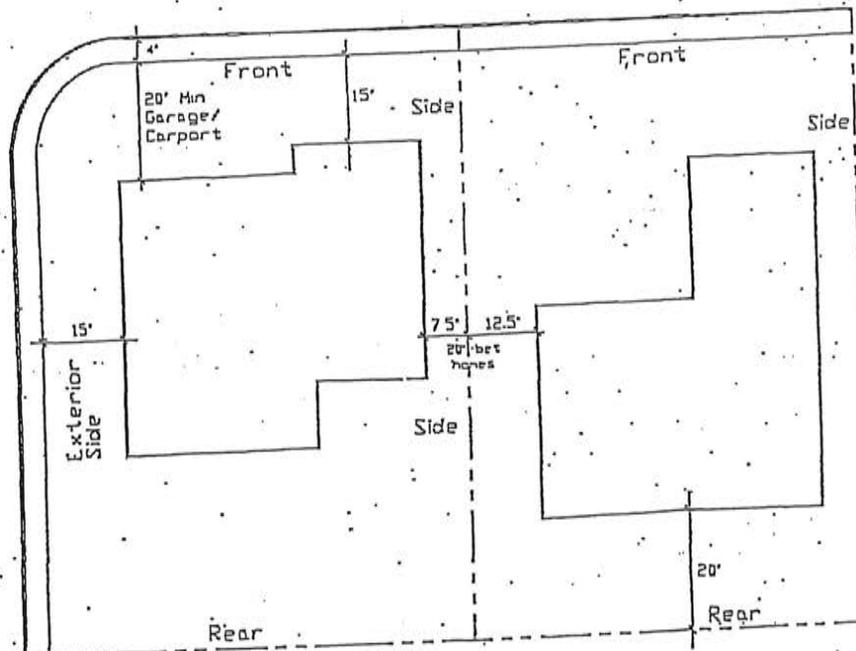


Figure 3-4 Yard Definitions/Setbacks for Villa Homes

Bella Vista (EDR) Homes

Description:

Bella Vista Homes provide custom and/or production lots for the individuals and/or families that desire to construct their personalized home style. Bella Vista residences will be located on the largest lots within the community that are typically located in the areas of most dramatic terrain and command the best views. These lots are removed from the village center and are larger due to the adaptive hillside grading and house sitting principles required in the design. The development on slopes will be reviewed through tentative maps and special use permit for the slopes.

- Lot Size:
- 10,000 sq. ft. minimum lots
- One acre minimum lots where abutting adjacent one acre lot subdivisions and/or where slopes require larger lots.

Allowed Uses:

- Single-family dwellings of a permanent nature.
- One detached guest building or "Granny Flat" (400 square foot maximum floor area, maximum height of 2 stories or 24 feet).
- Accessory uses customarily incident to the above uses, located on the same lot, including a private garage, tool/storage shed, garden house, private workshop, and/or private greenhouse (200 square foot maximum floor area, 12 feet maximum height).
- Temporary sales office and model homes and temporary construction yard.
- Children's playhouse (75 sq. ft. maximum floor area, 8' maximum height).
- In home child care for the number of children legally permitted with one care giver.
- Structural additions to the original house and any other structure, upon review and written approval of the project by the D'Andrea Community Association, providing all D'Andrea Design Handbook Standards are met prior to submittal to City for building permit.
- Home occupation per City regulations.

Prohibited Uses:

- Keeping of horses and/or other live stock.
- No recreational vehicle, boat or trailer storage is allowed outside of the garage. Pickup trucks with campers or vans that serve as primary transportation, are exempted from this provision.
- Garages will not be converted into living space and must be maintain vehicle parking space.
- No other uses are allowed.

Note: Above provisions will be included with the disclosure statement at the time of sale and resale of each house.

- Parking:
- One off-street parking space will be provided per bedroom.

- Minimum Lot Width and Depth:
- Minimum Lot Width: 85 feet
- Minimum Lot Depth: 120 feet
- Minimum Lot Frontage: 35 feet

Front Yard: 30' from front property line on slopes less than 4% slope.

20' from front property line on slopes greater than 4% slope.

The Village of Savona will be designed with minimum 20' front yard setbacks regardless of slope.

Side Yards: 10'

- Exterior Side Yards (corner lots) 15' from property line, adjacent to street.

Rear Yards: 30'

- Rear yard setbacks may be reduced to 20 feet in order to allow for a second story deck on lots that are adjacent to the golf course, public or private right of ways, or dedicated open space.

Projections into Required Setbacks:

Cornices, canopies, eaves, chimneys or similar architectural features may extend a maximum of 5 feet into a required setback and are not to exceed 5% of facade with architectural feature.

Accessory Buildings:

Rear and side yard: Same as main structure.

Maximum height: 12'

Minimum Distance from main structure: 10 feet

Building shall be approved by D'Andrea Community Association prior to submittal to City for building permit.

Variation in Setbacks:

Each adjacent home must vary in average front yard setback as follows: 2' minimum, while complying with minimum setbacks or provide sufficient architectural articulation such as varying elevation projections such as porches, side loaded garages, house forward of garage, courtyards, etc.

Height Limitation: Two stories (35 feet).

Fencing Requirements: See Hardscape, Fences and Walls.

Urban/Wildland Interface Zone - where village is adjacent to the transitional plant palette area as shown in Figure 3-6 and 1 acre or larger in size, the homeowner will utilize setback, landscaping and maintenance standards as listed in the Wildfire Protection Standards, refer to Page 3-40.

Other:

Any standards not addressed above will comply with City of Sparks, SMC Title 20.71 (RI-9).

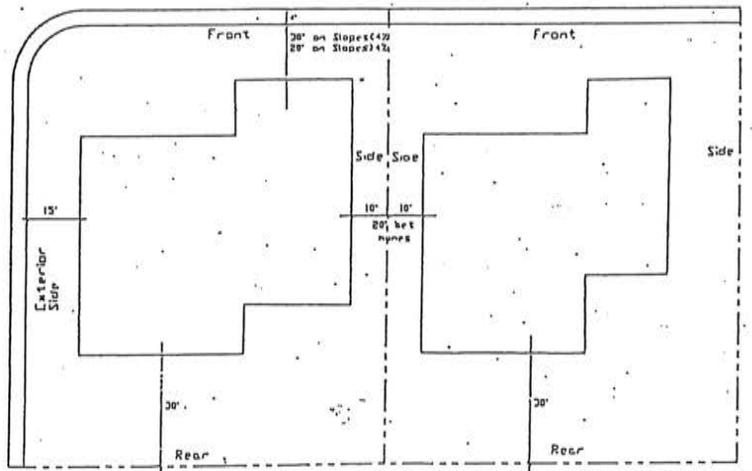


Figure 3-5 Yard Definitions/Setbacks for Bella Vista Homes

Mezzo (MDR-Multi-Family) Housing

Description:

The Mezzo multi-family homes will be located in the village center, northeast of the commercial/office land uses. This is strategically located within walking distance of the commercial uses and central park. The Mezzo multi-family homes will be subject to review and approval through the site plan review process by the City. The Planning Commission shall review the proposed village as a general business item (without public hearing) for comments and recommendations prior to submittal for site plan review to the City.

Permitted Uses:

- Apartments, condominiums, town homes.
- Temporary sales offices for condominiums and/or town houses.
- Manager's office accessory to the above use.
- Accessory structures incidental to the above uses.
- Temporary construction yard.

Prohibited Uses:

All uses not specifically identified above.

Amenities:

- An amenity package will be provided which will include laundry facilities (if not provided in the units), and a minimum of 100 square feet of usable open space (i.e. landscape area) per unit. A community room, exercise room, picnicking facilities, common barbeque areas, and other optional amenities (basketball courts, tennis courts, swimming pool, etc.) will also be provided for projects. The amenities provided will be appropriately sized for the project to the approval of the Parks and Recreation Director.

Lot Characteristics:

- Maximum Density: 18.0 units per acre.
- Maximum Units per Building: 32
- Min. Distance between Buildings: Any 1-story building, a minimum of 15 feet between adjacent buildings, any 2-story building, a minimum of 20 feet between adjacent buildings, any 3-story building, a minimum of 25 feet between adjacent buildings.
- Front Yard Setback: 20 feet from property line; 10 feet from parking lot
- Side Yard Setback: 20' at project perimeter, 10 feet from parking lot.
- Rear Yard Setback: 20' at project perimeter, 10 feet from parking lot.
- Maximum Height: 3 stories (45' feet)
- Parking Required: Per Sparks Municipal Code. One (1) covered parking space per unit is required; garages in addition to or in lieu of covered parking may be proposed to the approval of the DCC and Administrator or his designee at the time of the Site Plan Review for the village.

Landscaping Required:

- Landscape Area: 20% of site area:
- 1 tree per 300 square feet of required landscape area.
- 1 tree per every 7 required parking stalls.
- Turf areas will be required surrounding project amenity areas. Turf areas may not constitute more than 80% of required landscape area.

Special Conditions:

- Units must be buffered from abutting commercial uses and single family uses. Refer to buffering requirements in Chapter 3, Landscape.
- All units shall have porches, balconies, or patios.
- A landscape area of 10 foot minimum width must separate building and parking areas except for access aisles, utility pads and building encroachments such as porches or patios.
- Barbecues are not permitted within units, on porches, patios, decks or balconies.

Other:

- Any standards not addressed above will comply with Sparks Municipal Code 20.77 (R4).

- Apartment buildings with 16 units or more or three (3) stories shall in height require NFPA 13R fire sprinkler system that is locally monitored to the satisfaction of the Fire Chief.

General Commercial

Description:

The General Commercial is located in the northern portion of the property along Vista Boulevard. It will function as the activity center and is located within walking distance of the most intense residential areas. The General Commercial structures (main and accessory) will be subject to review and approval through the Site Plan Review process by the City.

Permitted Uses:

- Grocery and/or drug store, single uses not to exceed 70,000 square feet.
- Retail uses, single uses not to exceed 60,000 square feet.
- Specialty retail shops (jewelry, compact discs, books, craft, antiques, pets, etc.).
- Professional/medical offices.
- Restaurants.
- Educational centers/schools.
- Specialty food stores.
- Beauty salon/barbers.
- Travel Agency.
- Dry Cleaners, no cleaning on-premise.
- Bakery, retail (baking on-premises with all baked goods sold at retail on premises).
- Quick copy establishments.
- Office supply store.
- Car washes.
- Temporary construction yard.

Conditional Uses (subject to Special Use Permit approval by the City).

- Mini-market with fuel pumps.
- Child care facilities (in compliance with Washoe County Social Services, Child Care Services Regulations).
- Fast food establishments.
- Laboratories.
- Mini-storage.
- Public utility structures.
- Private Recreation Facilities/Fitness Centers.
- Gas Station.
- Any business whose hours of operation exceed seventeen (17) hours per day.

Prohibited Uses:

- Automotive repair establishments.
- All uses not specifically identified above.

Signage:

- Refer to Chapter 3, Hardscape, Signs.

Special Conditions:

- Commercial loading areas will be completely screened from adjacent streets and any abutting residential uses.
- Landscape strips a minimum of 10' in width must be provided between parking lots and streets, and 5' between parking lots and buildings. Refer to Buffering Requirements in Chapter 3, Landscape.

All roof-mounted equipment will be screened from adjacent properties and public rights-of-way.

Lot Characteristics:

- Maximum building coverage: 30%
- Maximum Floor Area Ratio: 75% (for 3 stories)
- Front Yard Setback: 10 feet
- Side Yard Setback: 10 feet, 20 feet if adjacent to residential with buffering.
- Rear Yard Setback: 10 feet, 20 feet if adjacent to residential with buffering.
- Building setback from internal/interior property lines: 0 feet
- Maximum Building Height: 35 feet with architectural embellishments exceeding 35 feet, to maximum of 55 feet.
- Minimum Building Separation: 0 feet
- Loading Dock Areas: Screened from public streets and designed to not be oriented towards residential development.

Parking Required:

- Retail/Commercial: 1 space per 200 s.f. of leasable area
- Professional Offices: 1 space per 250 s.f. of leasable area
- Medical Offices: 1 space per 200 s.f. of leasable area
- Restaurants, Bars: 1 space per 100 s.f. (for restaurants with greater than 20 seats. For less than 20 seats, 1 space per 200 s.f.)

Landscaping Required:

- 15% of site area:
 - 1 tree per 500 square feet of required landscaping.
 - 1 tree per every 10 required parking stalls.
 - Lawn cannot exceed 85% of required landscaping.
- Refer to Chapter 3, Landscape.

Other:

- Any conditions not addressed above will comply with Sparks Municipal Code 20.85 (C2).

Preview Center

The Preview Center will be located within the office/commercial complex off of North D Andrea Parkway. The Preview Center will feature the Northern Italian architectural theme and will be used for orientation of visitors to the community, for marketing and the sale of custom lots. The Preview Center will be approximately 1,500 square feet with at least 10 parking spaces. It is contemplated that the Preview Center will be converted to general office uses at some time in the future. The Preview Center will be subject to review and approval by the City through the Site Plan Review prior to submittal of building permit. Refer to Architectural Standards for building elevations on Figure 3-57.

Golf Course Clubhouse

The Clubhouse will be located on Milano Drive off of South D Andrea Parkway. It will be the home of the pro shop, bar and grill, tournament dining facilities, fitness center, and cart barn. The Golf Course Clubhouse shall be built as part of the Phase I construction of the golf course. It is currently planned to be two stories with the cart barn on the lower level. The Golf Course Clubhouse will be approximately 17,600 square feet with the required number parking spaces per Sparks Municipal Code Standards for uses proposed within the Clubhouse. The Clubhouse will be subject to review and approval by the City through Site Plan Review prior to submittal of building permit. The Golf Course Clubhouse will be fire sprinklered with NFPA 13 monitored system to the

satisfaction of the Fire Chief, if determined necessary. Refer to Architectural Standards for building elevation on Figure 3-55.

Maintenance Facility

This facility will be used for the storage, staging and repair functions for the golf course and the landscaping within the project. This facility will have offices, work areas for the grounds crew and storage for equipment and materials. The Maintenance Facility will be located between the power line easement and near the tee box of the eighth hole. The facility will be screened by landscaping and fencing. The Maintenance Facility will be subject to review and approval by the City through Site Plan Review prior to submittal of building permit.

Landscape

D'Andrea encompasses 861± acres, of which about ±179.19 acres will be for an 18-hole championship golf course, and about ±204.51 acres will be to parks and open space. Located on the eastern foothills of the City of Sparks, the majority of the property has spectacular views of the entire Truckee Meadows including the Sparks community, Reno skyline, the Sierra Nevada mountain range and other mountains surrounding the Truckee Meadows. The site ranges in elevation from 4,402 to 5,145 feet above sea level. An assortment of terraces, knolls, gentle slopes, rock outcroppings, and drainage ways provides an array of interesting and unique settings.

D'Andrea's has two major focal points: a village center and a championship golf course. The golf course's central location provides a core of lush green interconnected by a network of a natural open space and trail systems and will be one of the projects main attractions.

D'Andrea will portray a Northern Italian theme consisting of rows of trees, hedge plantings, formal planting, and "ornaments of nature" (i.e. Trellis, pergolas, arbors with plant material trained against them). The goals for D'Andrea's open space/landscape include: providing a community "sense of place", creating a design integrative and cohesive with the surrounding environment; and providing features that create interest and function to the community.

D'Andrea's landscape standards address streetscapes, major entries and intersections, open space and trail systems, site grading principles, commercial areas, and residential areas.

General Standards

Resource efficient landscape principles outlined in the City of Sparks Code, Chapter 20.32, are incorporated into these general standards. All common areas, including pedestrian easements, streetscapes, open space, "commons," trails and detention basins will be maintained by the D'Andrea Community Association, while actual flood conveyance system are maintained by the City. The following standards apply to all landscape areas. Standards specific to distinct areas within the project are described later in this chapter.

- Residential Landscaping - Developers of the predesigned housing products shall install landscaping and irrigation in the front yards of each home in their projects and stub out irrigation to the rear yard of each home.

Planting

- Plants will be selected by the developer and approved by the D'Andrea Community Association, the Parks and Recreation Director, and the Community Director prior to submittal of tentative map or Site Plan Review for the Village.
- Plant species selected will be those that are tolerant of the environment in which they will be grown including salinity, alkalinity, soil/water characteristics, soil physical properties, drainage and proneness to flooding, water tables, and any other influential factor.
- All landscaped areas will be maintained in a neat and attractive condition at all times. Minimum requirements include replacing dead or dying plant materials, mowing, watering and general clean-up.
- In areas where existing vegetation is to be retained, the overall forms of any introduced plant material will be complementary to the existing on-site vegetation.
- Species of plants will be massed to provide a simple, uncluttered look.
- Where screening is needed, plants that provide effective year-round screening, such as evergreens that branch close to the ground, will be used.
- Where shade is needed, plants with broad canopies, such as large deciduous trees, will be used.
- Plant species that produce objectional fruit drop will not be located adjacent to paved areas.
- Plants will be grouped with those that have similar growing requirements.
- Water conservation is an objective. Plant species that can survive on low to moderate amounts of irrigation will be used except in developed areas where a lush, high density appearance is desired.
- Plant materials selected will contain a combination of fast, medium and slow growth rates. Fast growth plants adapt quickly, provide quick cover, but have a relatively short life span and are sometimes subject to disease. Medium growth plants take over as the faster plants begin to die out, usually after 15-20 years. They are generally more attractive and less subject to disease. Slower growing plants remain small for a longer period of time, but tend to be more resistant to disease, longer-lived and are less prone to wind breakage.
- In informal areas, a variety of sizes will be planted to provide a more natural appearance.
- Use plants for micro-climate control where practical.
 - Use deciduous trees to shade the south and west sides of buildings, parking lots, and streets
 - Use evergreen trees for windbreaks
 - Use evergreens to insulate the north side of buildings.
 - Use turf for cooling around intensively used areas

Naturalistic grading will be used where complex recontouring and revegetation must occur. Continuous expanses of landforms will be created to look natural as opposed to contrived or manmade. All graded slopes shall be landscaped including within the individual residential lots by the homeowner of these lots.

Use of ground cover is preferred for landscape over other methods.

Irrigation

- Irrigation systems will be water efficient and low maintenance.
- Provide adequate water to establish and maintain landscape plantings and promote water conservation.
- All developed areas will have automatic irrigation systems.
- Transitional areas may be temporarily irrigated until vegetation is established. Temporary irrigation will remain in use until vegetation is well established and can survive without irrigation.
- Water for irrigation will come from an effluent or domestic water supply. If an effluent system is used, it will meet District Health Department requirements as well as State Health Regulations.
- Irrigation systems will be designed to provide complete and adequate coverage (taking into consideration wind patterns and other disruptive factor(s) while using water conserving methods.

Streetscapes

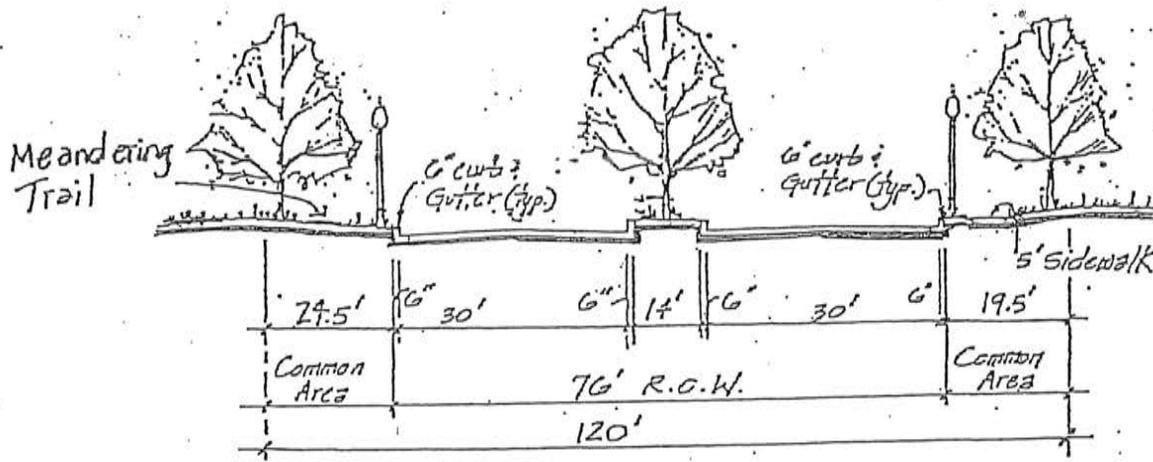
Streetscaping within D'Andrea will reflect the surrounding level of development. Landscaping, lighting and fencing in D'Andrea will be of a unified or complimentary design, providing a continuous Northern Italian theme to the entire project. Residential neighborhoods and the Village Center will feature tree lined streets. Streetscaping within the more developed areas will feature a more formal palette of ornamental and native species. Streetscaping adjacent to the natural open space will complement and enhance the existing plant community. Refer to Appendices. Streetscapes will be maintained by DCA. Refer to Figure 3-7 Street Cross Sections.

General Streetscape Standards

All streetscaping will conform to the following standards. Standards specific to different types of streets are described below.

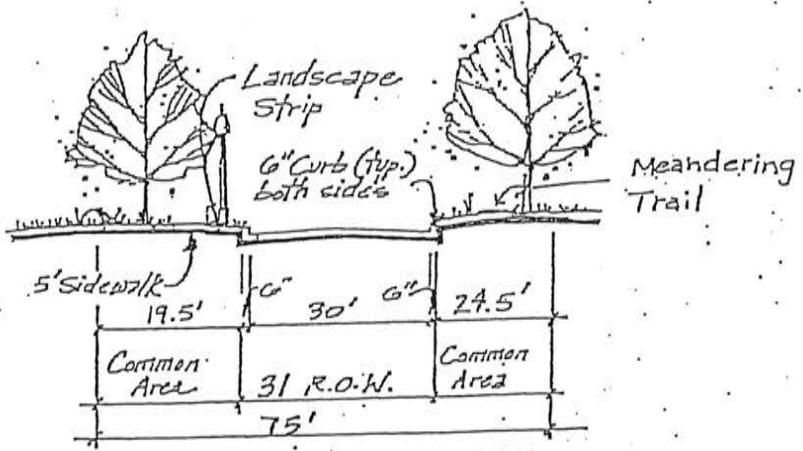
- Streetscaping will be installed as a part of roadway construction to provide a continuous landscape along streets regardless of the development phasing of individual parcels.
- As a part of final map approval, Right of Way Improvement Plan approval and Building Permit approval, engineering shall be submitted and included on improvement plans pertaining specifically to sight distance at individual intersections and driveways. Criteria for design shall be in accordance with applicable AASHTO standards. Design will be provided for stopping sight distance associates with fences, signs, landscaping, parking zones, and other sight obstructions.

- Visibility triangles shall be maintained at all intersections. Within visibility triangles all trees shall be pruned such that no branches extend lower than six feet above curb top, at time of planting, and 8' above curb top at full maturity. Shrubs within the visibility triangle shall not exceed eighteen (18) inches in height above any curb level. Refer to Figure 3-B, Landscape Triangle.
- Maintenance of all landscaping with the common areas for the parkway and community collector roadways shall be the responsibility of the D'Andrea Community Association (DCA).



PARKWAY ENTRY

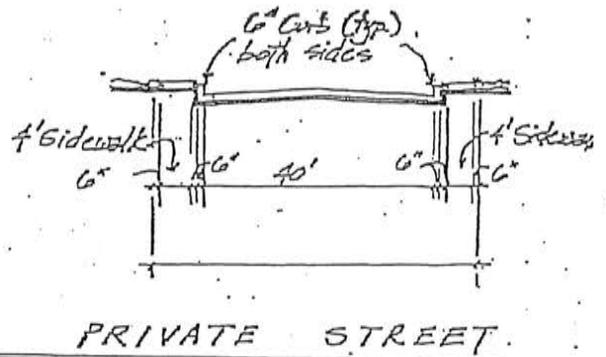
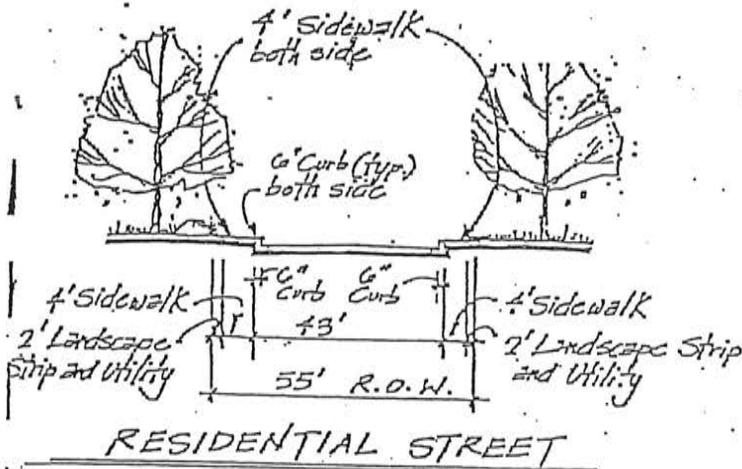
Note: Refer to Figure 2-3 Primary Trails/Sidewalks for location.



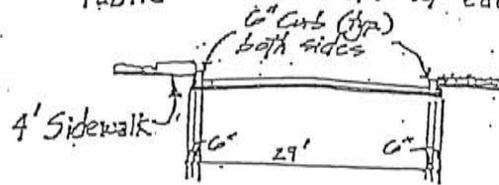
COMMUNITY COLLECTOR STREET

Note: Refer to Figure 2-3 Primary Trails/Sidewalks for location.

Figure 3-6a Street Cross Sections



(Note: Active Adult Mezzo, Veranda and Bella Vista projects only. Subject to review and approval by the Public Works Director w/ each project.)



No On Street Parking - There will be 90° parking pockets at 0.5 spaces/unit.
(Note: only for Mezzo, Veranda and Bella Vista projects, subject to review and approval by the Public Works Director w/ each project.)

Figure 3-6b Street Cross Sections

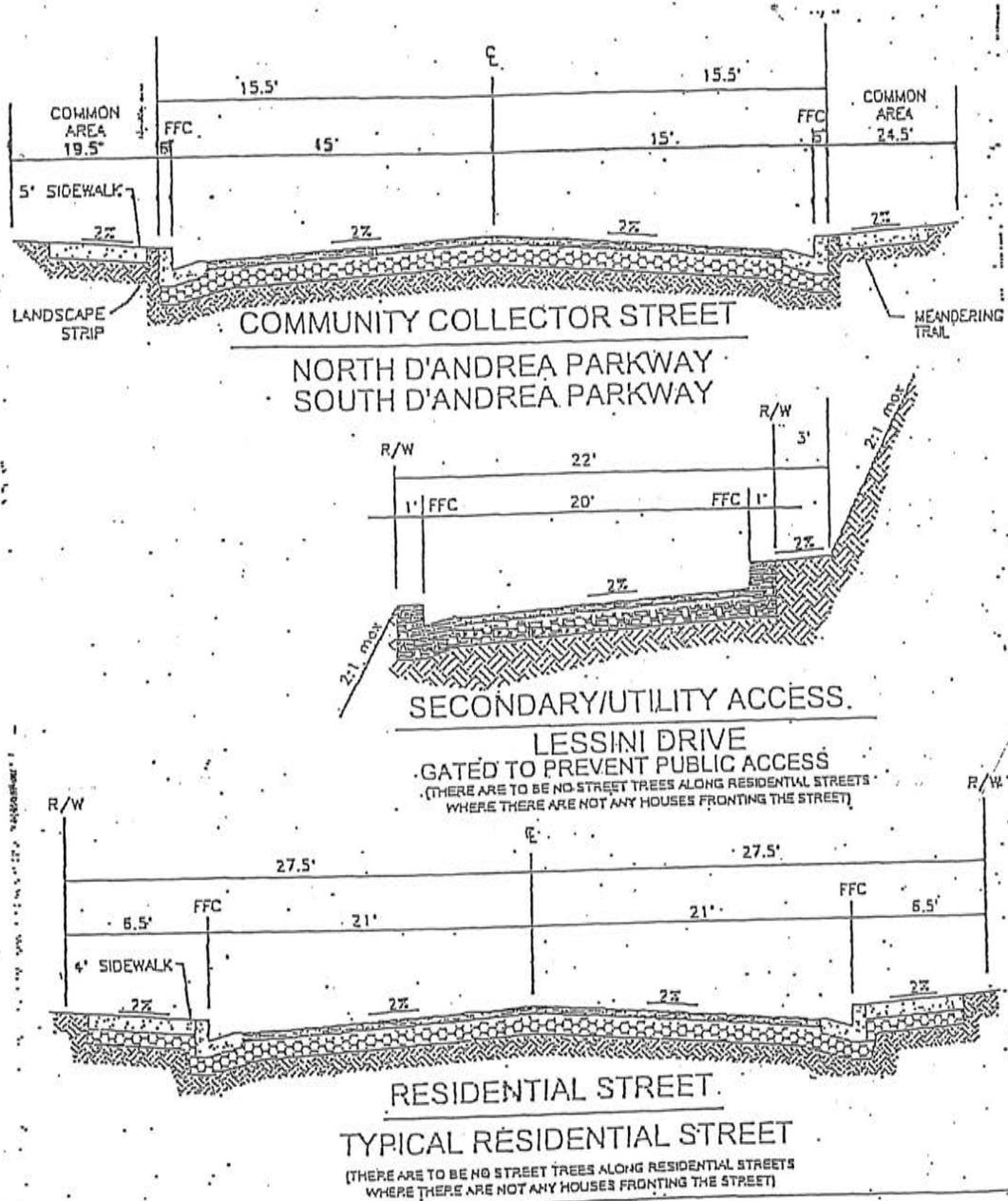
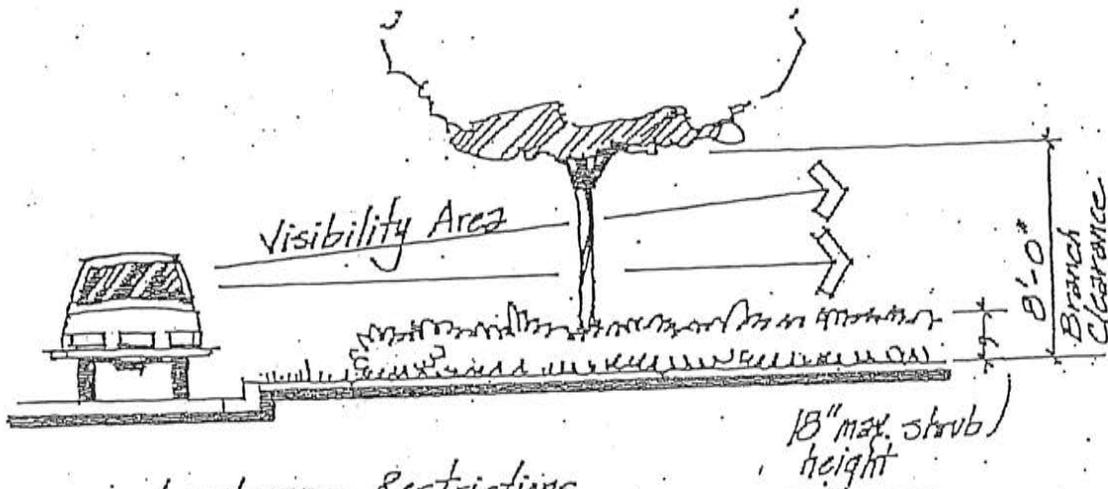


Figure 3-7a Street Cross Sections



Landscape Restrictions

- 18" height max. above top of Curb for Shrubs
- 8' min. (above top of curb) branch Clearance

Figure 3-8 Landscape Triangle

- A single tree species shall be used along each individual street and between intersections on long streets (i.e., D'Andrea Parkway). Tree species shall change at intersections on long streets. Tree species may be repeated throughout the project. Different cultivars of the same species may be used on a single street.
- On collector streets fronting common open space areas or adjacent properties not within D'Andrea Development, landscaping will use formal rows of street trees and native species to complement and enhance the existing plant community.
- On collector streets fronting residential or commercial property, landscaping shall use formal street tree pattern. This is to establish the community's image and the Northern Italian theme.

General Parkway Standards

- On parkways, deciduous canopy trees shall have a minimum caliper of two (2) inches at the time of planting measured D.B.H. (diameter at breast height). Canopy trees will be 90% of the street trees planted along the parkway. Refer to Figure 3-9 Parkway Streetscape.
- Deciduous accent trees shall have a minimum caliper of one and one-half (1-1/2) inches at the time of planting measured D.B.H. (diameter at breast height). Accent trees will be 10% of the street trees planted along the parkway. Refer to Figure 3-9 Parkway Streetscape.
- Evergreen trees shall consist of the following height mix at the time of planting: 60% at 6' height, 20% at 8' height, and 20% at 10' height (measured from finished grade to tree apex).
- A minimum of 50% of the shrubs installed shall be 5 gallon size or larger.
- Trees will be planted in formal rows within the planting strip.
- Within the parkway median, street trees shall be planted at approximately 50' on center.

Use of lawn shall be limited and used in areas to accent entries, intersections, and complement adjacent golf courses. Use of alternative living ground cover material in all other landscaping and planter strips to the approval of the DCC, Public Works Director, Parks & Recreation Director, and Administrator, prior to the approval of improvement plans and issuance of grading permit.

- The parkway median shall have a minimum planting bed width of 6 feet and shall include a minimum 2 foot hardscape perimeter working zone for maintenance personal safety.
- Street trees shall be planted at approximately 30' on center.

Community Collector Streetscape Standards

Community collector streetscaping shall feature traditional elements that emphasize pedestrian access and reinforce the project image.

- Deciduous accent trees shall have a minimum caliper of 50% two (2) inches and 50% one and one-half (1-1/2) inches at the time of planting measured D.B.H. (diameter at breast height).

- Evergreen trees shall consist of the following height mix at the time of planting: 60% at 6' height, 20% at 8' height, and 20% at 10' height (measured from finished grade to tree apex).
- A minimum of 50% of the shrubs installed shall be 5 gallon size or larger.
- Use of lawn shall be limited and used in areas to accent entries, intersections, and complement adjacent golf courses. Use of alternative living ground cover material in all other landscaping and planter strips to the approval of the DCC, Public Works Director, Parks and Recreation Director, Administrator, prior to approval of improvement plans and issuance of grading permit. No lawn shall be allowed in street medians, less than 8 feet wide.
- Trees will be planted in formal groupings to respect sight visibility triangles and scenic views. Refer to Figure 3-10 Community Collector Streetscape.
- One street tree shall be planted on each side of the street for every 50 lineal feet of street frontage. Refer to Figure 3-10 Community Collector Streetscape.

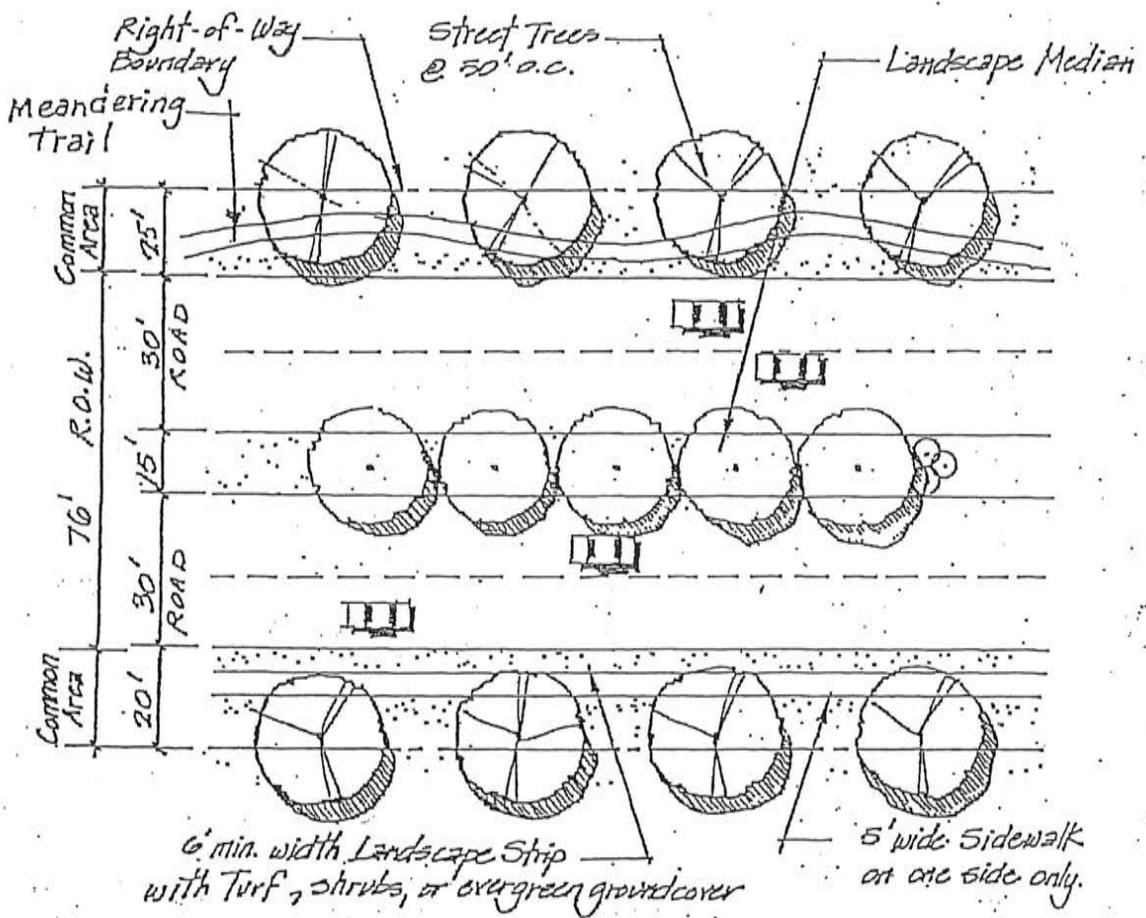


Figure 3-9 Parkway Streetscape

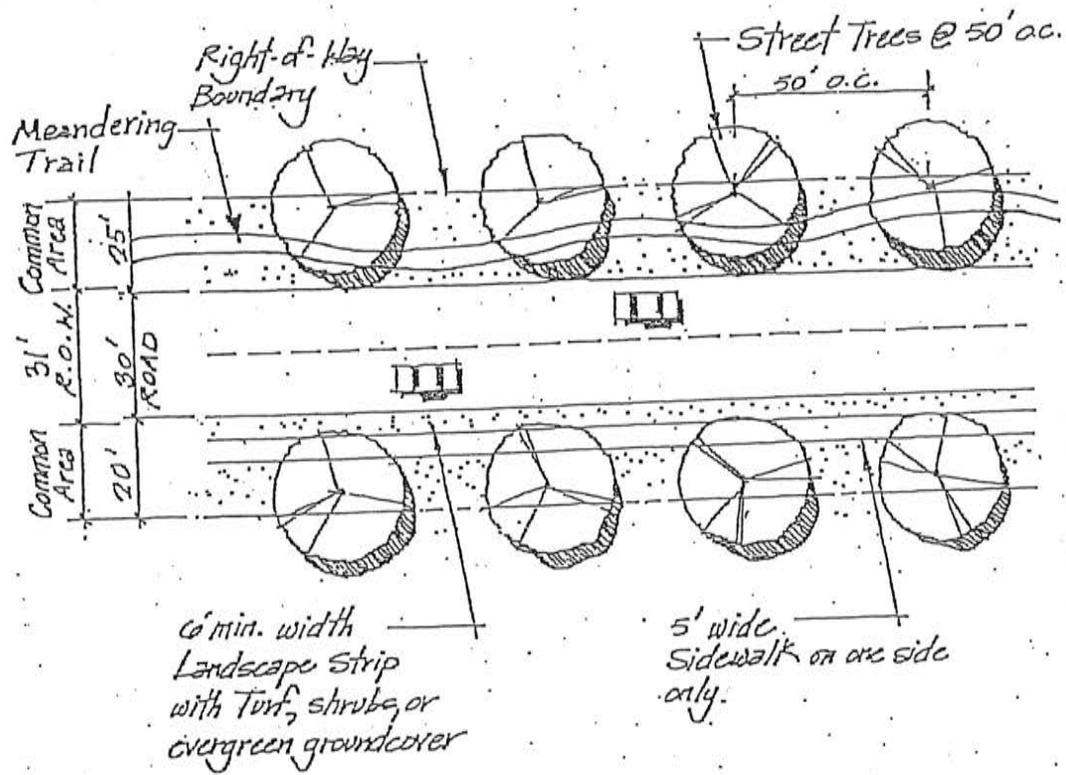


Figure 3-10 Community Collector Streetscape

Residential Streetscape Standards

- Trees shall be a minimum of 50% two (2) inches caliper size and 50% one and one-half (1-1/2) inches caliper size, measured 6" D.B.H. (diameter at breast height), at the time of planting.
- Trees shall be planted in formal rows within private property along the street. Gaps between trees shall not exceed fifty (50) feet except at intersections and areas where an adjustment for site conditions (driveways, utility boxes, etc.) apply.
- Trees shall be planted by developer of Village and reviewed and approved with the tentative map or Site Plan Review by the DCC and the City's Parks & Recreation Director, Public Works Director and Administrator.
- Refer to Figure 3-11, Residential Streetscape.

Village Streetscape Standards

- Landscaping within each village area will complement the project's overall Northern Italian theme.
- Trees shall be a minimum of 50% two (2) inches caliper size and 50% one & one-half (1-1/2) caliper size, measured D.B.H. (diameter at breast height), at the time of planting.
- Trees shall be planted by developer of Village and reviewed and approved with the tentative map or Site Plan Review by the DCC and the City's Parks & Recreation Director, Public Works Director and Administrator.
- Refer to Figure 3-12, Commercial Landscape Detail and Buffer Concept.
- Each lot shall contain a minimum of one (1) street tree and corner lots shall contain a minimum of three (3) street trees.

Major Entries & Intersections

Dramatic landscaping that reinforces the community image will be planted at major entries and intersections.

- Entry and intersection planting schemes will reflect the level of development of the adjacent streetscape. For example, intersections located within the Village Center will feature more formal plantings. Those located in outlying areas will feature rugged, naturalistic plantings.
- Landscaping will establish the Northern Italian theme and create an introduction to the project.
- Monument signs will be provided as community identification and be an integral, and cohesive part of the project's design concept at major entries (i.e. North D Andrea Parkway entry and South D Andrea Parkway entry).

General Standards

- Clear views for traffic safety shall be maintained. Refer to Figure 3-B, Visibility Triangle Standards:
- Deciduous canopy trees shall be sized with 50% having a minimum caliper of one and one-half (1-1/2) inches and 50% having a minimum caliper of two (2) inches at the time of planting measured D.B.H (diameter at breast height).
- Deciduous accent trees shall have a minimum caliper of one and one-half (1-1/2) inches at the planting measured D.B.H (diameter at breast height).

- Evergreen trees shall consist of the following height mix at the time of planting: 60% at 6' height, 20% at 8' height, and 20% at 10' height (measured from finished grade to tree apex).
- A minimum of 50% of the shrubs installed shall be 5 gallon size or larger.

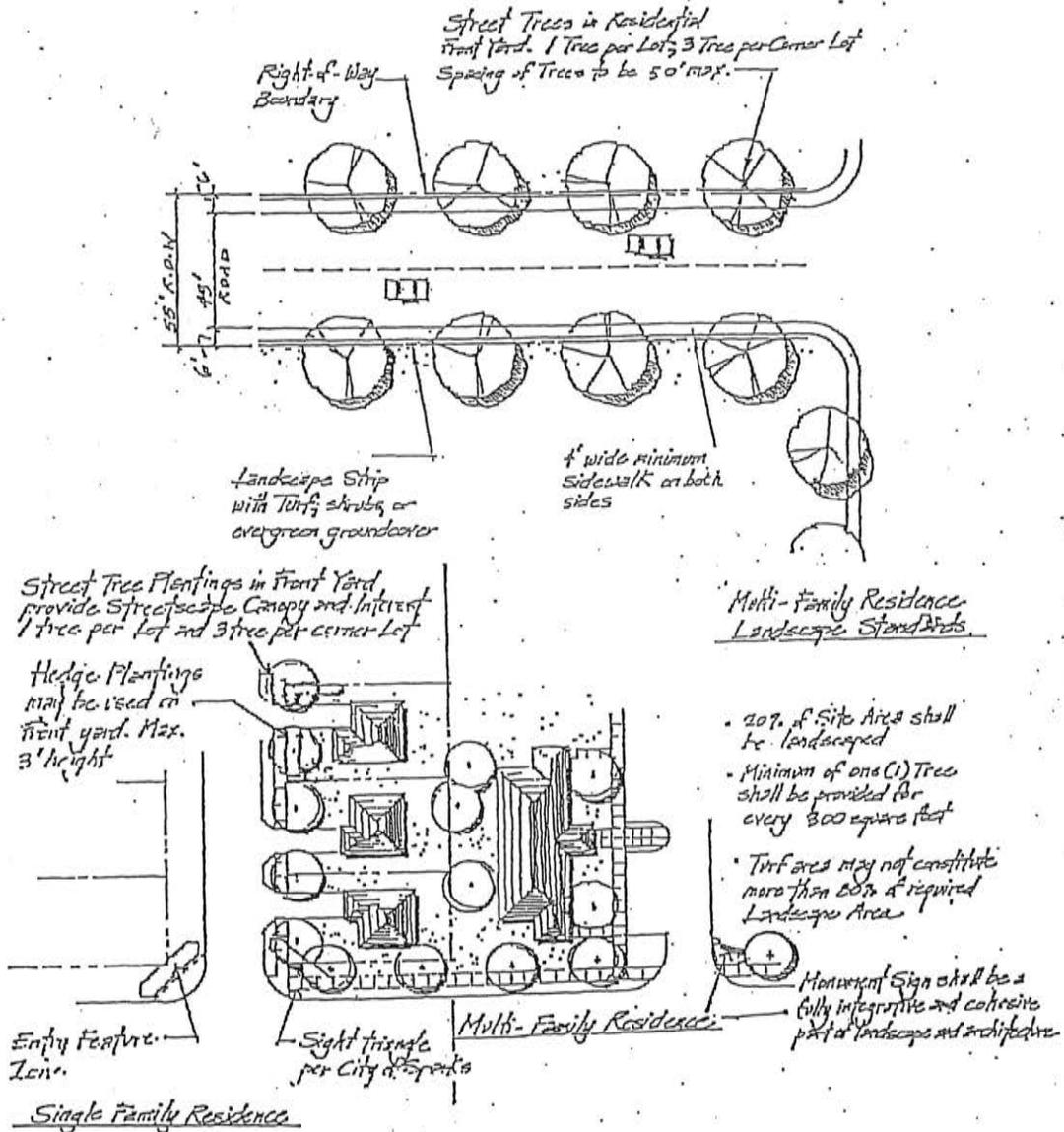


Figure 3-1 | Residential Streetscape

Vista Boulevard

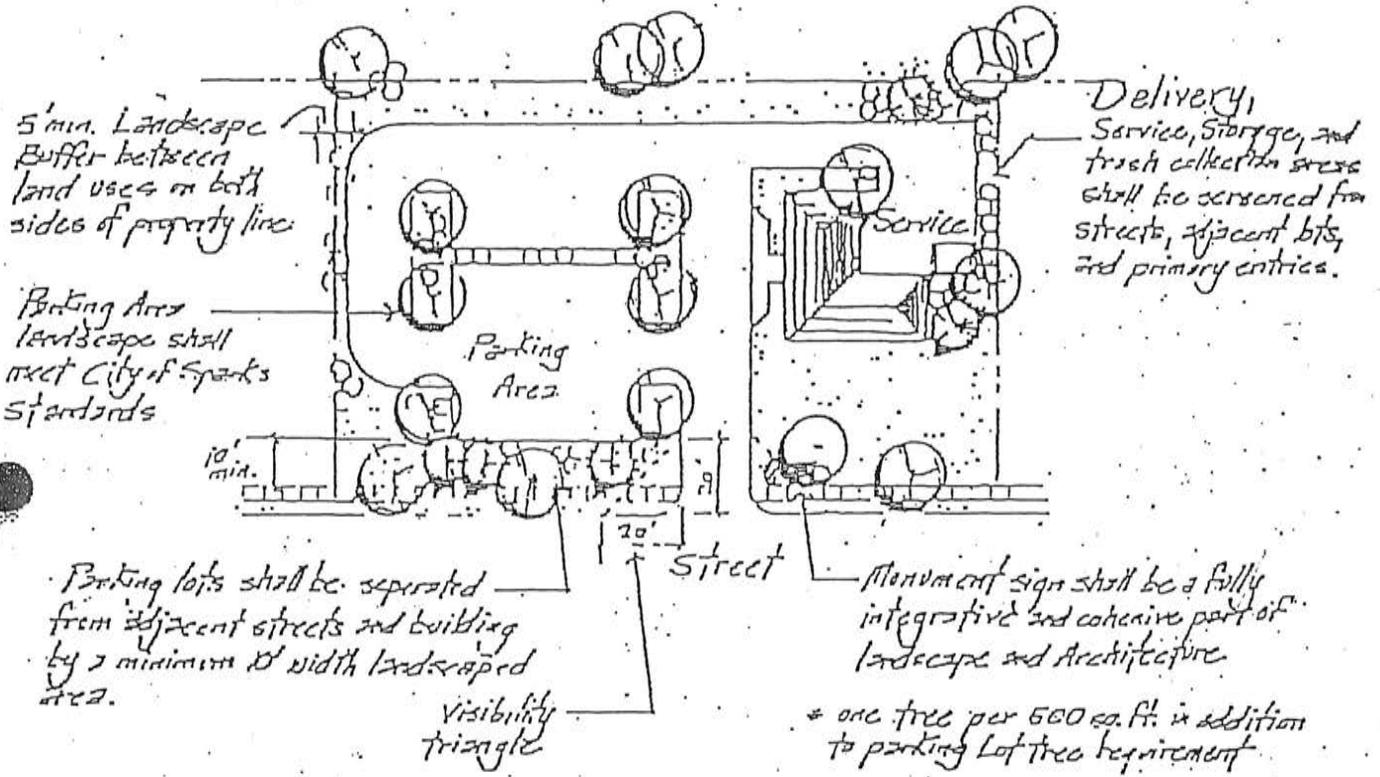


Figure 3-12 Commercial Landscape Detail and Buffer Concept

North D Andrea Parkway Entry

The North D Andrea Parkway Entry will be located where Baring Boulevard intersects Vista Boulevard.

- A Water feature shall be incorporated to reinforce the Northern Italian theme for the project. It shall be designed to be aesthetically pleasing with or without water.
- Refer to Figure 3-13 North D Andrea Entry and Figure 3-17 Open Space Trail Cross Section and overlook/shade structure.
- Overlook/shade arbor and pathway to overlook are subject to the approval of the Public Works Director.

South D Andrea Parkway Entry

The South D Andrea Parkway entry will be located where Whitewood Drive intersects Vista Boulevard.

- A Water feature shall be incorporated to reinforce the Northern Italian theme for the project. It shall be designed to be aesthetically pleasing with or without water.
- Refer to Figure 3-14 South D Andrea Entry and Figure 3-17 Open Space Trail Cross Section and overlook/shade structure.

Vista Boulevard Treatment

Edge treatments where D'Andrea bounds Vista Boulevard will provide screening and buffering of unwanted views and provide a sense of privacy.

- Landscaping along Vista Boulevard will reflect the project's Northern Italian theme.
- In areas where screening is needed, the plants shall be enhanced by berming to increase effectiveness. Evergreen trees and shrubs which branch close to the ground will be favored.
- Deciduous and evergreen plants shall be intermixed to provide visual interest through seasonal color, texture, and form to the project.
- Wall structure along the edge adjacent to residential zone property will serve as sound barrier and visual screening of vehicular traffic along Vista Boulevard and maintained by DCA. A public use easement will be placed on the sidewalk within the common area for acceptance by the City of Sparks subject to the approval of the Public Works Director. The sidewalks shall be maintained by DCA.
- Refer to Figure 3-15 Vista Boulevard Frontage.

Roundabouts

D'Andrea will have roundabouts, located within the public rights-of-way at the project's major entries. The project's roundabouts will serve as focal points for D'Andrea. The City of Sparks has not developed design standards for roundabouts. The developer will be required to submit design guidelines and traffic studies from agencies that have incorporated roundabouts into their street systems. Additional information will be provided as required by the Public Works Director. The guidelines, studies, and related information shall be submitted for review and approval by the Public Works Director and the RTC as needed prior to final design.

Conventional four way intersections will be provided in lieu of an approved roundabout design. The North D Andrea Parkway Entry will circle south of an existing rock outcrop to which will remain largely in its natural

- Roundabouts will be designed to limit pedestrian access to those accesses as approval by Public Works Director.
- Landscaping within the roundabout shall complement the architecture and features in the roundabout and subject to review and approval by the Public Works Director; Park and Recreation Director, and Administrator.
- Deciduous and evergreen plants shall be intermixed to provide visual interest through seasonal color, texture, and form to the project.
- The roundabout shall be designed to discourage pedestrians from accessing the traffic circle in the center of the roundabout to the approval of the Public Works Director.

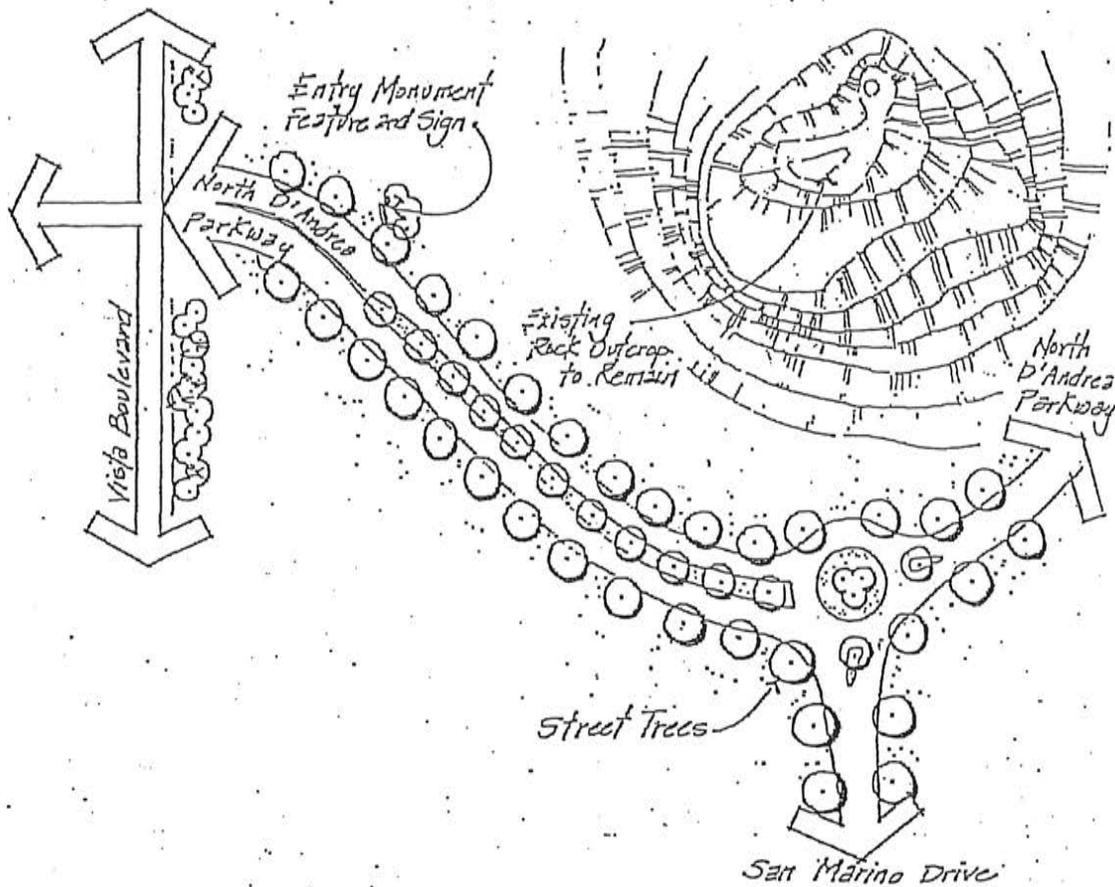


Figure 3-13 North D Andrea Parkway Entry

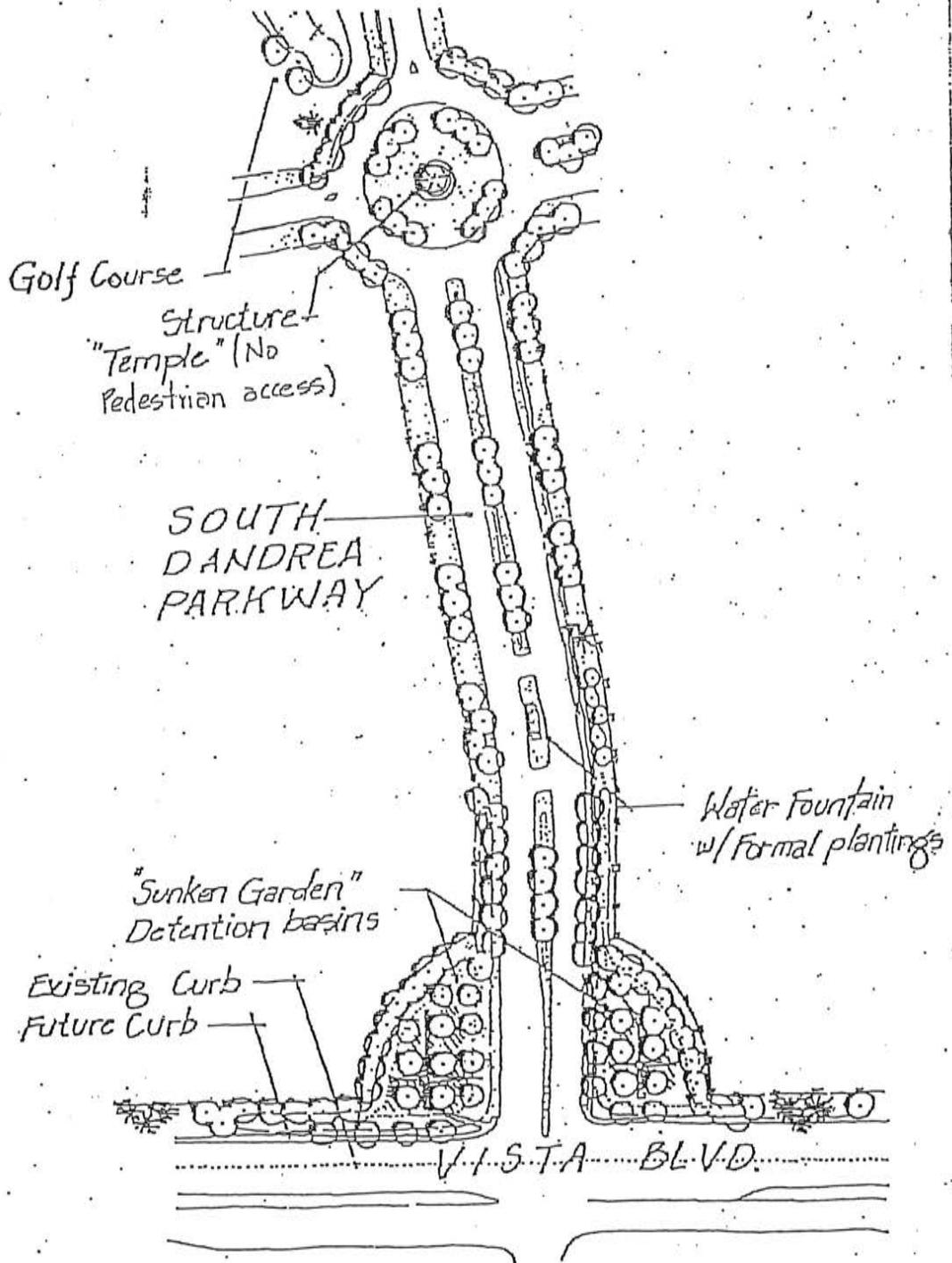


Figure 3-14 South D'Andrea Parkway Entry

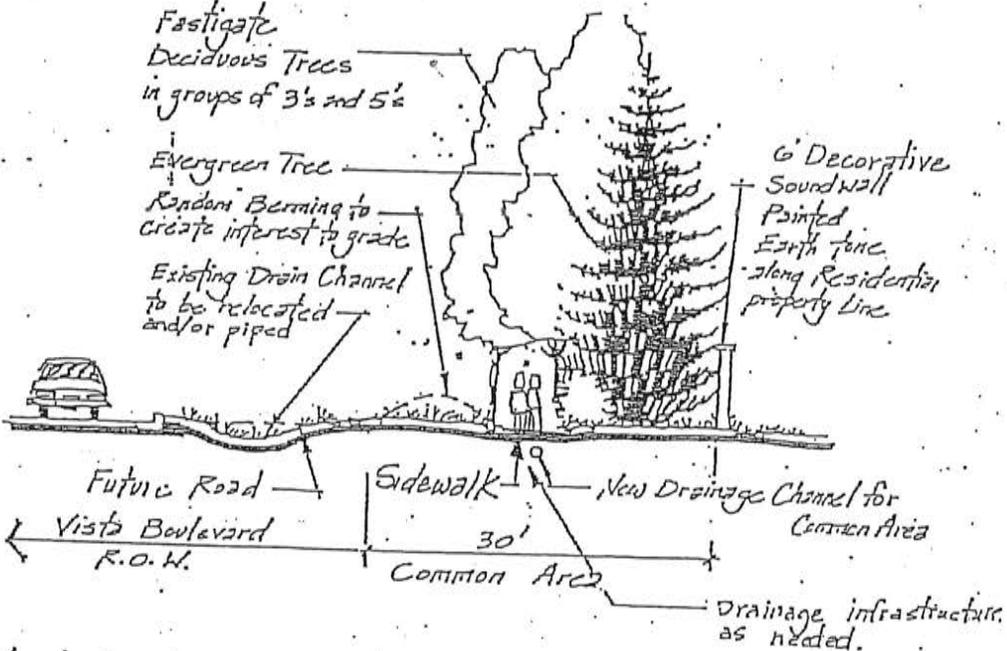
Fastigate
Deciduous Trees
in groups of 3's and 5's

Evergreen Tree

Random Berming to
create interest in grade

Existing Drain Channel
to be relocated
and/or piped

Decorative
Sound Wall
Painted
Earth tone
along Residential
property line



Fastigate Deciduous Trees
in groups of 3's and 5's

Evergreen Tree

New Drainage Channel for common area
rock lined and meandering
through open space

Sound Barrier Wall
Split face conc. block
painted tan/beige
(with anti-graffiti
coating)

relocated
street lights

Existing Drainage Channel
to be moved and piped
through Common Area

Sidewalk

30' Buffer
Common
Area

Future Road
Widening

Vista Boulevard

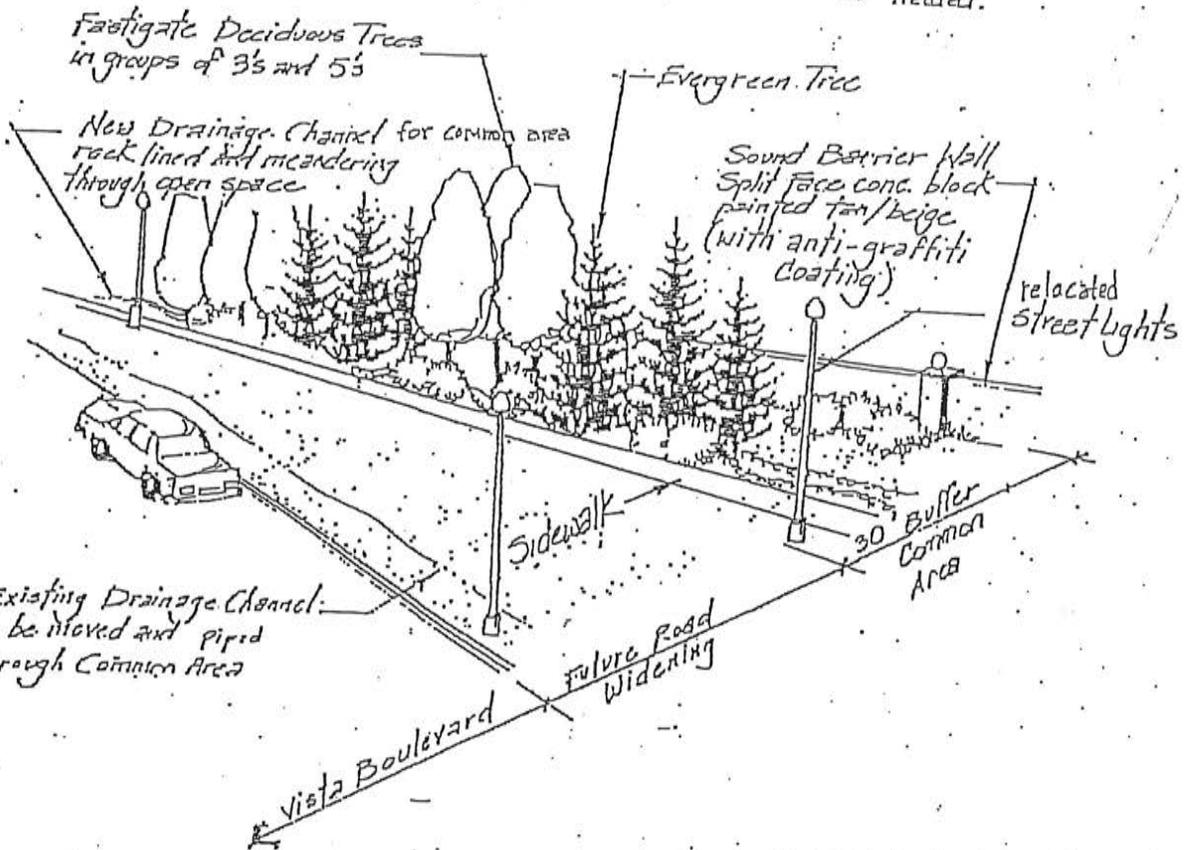


Figure 3-15 Vista Boulevard Frontage

Village Entries

Project entries will be designed to help establish the desired project image and identity.

- Landscaping will be provided to serve as introduction to the project and provide the project image.
- The project entries will be of a unified or complementary design in maintaining the continuous Northern Italian theme of the project.
- Project entries will be reviewed and approved by the Public Works Director and Community Development Director prior to installation for conformance to visibility triangle standards.
- Refer to Figure 3-22 Village Entry Concept.

Open Space

D'Andrea will offer its residents approximately 203 ± acres of open space made up of village commons, and natural open space. Refer to Figure 3-17, Open Space and Trails System Map. Open space will be owned and maintained by the Community Association. The open space, trails, and streetscapes will be developed by D'Andrea Nevada, LLC as part of the backbone infrastructure. The public parks will be dedicated to the City of Sparks after development.

D'Andrea as a Master Planned Community is required by SMC to have 20% of the project to be common open space owned and maintained by the D'Andrea Community Association. D'Andrea consists of 861± acres therefore the required open space is 172.2 acres. The golf course/clubhouse is 179.19 acres, because the homeowners of D'Andrea will have preferential tee times, green fees discount and pro shop discounts, the City of Sparks allows 25% of the golf acreage to be credited towards the open space requirement for a total of 44.80 acres. The two existing parks will be dedicated to the City of Sparks. The dedicated 8 acres of park will be maintained by the Parks and Recreation Department. The four pocket parks within the D'Andrea development will be maintained by the Homeowners association. Table 3-1 summarizes the total open space/golf course/parks and the provided 20% open space area.

Table 3-1 Open Space

	TOTAL ACRES	PROVIDED OPEN SPACE (AC)
Golf Course/Clubhouse	179.19	±44.80
Park	7.66	0
Open Space/Streetscape	±203.43	±203.43
Total	±390.28	±251.23

Natural Open Space

- Undisturbed common areas shall be maintained in a natural state.
- Areas designated as natural open space disturbed during construction of roads, trails, and utilities shall

be enhanced/revegetated.

- Open space shall be used to preserve or enhance "natural" features such as drainageways and rock outcroppings.

Open Space and Trail System

- Open space will provide path/trail linkages between community and recreational facilities and other site amenities for the enjoyment of users. Refer to Figure 3-16 Primary Trails System Map.
- Where practical, preserve and protect existing native plant communities by minimizing removal of vegetation and grading as required to provide a smooth, safe trail surface.
- A sidewalk along Vista Boulevard shall be provided to Country Ridge which will eliminate the school crossing on Vista Boulevard.
- Refer to Figure 3-17 Open Space Trails Cross Sections.

Parks

Approximately 8 acres will be dedicated to a total of two (2) parks. The Developer will build the parks and dedicate 2 of the parks to City of Sparks and the HOA will maintain the other 5. The developer will receive residential construction tax credits. This will assure timely construction of the parks to serve the community's residents. The park facility will provide a community-wide place to gather, offering opportunities for both passive and active recreation. Park amenities may include group picnic areas, play areas, and open turf. Refer to Figure 3-18 Park Location.

The 2.9 +/- acre Treviso Park will be provided adjacent to an existing school property and Toscana Village. This park may include a perimeter pathway with seating/rest areas for the enjoyment of the nearby adult community and a central open turf area and play lot for the community residents.

The 2.5 +/- acre San Manno Park will be located southeast of the North D'Andrea Entry roundabout at the corner of North D'Andrea Boulevard and San Marino Drive. This park may include a play lot, picnic tables, and possibly a shade structure. Pathways within the park will connect to the open space trails system.

The parks will be designed to City standards and subject to approval by the Parks and Recreation Director. The parks will be completed prior to the acceptance of any public improvements within the Villages. Park amenities will be selected subject to review and approval by Parks and Recreation Director.



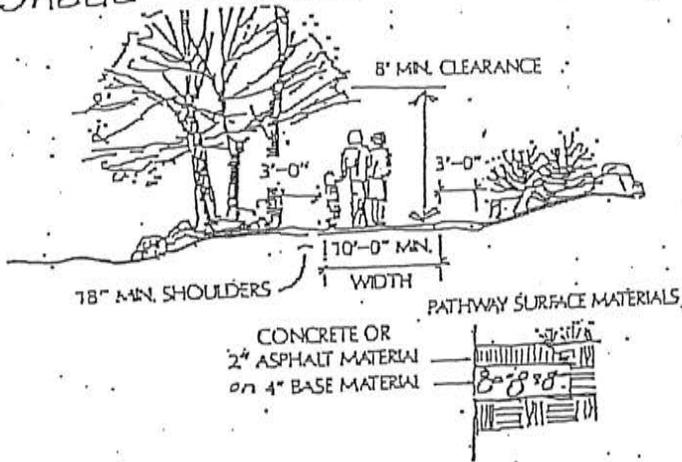
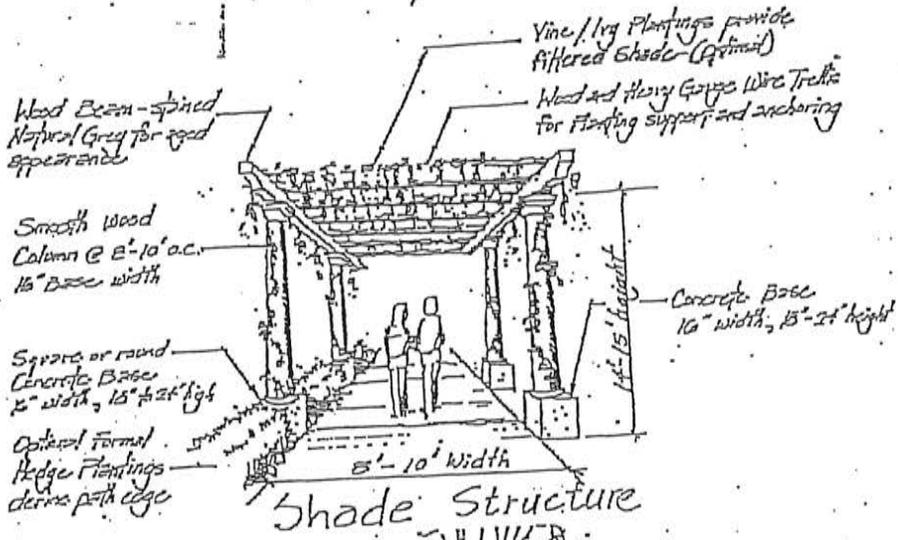
Figure 3-16 Primary Trails System Map

Trails

- Trails shall meander within open space, designed as sweeping curves that create visually appealing landscape forms and follow the natural grade. Abrupt or irregular curves and jogs are not permitted. Curved paths shall not be used in areas too narrow to allow a sweeping curve.
- Trails will be provided as linkages to the community, recreational facilities, and residential uses.
- All trails will be maintained by the D'Andrea Community Association, except for trails within the City parks.
- New trails cut through natural, undisturbed terrain shall minimize removal of vegetation and grading as required to provide a smooth, safe traveling surface.
- Trails within the City Parks shall be a minimum of 10 feet and concrete construction.
- Refer to Figure 3-17, Open Space Trails Cross Sections.

Trailhead

- Trailheads shall be provided, where needed, with signage to identify trails and to provide directions to users.
- A Trailhead for access to the BLM lands to the east will be located at Vecchio Drive. A turn around with a minimum 45' radius will be provided to allow emergency vehicle maneuvering area. No horse trailers will be permitted. A parking lot will be provided as directed by the Public Works Director, Parks & Recreation Director, and Administrator.
- Refer to Figure 3-19 Trailhead Detail.



(Subject to approval of Public Works Director and Parks and Recreation Director)

Figure 3-17 Open Space Trails Cross Section

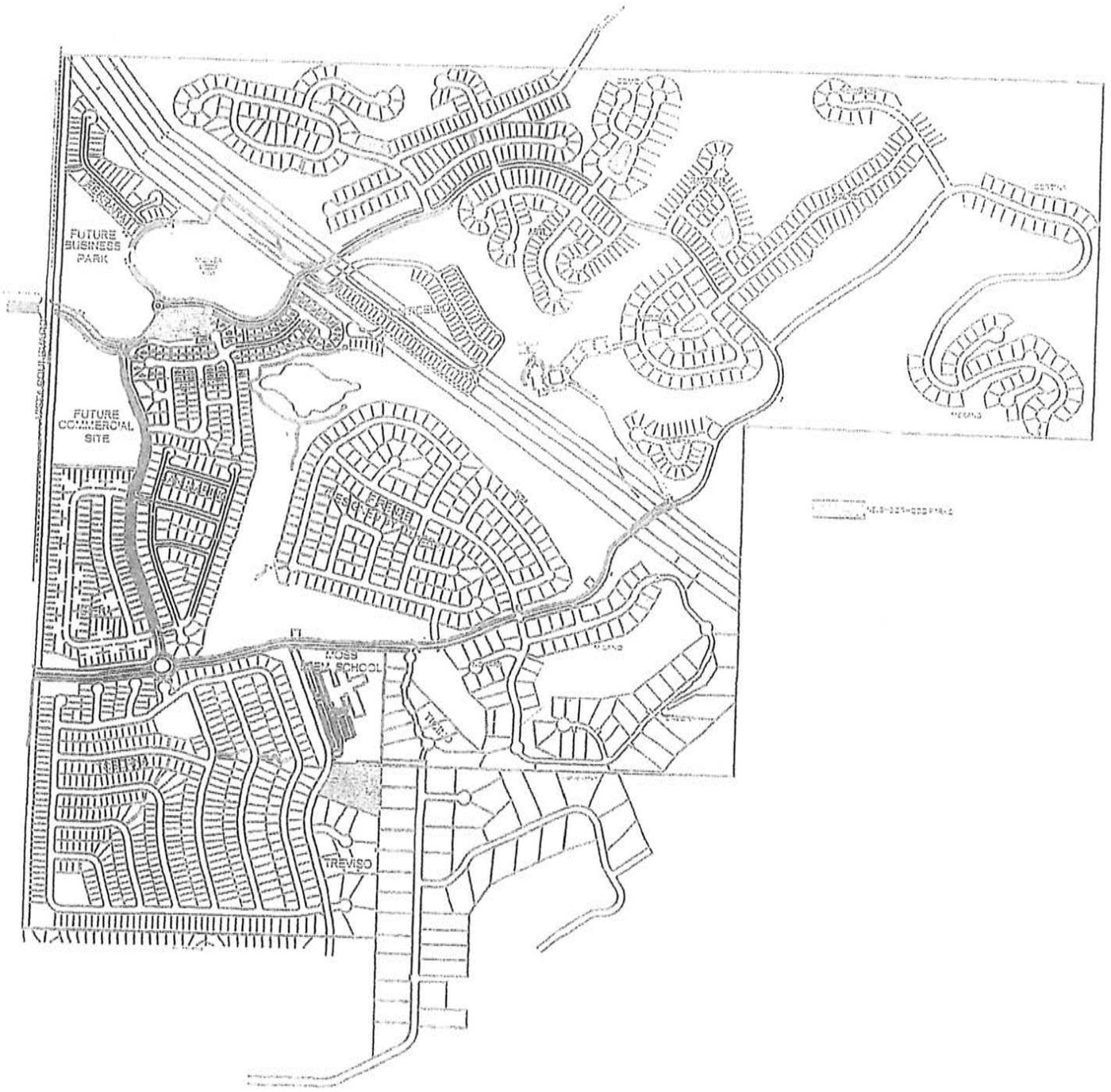
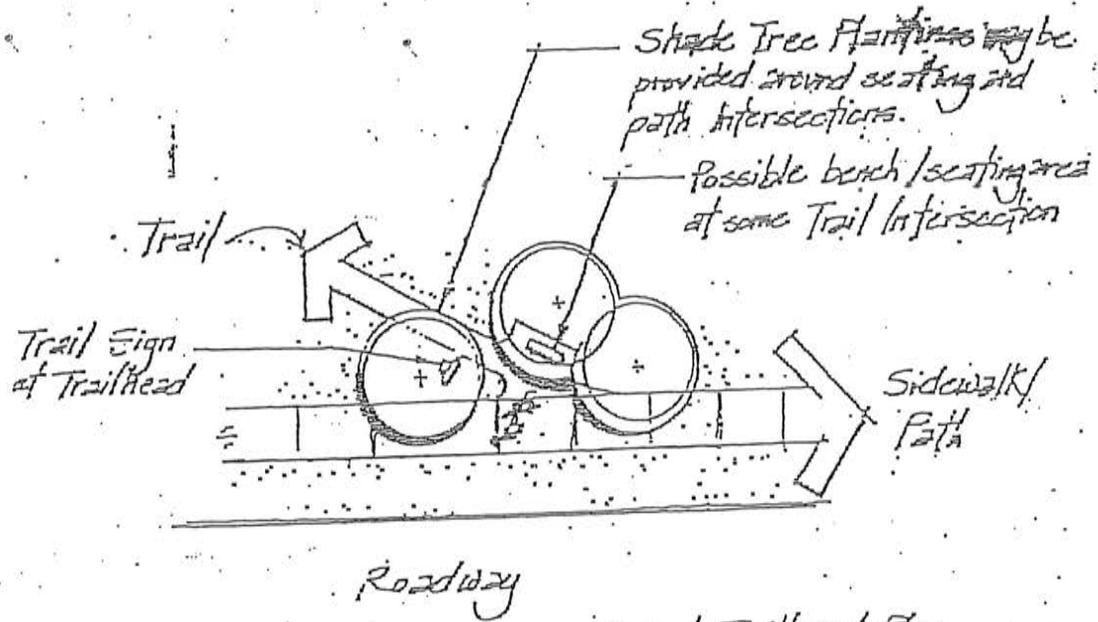


Figure 3-18 Park Location



Typical Trailhead Plan

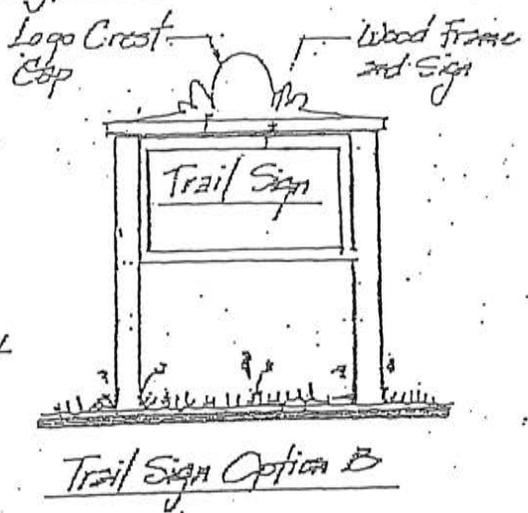
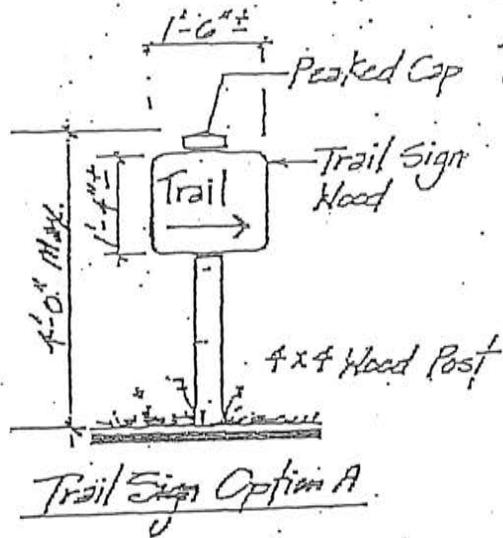


Figure 3-19 Trailhead Detail

Site Grading Principles

The D'Andrea property includes unique characteristics which have been reflected in the following grading solutions. These solutions include the use of the best available techniques to minimize the visual impact from site grading as much as possible, given the site visual topography. Single loaded streets and split level homes with walkout basements will be used rather than standard flat pad homes in an attempt to reduce the amount of grading. Other techniques have been implemented to minimize visual impact from arterial roadways, including maintaining increased setbacks from ridgelines and hilltops and the strategic placement of two story homes in an attempt to screen necessary cut/ fill slopes.

Consideration is given to limit aesthetic degradation where grading is necessary by the use of contoured grading and the revegetation of slopes using stockpiled topsoil, shrub plugs, geotextile fabric (as necessary for erosion protection), and revegetation seed blend, in combination with existing rocks and boulders in an effort to mimic natural terrain.

Specific examples of how these principles will be achieved include but are not limited to, the following:

Split Level Homes:

Split level homes with walkout basements are included in the villages of Carrera, Bolzano, Merano, and Monticello. These split level homes are designed to help reduce the impact of cut/ fill slopes associated with development.

Single Loaded Streets:

Single loaded streets are proposed in many of the villages (i.e. Carrera, Merano, Trento, Como, Asti, etc.). The use of single loaded streets is designed to help reduce the impact of grading on hillsides. When the single loaded streets are combined with split level homes, the visual impact is even less noticeable.

Hilltops:

In the village of Carrera approximately 13 units were eliminated in order to preserve a prominent land feature designated in the Sparks Municipal Code 20.99 as a significant ridgeline. The current design attempts to integrate existing land forms into the development by providing adequate setbacks from hilltops and ridgelines.

Two Story Homes:

Two story homes will be used where appropriate to eliminate or reduce the visibility of cut/ fill slopes. Two story homes help screen slopes from public view thus minimizing visual impact within the development and from adjacent arterial roadways.

Golf Course

The golf experience at D'Andrea will combine dramatic topography with classical strategic elements to provide golfers an enjoyable and challenging experience. With the clubhouse sited upon a natural rock overlook, the golf course proceeds to utilize the entire site to its best advantage. There is a difference of 350 feet from the highest point on the course to the lowest, with the clubhouse located at about mid-grade.

The golf holes will work with the existing topography, which will reduce the overall dirt moving requirement. Tees will be benched into the hillsides using the native rock for support. Disturbance will be kept to a minimum, and preferably within the eventual limits of maintained turf. Areas outside of the fairways and rough will be left native, providing a stunning contrast between the dark green of the golf course with the browns and light greens of the native vegetation. This ties together the theme of the golf course as a course working with its

natural environment. The native areas will reduce the irrigation demand on the project. In addition, the irrigation system will utilize effluent water to irrigate the golf course.

Variety and playability will be key elements of the golfing experience at D'Andrea. Half the holes play downhill and half uphill, while there is also a mix of doglegs right, doglegs left, and straightaway holes. This variety will allow each hole to be a completely different experience.

Variety is tied to playability in the number of tees available to the golfers, as well as the bunkering of the course. There are five sets of tees, which will provide golfers of all abilities the opportunity to enjoy the golf experience on par 4 or longer holes, there will be a sixth tee approximately 100 to 150 yards from the green for inexperienced players or physically challenged players. The purpose is to allow opportunity to play on a championship golf course. The back tees will play at a little over 6750± yards, while the forward tees will play at 5100± yards.

The bunkering on the golf course will provide for playability for all golfers, while also providing strategic options to those golfers who would challenge par for the course. The bunkering will be set on angles, and tied into natural features. Though the ideal route usually brings a bunker into play, the option is always available to play around.

A unique feature of the golf course will be the addition of a par 3 tee on every hole, thus allowing the course to be played as a par 3 course. This tee will be beneficial to beginners, juniors, seniors, or anyone looking to fine tune their iron play or get around the course quicker during off-peak times.

Urban/ Wildland Interface Zone

Homeowners on the fringe of D'Andrea will comply with defensible space guidelines per Nevada Division of Forestry "Wildfire Protection for Homeowners and Developers."

Defensible space refers to that area between a house and an oncoming wildland fire where the vegetation has been modified to reduce the wildfire threat and which provides an opportunity for firefighters to safely defend the house. It is a key area which can make the difference between a house surviving a wildland fire or being destroyed. Landscape maintenance is a key element in overall good defensible space programs for homes in wildland areas.

The laws in Nevada require the clearing of all flammable vegetation a minimum of 30-feet around your home and other structures. An increase in slope on the property requires an increase in separation between the main structures and any flammable landscaping. This does not mean that a ring of bare dirt around the home is the only method to create a defensible space. The homeowner can create a defensible space and also beautify the property.

The homeowner will modify wildland fire intensity and behavior through landscaping plans that include patterns of fire resistive vegetation and safe zones created by stone walls, patios, swimming pools, decks and driveway or roadway areas. Establish islands of plants or plant groups which effectively break up continuity of vegetation. This can assist in modification of wildland fire behavior and slow the spread of fire through D'Andrea.

The following are standards for the eastern upland portions of the property:

- Use of lower growing herbaceous plants if they can be kept green during the fire season.
- Use of lower growing deciduous shrubs.

- No irrigated coniferous shrubs.
- Planting with thick broad leaf plants over narrow, needle-like leaves.
- Deciduous shrubs and trees need to be kept green during the fire season and are free of dead wood, ladder fuels and situated such that adjacent wildland vegetation cannot convey a fire.

The D'Andrea Community Association (DCC) shall review and approve, in conjunction with Sparks Fire Prevention Division and Administrator, the landscaping design, installation of the approved landscaping and maintenance of the approved landscaping for the Bella Vista Villages, fringe villages and common areas. This is to ensure that patterns of fire resistive vegetation, safe zones of hardscape material and building materials that are fire resistant are utilized to minimize loss during wildfires.

Hardscape

D'Andrea's hardscape palette includes signs, paving, site furnishings, lighting, walls, and fences. These elements will strengthen the project's Northern Italian theme by establishing a cohesive look, reflecting circulation hierarchies, and creating focal points within the community.

The use of hardscape elements within the more developed major project entries, village entries, parkways and the golf course clubhouse/recreation center requires higher level of design detail and style to reinforce the Northern Italian image. Hardscaping within areas outside of the more developed areas, i.e., residential streets, land to be dedicated to public use, and private parcels will vary in level of detail. Hardscape elements in the outlying areas shall coordinate with the styles, materials and colors of the more developed area. This approach permits the establishment of unique yet related identities for distinct areas within D'Andrea.

Signs

Sign standards provide for a cohesive, coordinated means to promote community image, identify, direction.

General Sign Standards

All signs shall comply with the following standards. Sign standards specific to different treatment areas within the community are described later. A uniform common area graphic and signs system shall be designed for all signs related to major entries and identification, public common areas and the path/trail system prior to approval of the first final map. Refer to Figure 3-20. Refer to Lighting Standards, in the following section, for lighting information relating to signs.

- All signs shall comply with the City of Sparks ordinance for on-premises and off-premises signs. All public street/traffic safety signs shall comply with the City of Sparks standards and will be installed per the "Standard Details for Public Works Construction" Book in conjunction with the "Standard Specifications for Public Works Construction" (Orange Book) and MUTCD.
- The following signs are prohibited in addition to those prohibited by the City of Sparks code:
 - Any revolving beacon, flashing and/or rotating sign, any sign with intermittent lighting (with the exception of flashing school crossing signs or temporary construction or other safety signs).
 - Any sign which extends above the roof line or parapet, whichever is higher.
 - Any billboard.
 - Signs will not obstruct the visibility of traffic or public signs, or traffic control devices. Signs will not

interfere with traffic visibility triangles.

- Special community events and election related signs will be permitted as mandated by the Sparks Municipal Code.
- Signs and sign structures shall be maintained at all times in good repair, with supports and fastenings free from deterioration, rust or loosening. Signs shall be designed to withstand wind pressures in the area in which they are located.
- Signs shall be a fully integrative and cohesive part of the landscape and match or coordinate with the style and detail design of associated hardscape and architecture. The D'Andrea logo will be used on all D'Andrea Community signage as part of the overall theme, except for street/traffic safety signs.

Community Entry

A community entry monument sign will be located at each of the major project entries, i.e., North D Andrea Parkway and South D Andrea Parkway entries. Smaller monumentation will be located at Pismo Drive and from Desert Highlands.

- The community entry signs shall be monument type signs identifying D'Andrea and establishing a community image. Refer to Figure 3-21 Community Entry Monument.
- The community entry materials shall be:
 - Monument: Masonry with stucco or plaster
- The community entry monument will be located within common area near the project boundary, outside of any visibility triangles.
- The community entry monument shall be a fully integrative and cohesive part of the landscape and architecture.
- Lighting will be incorporated into the project entry, per the Lighting section.

Project Entries (for Single Builders)

Project entry features will identify residential villages, orient visitors and residents, and add interest to the streetscape. Primary entry areas shall integrate walkways or paths, street plantings, and associated architectural elements into a single complex maximizing the value of these improvements, and establishing a quality image at the onset of project development.

- The Village entries will establish a distinct image, using materials and detailing to distinguish this individual neighborhoods from surrounding portions of the project. Refer to Figure 3-22 Village Entry Monuments.
- The village entry monument materials shall be:
 - Monument: split-faced block, slump stone or decorative concrete block wall faced finish to coordinate with architecture subject to review and approval by DCC.
 - Pilasters: split-faced block, slump stone or a decorative concrete block faced with finish to coordinate with architecture subject to review and approval by DCC.



Figure 3-20 Proposed Logo

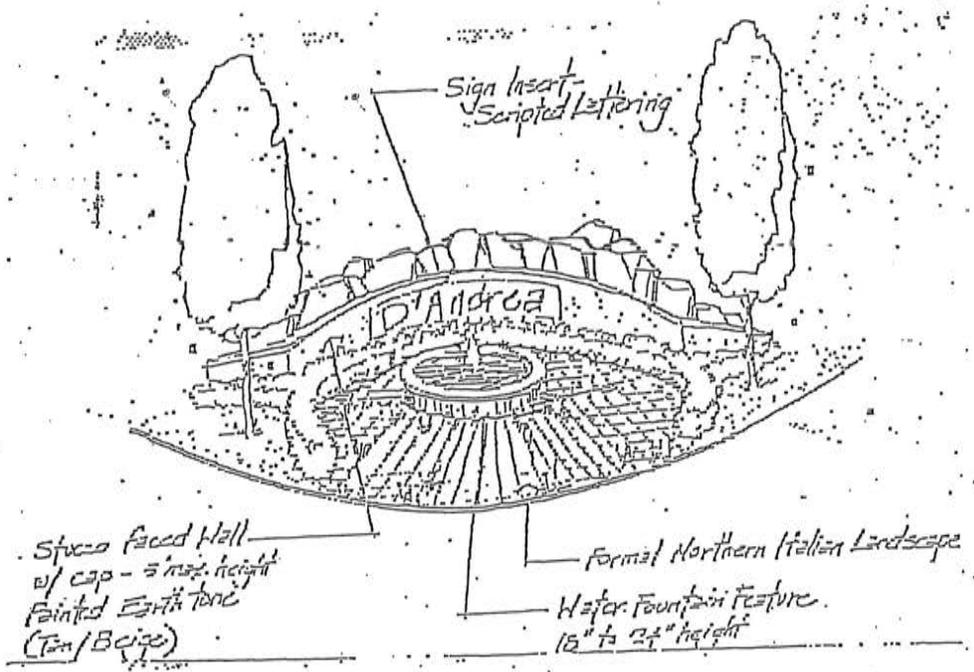


Figure 3-21 Community Entry Monument

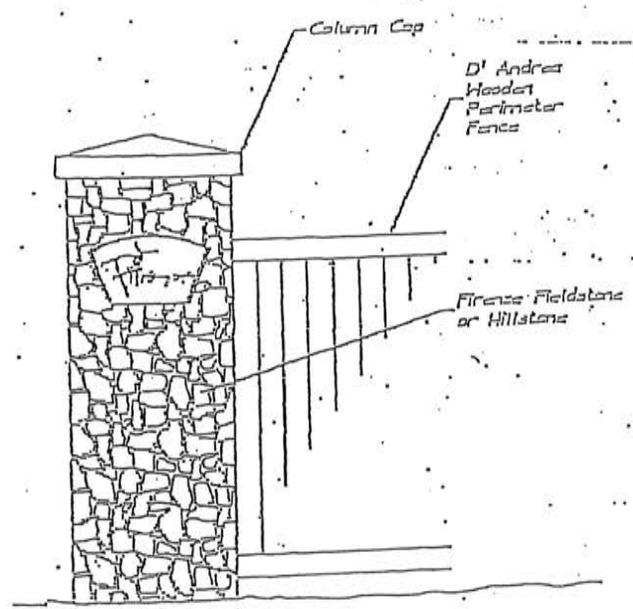


Figure 3-22 Village Entry Concept

- Signs shall be located in common areas and will be located outside of any visibility triangles or public right-of-way.
- Project lettering/logo will be incorporated into Village entries signs.
- Final design and letter style shall be subject to the approval of the Administrator and the D'Andrea Control Committee (DCC) during the tentative map review process.

Vehicular Signs

Vehicular signs include street signs, traffic signs, and directional signs that control vehicular traffic and/or are intended to be viewed from a vehicle.

- All public street/traffic safety signs shall comply with City of Sparks and MUTCD standards and will be installed per the "Standard Details for Public Works Construction" Book in conjunction with the "Standard Specifications for Public Works Construction" (Orange Book).
- Non-standard vehicular signs shall comply with the uniform graphics package to be developed to approval of Public Works Director.
- Refer to Figure 3-23 Vehicular Signs.

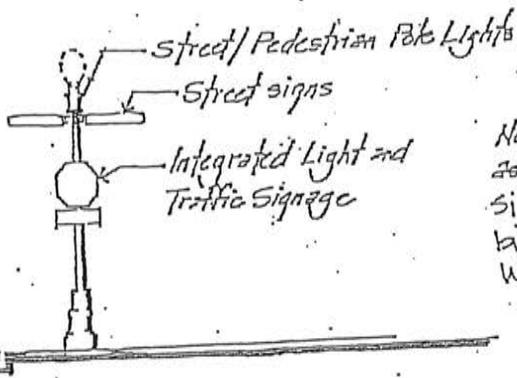
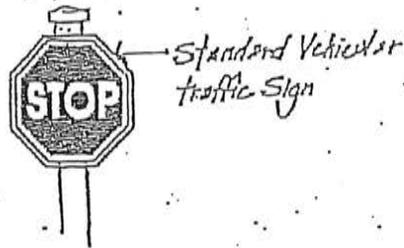
Pedestrian Signs

- Pedestrian sign materials shall be:
 - Post and Frame: Metal
 - Sign Face: To be developed as part of uniform graphics system subject to review and approval of the Public Works Director.
- Pedestrian signs shall comply with the uniform graphics package to be developed during final mapping.
- Refer to Figure 3-24 Pedestrian Signs.

Trailhead Signs

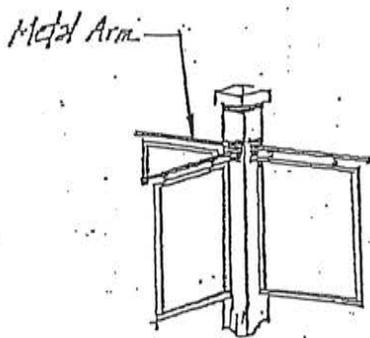
Trailhead signs are identity and directional signs that serve as trailhead markers and/or trail entry identification.

- Trailhead signs shall comply with the uniform graphics package to be developed during final mapping.
- Trailhead signs will be located, where needed, at trail heads and access points into B.L.M. property and where trails terminate or cross property boundaries.
- Trailhead sign materials shall be:
 - Post and Frame: Redwood or Metal
 - Sign Face: To be developed as part of uniform graphic system subject to review and approval of the Parks and Recreation Director and Public Works Director.
- Refer to Figure 3-19 Trailhead Detail.

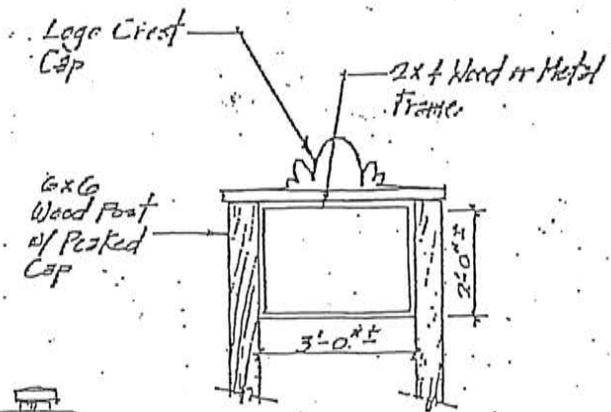


Note: Maximum Sign face
as per MUTCD
Signs per approval
by Sparks Public
Works Director.

Figure 3-23 Vehicular Signs.



Metal Arm



Logo Crest
Cap

2x4 Wood or Metal
Frame

6x6
Wood Post
of Peaked
Cap

3'-0"±

2'-0"±



4x4 Wood Post of
peaked cap and optional Ball Door

Figure 3-24 Pedestrian Sign

Commercial Signs

Commercial signs are those used to identify and locate commercial, business, and office areas.

- Building signs shall be integrated into building architecture and composed of materials compatible with the materials of the building. The colors and materials of signs shall complement the associated buildings to which they refer, subject to the approval of the Administrator and the Design Control Committee, and shall comply with the uniform graphics system to be developed during the site plan process for the general commercial development.
- Signs shall not be standard cabinet construction (canned) using plastic components.
- Monument signs shall be fully integrated into the commercial village architecturally and be enhanced with landscaping.
- Signs shall identify and locate, rather than advertise and sell.
- The light from any source intended to illuminate a sign shall be shaded, shielded, or directed so that light intensity or brightness does not adversely affect surrounding or facing properties, nor adversely affect safe vision of pedestrians or operations of vehicles moving on public streets, driveways or parking areas.

Builder Project Signs (for Multi-Builder Villages)

This section signs applies where there are multiple builders or developers within a village. Each builder or developer within a village may provide a sign for their property in compliance with the following standards.

- Builder Project signs are subject to the approval of the Administrator and the D'Andrea Design Control Committee.
- Signs shall be smaller than the Community and Project entry signs. Refer to Table 3-2 for size restrictions.
- Lighting may be incorporated into Builder Project entries, per the Lighting section.
- Temporary sales or directional signs are allowed to direct traffic to project and community facilities during construction and sales in compliance with City of Sparks code and Table 3-2. All temporary sales/directional signs shall be located on private property if granted permission from property or common areas if approved by DCC and Administrator. The design of all the temporary sales/directional signs shall be reviewed and approved by DCC and Administrator.
- Off-site Sales Sign (within D'Andrea) - one temporary project sign, to be removed when the model homes are sold.
- On-site Sales Sign - one temporary project sign, to be removed when the model homes are sold.
- On-site Directional - a maximum of 3 temporary directional signs are permitted for each new project or for resale of an existing home to be removed when model homes/existing home is sold.

- Model Home Sign - may be provided only as approved via the Site Plan Review for model home/sale office for the development.
- Custom Home Sign - one sign is permitted for each custom home.
- All signs shall be per SMC.

Table 3-2 - Sign Standards

Sign Type	Max #	Allowed Information	Max Square Feet Sign Face Each	Lighting	Note
Community Entry Monument	2	Project Name Project Logo	120 SF	Per Lighting Section and City Code	8' height max.
Project Entry Monument	2	Village Center Name Project Logo	75 SF	Per Lighting Section and City Code	8' height max.
Builder Project Entry (applies to multiple builders w/in a village)	2	Builder Name & Logo Project Logo Village Names	50 SF	Per Lighting Section and City Code	8' height max.
Vehicular Sign	As Needed	Traffic Regulatory and Directional Information, Street Names	Per MUTCD	None	
Pedestrian Signs	As Needed	Project Logo Pedestrian Orientation/Directional Information	6 SF	None	
Trailhead Signs	As Needed	Project Logo, Restrictive, Informational/Interpretive Information or Icons	6 SF	None	Comply With Project Uniform Graphics System 6' height max.
Commercial Building Mounted	Per SMC	Per SMC	Per SMC C I Zone	Per Lighting Section and City Code	Comply With Project Uniform Graphics System
Commercial Monument 8,000+ SF Pad	Per SMC	Per SMC	Per SMC C I Zone	Per Lighting Section and City Code	Comply With Project Uniform Graphics System 8' height max.
Commercial Monument >7,000 SF	Per SMC	Per SMC	Per SMC C I Zone	Per Lighting Section and	Comply With Project Uniform Graphics System

Sign Type	Max #	Allowed Information	Max Square Feet Sign Face Each	Lighting	Note
Pad				City Code	8' height max.
Commercial Monument Multiple Pads	Per SMC	Per SMC	Per SMC CI Zone	Per Lighting Section and City Code	Comply With Project Uniform Graphics System 8' height max.
Temporary On-Site Sales	One Per Village	Product Name Builder's Name Prices	24 SF	None	Comply With Project Uniform Graphics System. Remove within 15 Days of Final Model Home Sale 6' height max.
Temporary On-Site Directional	3 Per Village	Traffic Regulatory, Directional With Project/Village	4 SF	None	Comply With Project Uniform Graphics System. Remove within 15 Days of Final Model Home Sale 3' height max.
Custom Home	One Per Home	Builder's Name, Architect, Realtor, Owner, Marketing Info	10 SF	None	Comply With Project Uniform Graphics System. Remove within 10 Days of Sign Off of Final Inspection 6' height max.
Model Homes	1 Per Village	Builder's Name Village Name	10 SF	None	Remove within 10 days of final model home sales. 8' height max.

Site Furnishings

Site furnishings include benches, trash receptacles, bicycle racks, bus shelters, and picnic tables.

Site Furnishing Standards

Furnishings that range from urbane, detailed pieces for use in the projects entries, commercial/office, and residential areas to simpler pieces for open spaces and recreation areas. This approach creates a hierarchy of style that will augment the sense of arrival into the Community.

- Site furnishings shall be:
 - Constructed of durable, readily maintained materials
 - Designed to discourage vandalism and skateboard "riding"

- Located to facilitate maintenance
- Located to coordinate with adjacent paving, architecture, landscaping, and other amenities.
- At a minimum, site furnishings will be provided at trail heads, bus waiting areas, within the Parkway and collector street right-of-way and along pedestrian routes.
- Furnishings used in open spaces and recreation areas will feature simpler style and design to complement the landscape, subject to the approval of the Parks and Recreation Director, the Administrator and the D'Andrea Design Control Committee.
- Site furnishings located within the projects entries, golf course clubhouse/recreation center, commercial/office areas, and residential villages shall feature a higher level of detailing and style subject to review and approval of the DCC. All site furnishings shall be maintained by the DCA other than those located within the public park sites.

Lighting

D'Andrea's lighting enhances safety and function while promoting aesthetics. Lighting will vary from larger scale roadway lighting to more intimate pedestrian scale lighting. Lighting will be used to create mood and reinforce projects Northern Italian image.

General Light Standards

All lighting shall comply with the following standards. Standards that apply to specific areas are described later.

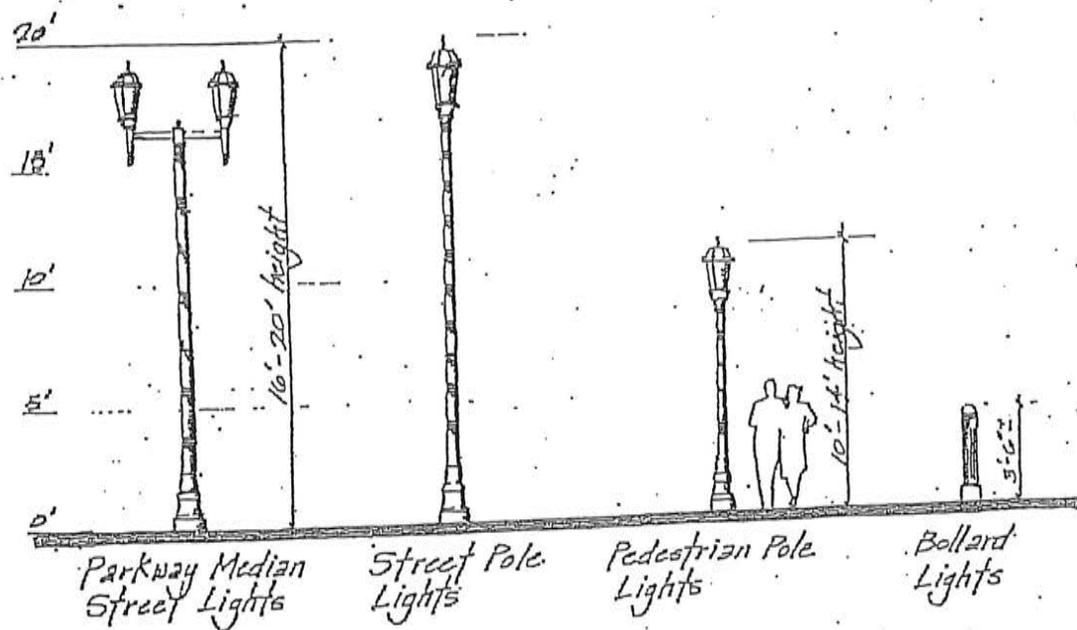
- Street lights within the Public Right of Way shall be in accordance with a street light survey that will be provided for approval by the Public Works Director. A development agreement for a lighting district will be required prior to the submittal of the first final map and/or public improvement plans. The agreement will be established such that the lights will be in accordance with Sierra Pacific Power Co. standards, maintained by SPPCo and the maintenance shall be paid for by the district. In addition, the agreement will specify that a supply of lights will be available at all times for immediate replacement. The agreement shall be written to the satisfaction of the Public Works Director.
- Fixture scale and illumination levels shall be consistent with the specific use.
- Lighting shall not extend beyond its tasks. Fixtures employing cut-off features, refractors, or housing shields to eliminate lighting spillover onto adjoining uses will be required.
- Energy efficient lighting design shall be used.
- Refer to Figure 3-25 General Light Standards.

Landscape Lighting

- Landscape lighting shall be used where appropriate to create mood and to accent focal points.
- When used, landscape lighting shall be soft and unobtrusive. Light shall be directed and/or shielded to prevent glare.

Sign Lighting

- Where sign lighting is provided, it shall be unobtrusive and shall relate to the design and character of the sign.
 - Internally lighted free-standing signs are prohibited.
 - Sign lighting shall be shaded, shielded, or directed to prevent the light from adversely affecting surrounding or facing properties or adversely affecting safe vision of pedestrians or operations of moving vehicles.
- Recessed lights shall have rock guards to prevent injury to pedestrians touching hot glass and to minimize vandalism. For directional light cut off and glare control, half shields shall be used on above grade fixtures where adjacent land uses or motorists could be affected.



- Light Design Concept shown are based on 'Towns Commons series', Architectural Area Lighting.
- Light Poles, Arms, Brackets to be Cast or extruded aluminum or steel.
- All exposed metal to be powder coated. Color(s) to be determined during final mapping.

Figure 3-25 General Light Standards

Community Light Standards

The Community Treatment includes street and pedestrian lighting within street rights of way and common areas.

Streetlights

- Street lights shall comply with City's street lighting standards and provide safe illumination of roadways minimizing glare. At a minimum, street lights shall be located at all intersections, pedestrian crossings, bus stops, and traffic circles.
- The scale and spacing of street lights will reflect the street hierarchy.
- All street lights shall be consistent and compatible throughout the project. Through the Maintenance Agreement, there shall be provisions for adequate maintenance and replacement when necessary.
- Refer to Figure 3-25 General Light Standards.

Pedestrian Lights

- Pedestrian scale lights such as lighted bollards or short (10'-14' height) pole lights will be used in pedestrian areas.
- Pedestrian lighting will reflect the level of activity intended for the specific area. High light levels are appropriate in intensive use areas such as shopping district or plazas with proper shielding. Low light levels are appropriate in more natural open areas. Where little or no light will be provided in adjacent areas, low lighting levels will be used to prevent "blind spots" at the interface between lit and unlit areas.
- Bollard lights along pathways will generally be located on a single side of the path rather than staggered on both sides of the path.
- Fixtures shall minimize glare.

Village Light Standards

- Lighting used in all village areas must match or coordinate with the style, colors, and materials of the community light fixtures, subject to the approval of the Administrator and the Design Control Committee. Lighting on private residential property is excluded from this standard.
- Refer to Figure 3-25 General Light Standards.

Commercial Lighting Standards

- Commercial area lighting shall coordinate with the associated building architecture.
- Decorative building lighting will clarify pedestrian routes and highlight building entries.
- At a minimum, commercial areas shall provide parking lot lighting and lighting at all pedestrian routes.
- Decorative building lighting shall be integrated with the architectural design of the building.
- Building illumination and architectural lighting shall be indirect in character. Architectural lighting will

accent and animate the building in addition to providing functional lighting for safety.

- Service area lighting shall be contained within the service area boundaries and enclosure walls to minimize light "spill-over" outside of service areas.
- Lighting fixtures shall be located to reduce shadow or interference from trees and other objects in the landscape.
- Parking lot lights will clarify vehicular and pedestrian circulation routes.
- Parking lot light standards shall not exceed 25' in height.
- Refer to Figure 3-26 Commercial/Office Light Standards.

Residential Lighting Standards

- Exterior fixtures mounted on buildings shall be no higher than the line of the first story eave or, where no eave exists, no higher than 12 feet above finished grade.
- Exterior fixtures will be provided over stairways, and second story entries for security and safety.
- Building lights shall be shielded to prevent light spillage onto adjacent property or streets.
- Refer to Figure 3-27 Single Family Light Standards and Figure 3-28 Multi-Family Light Standards.

Fences And Walls

A combination of transparent and opaque fencing and walls will be featured to reinforce D'Andrea's image and to provide buffering, enclosure, and separation of uses.

General Fence and Wall Standards

All fences and walls shall comply with the following standards. Standards that apply to specific treatment areas are described later. All fences and walls are required to be approved by the DCA, and City either in improvement plans for perimeter walls or building permits for all other walls.

- Adequate sight distances shall be maintained along roadways and intersections according to City of Sparks code and accepted engineering practices and roadway design speeds as determined by Public Works Director. Engineering will be provided for final map and/or building permit approval as needed.
- Fences/walls shall not exceed six feet in height, except where practical to accommodate grade change. The maximum height at grade changes is 7'-6". Pilasters shall not exceed eight feet in height except where wall height is increased to accommodate grade change. Refer to Figure 3-29 Fence Detail.
- Fences/walls shall not abut sidewalks. A minimum three foot landscaped separation is required between fences/walls and side walks. Refer to Figure 3-30 Landscaped Separation at Fences/Walls.

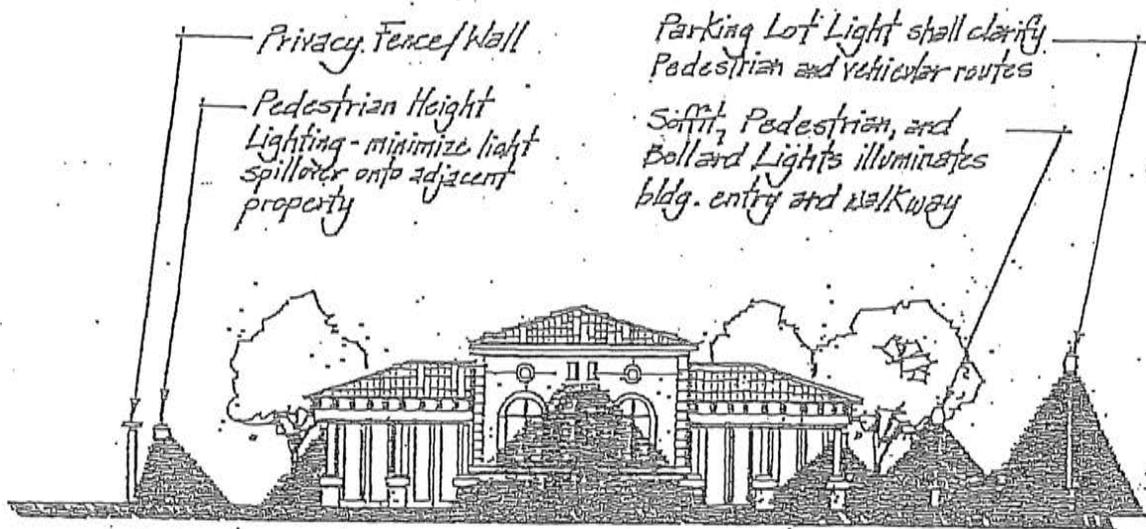


Figure 3-26 Commercial/Office Light Standards

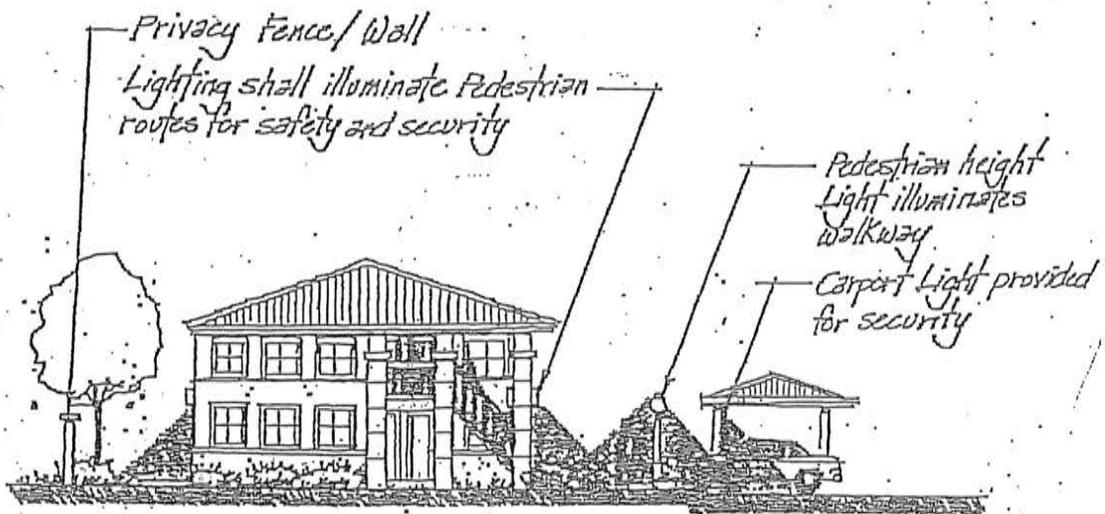


Figure 3-28 Multi-Family Light Standards

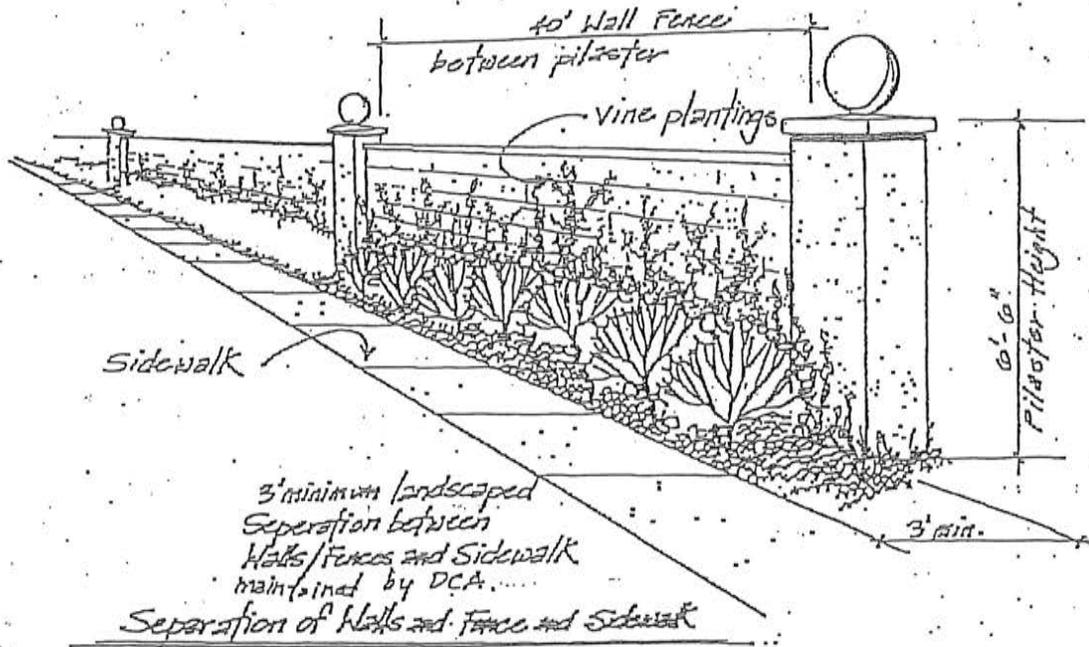


Figure 3-30 Landscaped Separation at Fences/Walls

- Retaining walls shall meet the specified area walls described below or shall be constructed of natural stone. Manufactured concrete block "systems" or Keystone walls shall be subject to the approval of the Design Control Committee. Wood retaining walls are not permitted with the exception of walls three feet or less in height within side and rear yards of residential lots.
- To maximize views, transparent fencing or no fencing at all is preferred over opaque fencing. Fencing and walls will be used in limited areas to reinforce the project image, define boundaries, provide privacy or retain graded slopes.
- The fence design and color shall be consistent or complement each other within each village to the approval of D'Andrea Design Control Committee and Administrator.
- Fences will be attractive from both sides (good neighbor fences).
- Wooden Fences and walls shall slope to accommodate grade changes.
- All fences shall be stained/painted the same color within a village to avoid color changes between properties within the same village.
- Decorative concrete walls will slope parallel to grade and where grade slope is 5% or more. Pilaster will be spaced 40' o.c. maximum.
- Top of fence shall be of uniform height.

Residential Fences

Residential fences define individual lots and provide security, privacy, and enclosure. Refer to Figure 3-31 Residential Fence Standards.

- Rear and/or side yard fences adjacent to parkway and collector streets will be opaque to screen views of rear yards and provide privacy for residents.
- Fence supports, such as pilasters and posts, shall be well defined and in scale with the purpose and context of the fence. They shall be coordinated in design and materials complementary to the primary building material and architecture.
- When used, pilasters shall be coordinated in design and materials complementary to the walls and fences.
- Color or stain of fences shall be maintained consistent within a village to avoid color/material changes between properties within the same village.
- No Chain-link fences will be allowed.
- Opaque fencing will be used only where needed for safety, security, and/or privacy.
- Opaque fencing or fencing exceeding 4' in height may only be installed in the rear and side yards of homes. Taller or opaque rear/side yard fencing must terminate into the side wall of dwelling at least five feet behind street side facades. Refer to Figure 3-32 Rear Yard Fencing.

- Open (6 foot maximum) fencing shall be used in rear yards that abut open space or golf course.
- Front yard fencing shall be limited to low, open fencing, not to exceed four feet in height. Fencing shall be a fully integrative and cohesive part of the architecture and landscape, subject to approval from the D'Andrea Community Association (DCC).
- Front yard hedge plantings are permitted, but shall be kept trimmed to a maximum height of 3 feet from the finished grade by the homeowner.
- A 2' wide landscape/utility strip runs across the front of all the residential lots fronting on residential street sections (Refer to Figure 3-7b Residential Street and Figure 3-31 Residential Fence Standards. All front yard fencing shall occur on the residential lot side of the landscape/utility strip.

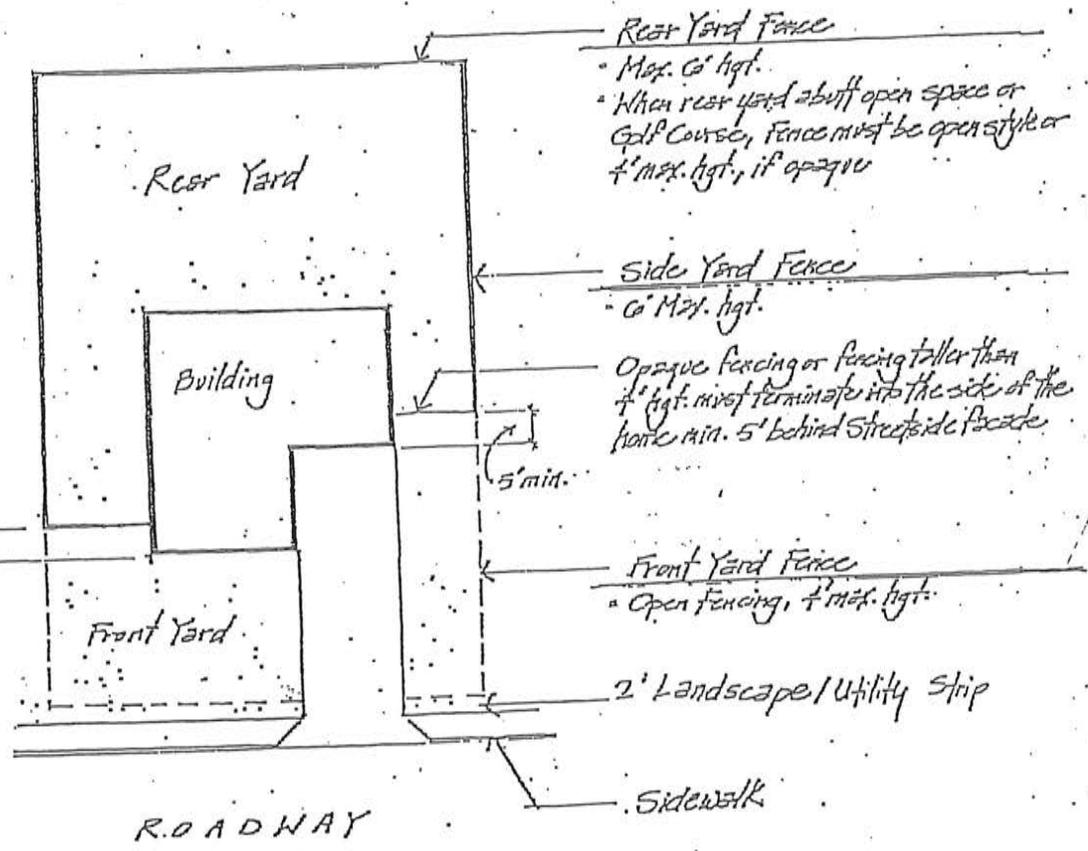
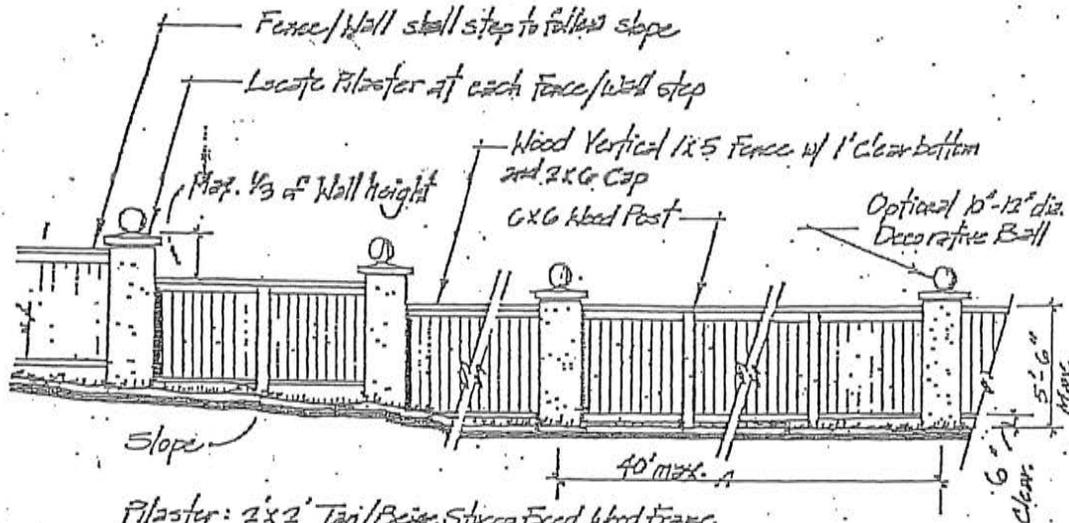
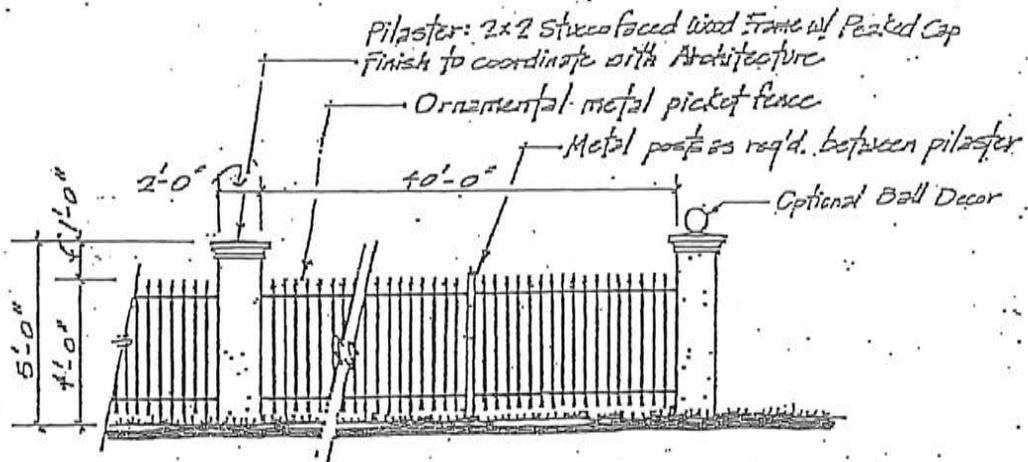


Figure 3-31 Residential Fence Standards



Pilaster: 2x2 Tan/Beige Stucco Faced Wood Frame
 Fence: Wood to be stained

WOOD FENCE CONCEPT



OPEN FENCE CONCEPT:

Figure 3-32 Rear Yard Fencing

Commercial/Office Fences and Walls

Commercial/Office fences and walls are those used within individual villages. Refer to Figure 3-33 Commercial/Office Fence Standards.

- A consistent fencing/wall program shall be required within each village, to be submitted at the time of the first final map within a village to the approval of Administrator and D'Andrea Design Control Committee.
- Commercial/Office fences and walls shall coordinate with the styling, detailing, materials, and colors of the associated architecture, subject to the approval of the Administrator and the D'Andrea Design Control Committee.
- Wall pilasters shall complement the walls and fences.
- Minimum 3 foot landscaped separation between sidewalk and fence shall be located outside of the public right-of-way, installed and maintained by commercial/office village.

Fences adjacent to Golf Course

Fences adjacent to Golf Course define golf course boundaries, individual lots and provide security, privacy, and enclosure. Refer to Figure 3-34 Fencing Adjacent to Golf Course.

- Fencing and walls will be used in limited areas to reinforce the project image, define boundaries, provide privacy or retain graded slopes.
- Fencing which incorporates openings or transparent fence type and are 4' or less in height shall be used where appropriate to allow residents to view surrounding open space and common area amenities.
- Fence supports, such as pilasters and posts, shall be well defined and in scale with the purpose and context of the fence and shall be coordinated in design and materials complementary to the primary building material and architecture to the approval of the DCC and Administrator.
- Color of stain of fences shall be maintained consistent within a village to avoid color changes/materials between properties within the same village.
- When used, pilasters shall be coordinated in design and materials complementary to the walls and fences.
- Opaque fencing shall be used only where needed for safety, security, and/or privacy, subject to the approval of Design Control Committee and Administrator.
- Rear residential yard adjacent to Golf Course shall be transparent (6 foot maximum) except as noted below.
- Side-rear yards adjacent to pedestrian entrances to golf course are permitted to have up to 6' height opaque or transparent fencing. Side-front yards adjacent to pedestrian entrances to golf course are permitted to have transparent fencing up to 6 feet in height.
- Opaque 6 foot high fencing is permitted on rear or rear side property lines adjacent to common areas located along collector streets.

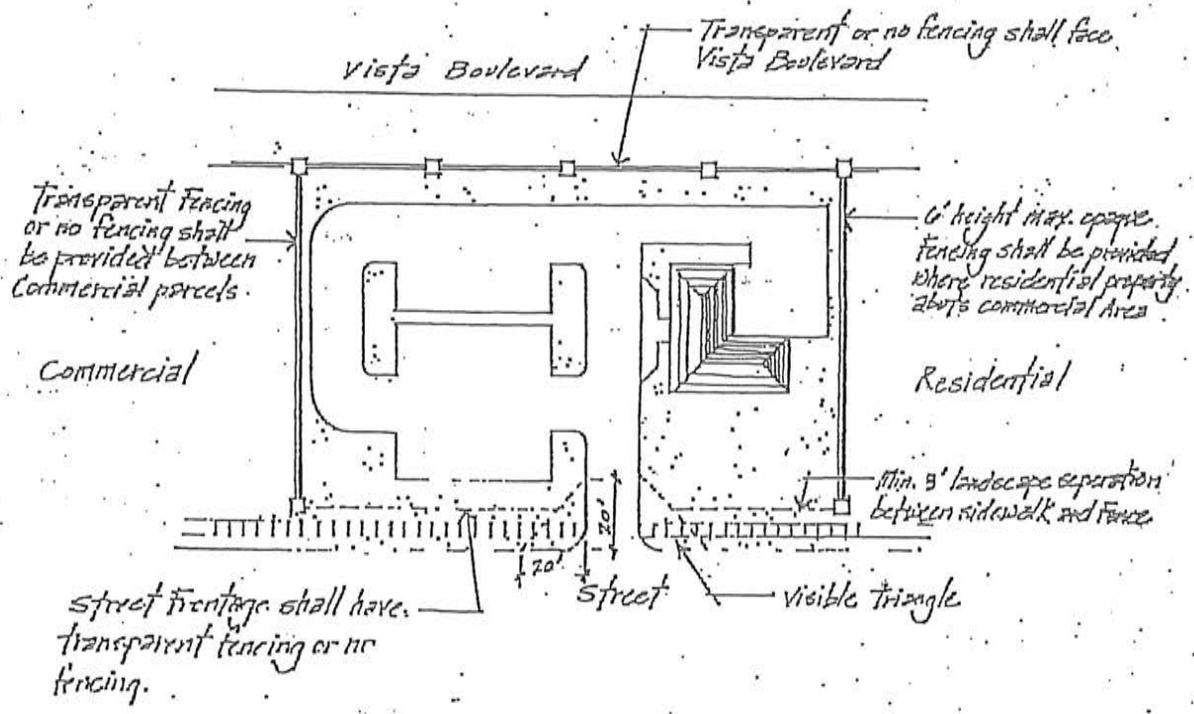
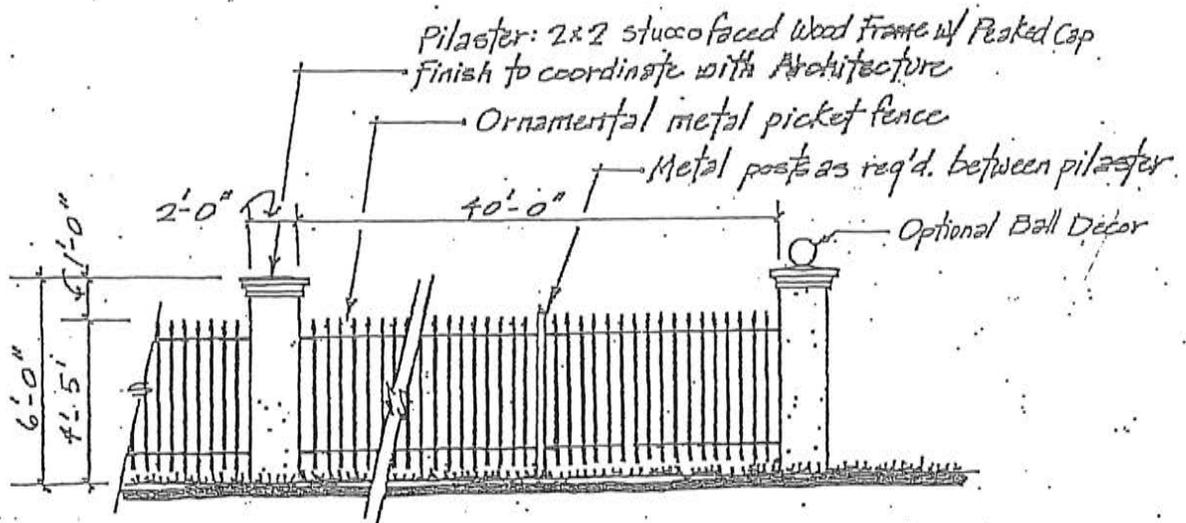
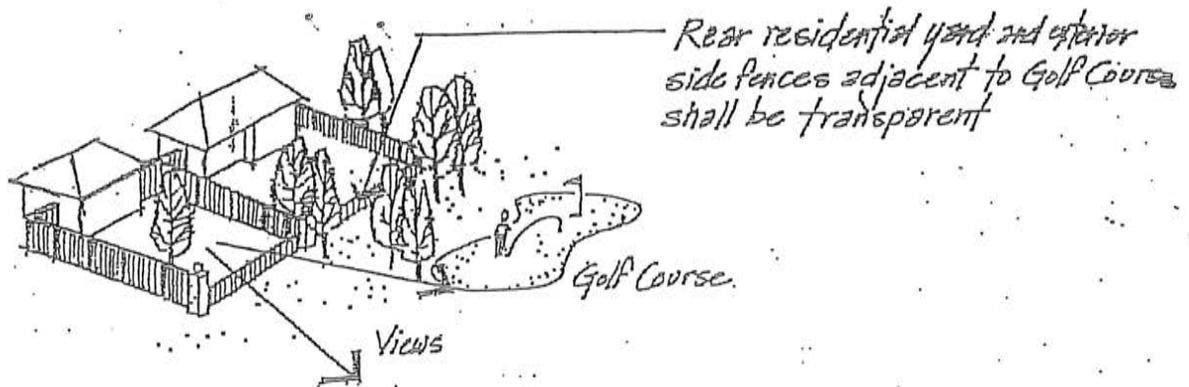


Figure 3-33 Commercial/Office Fence Standards



OPEN FENCE CONCEPT:

Figure 3-34 Fencing Adjacent to Golf Course

- Fences and walls shall have a space in front for landscape (natural and/or developed). A sidewalk will not directly abut a fence, but have at least 3 feet of landscaped area separating them installed by developer and maintained by the Golf Course operator, reviewed and approved by the DCC.
- Tops of fences and walls shall slope to accommodate grade changes.

Fences Adjacent to Open Spaces/Trails

Fences adjacent to Open Spaces/Trails define open space boundaries, individual lots and provide security, privacy, and enclosure. Refer to Figure 3-35 Fencing Adjacent to Open Space/Trail System.

- Fencing and walls will be used in limited areas to reinforce the project image, define boundaries, provide privacy, or retain graded slopes.
- Fencing which incorporates openings or transparent fence type and are 4 feet or less in height shall be used where appropriate, as approved by the DCC, to allow residents to view surrounding open space and common area amenities.
- Fence supports, such as pilasters and posts, shall be well defined and in scale with the purpose and context of the fence and shall be coordinated in design and materials complementary to the primary building material and architecture.
- Color of stain of fences shall be maintained consistent within a village to avoid color changes/materials between properties within the same village.
- When used, pilasters shall complement walls and fences in design and material.
- Opaque fencing shall be used only where needed for safety, security, and/or privacy, subject to approval from the Design Control Committee.
- Rear residential yard and exterior side fences adjacent to open spaces/trails shall be 4 feet height maximum, if opaque, except as noted below.
- Side-rear yards adjacent to open spaces/trails are permitted to have up to 6 foot high transparent fencing. Side-front yards adjacent to open space/trails are permitted to have opaque fencing up to 4 feet height.
- Opaque 6 feet high fencing is permitted on rear or rear side property lines adjacent to open space/trail areas located along collector streets per standards.
- Fences and walls adjacent to open spaces/trails shall have a space in front for landscape (natural and/or developed). A sidewalk shall not directly abut a fence, but have at least 3 feet of landscaped area separating them reviewed and approved by the DCC, installed by the developer and maintained by DCA.

Rear residential yard and exterior
side fences adjacent to open space/
Trail system shall be 4' max height
for opaque fencing

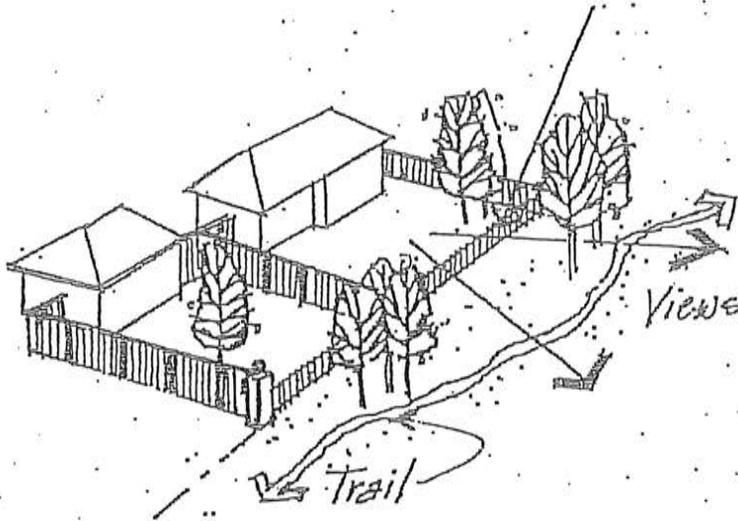


Figure 3-35 Fencing Adjacent to Open Space/Trail System

Mailboxes

- Mailboxes and mailbox clusters will be subject to the approval of the U.S. Postal Service and the Administrator prior to installation.

Architectural Standards

Purpose of the Architectural Standards

The purpose and intent of the architectural design standards for D'Andrea is to result in design that has a distinct identity, expressing a thoughtful integration of building structures and the environment. These standards will provide the City of Sparks and the residents of D' Andrea with the necessary assurances that this community will develop in accordance with the quality and character envisioned in the Specific Plan.

Community Character

A mixture of Northern Italian, French Normandy, and Craftsman vernacular will characterize the built environment of D'Andrea. These distinct, yet analogous, styles of architecture will be integrated within this master-planned community to create a strong sense of identity and community through the use of harmonious architectural concepts, colors, and materials.

Although the climate includes summer heat, gusting winds and temperate winters, the aesthetic beauty of the surrounding hillsides and distant mountains creates an Alpine environment. The open space of D'Andrea sets the tone for a strong architectural character where the buildings are solid, firmly based on the ground and in harmony with the shapes, angles, and colors of the foothills and surrounding mountains.

The Northern Italian architectural character will be used for all the major community buildings (golf clubhouse, recreation center, and office/retail center) and entry monumentation and landscape thus creating an overall harmonious theme amongst the various individual neighborhoods.

Architectural Styles

In an effort to promote a diversity of architectural styles, a theme has been developed incorporating styles that are popular in this type of geography in Europe and are reminiscent of the housing styles of old Southwest Reno. The styles are referred to as Northern Italian, French Normandy, and Craftsman/Bungalow. A mix of architectural styles is encouraged in individual neighborhoods (custom homes, predesigned housing, and multifamily housing) thus helping to create that traditional neighborhood feeling with a variety of styles.

These styles of architecture are illustrated on the following pages on various image collages and elevation sketches showing the various components that make up these styles. The examples of architectural styles which are presented to provide guidance and inspiration in creating this image. By no means, however, is exact reproduction of these styles mandated. Contemporary interpretations of these styles using the visual connection of these images and tradition will be implemented. The architectural styles shall dictate the minimum standards for the built environment within D'Andrea.

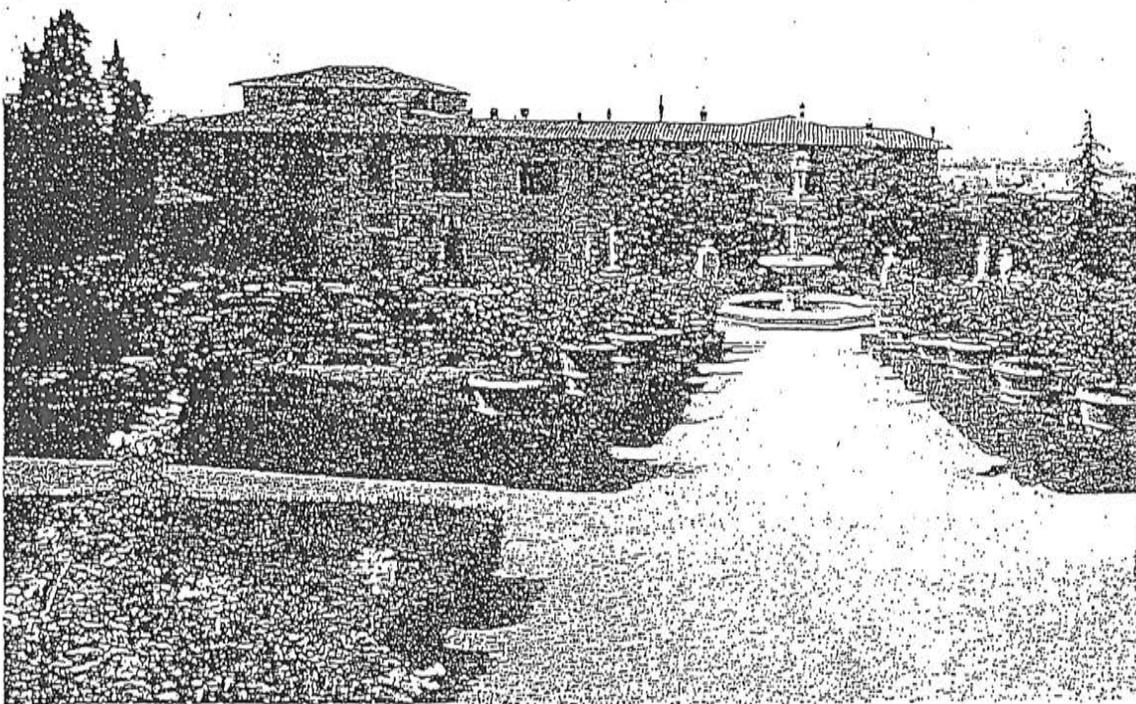


Figure 3-37 Northern Italian Architectural Character

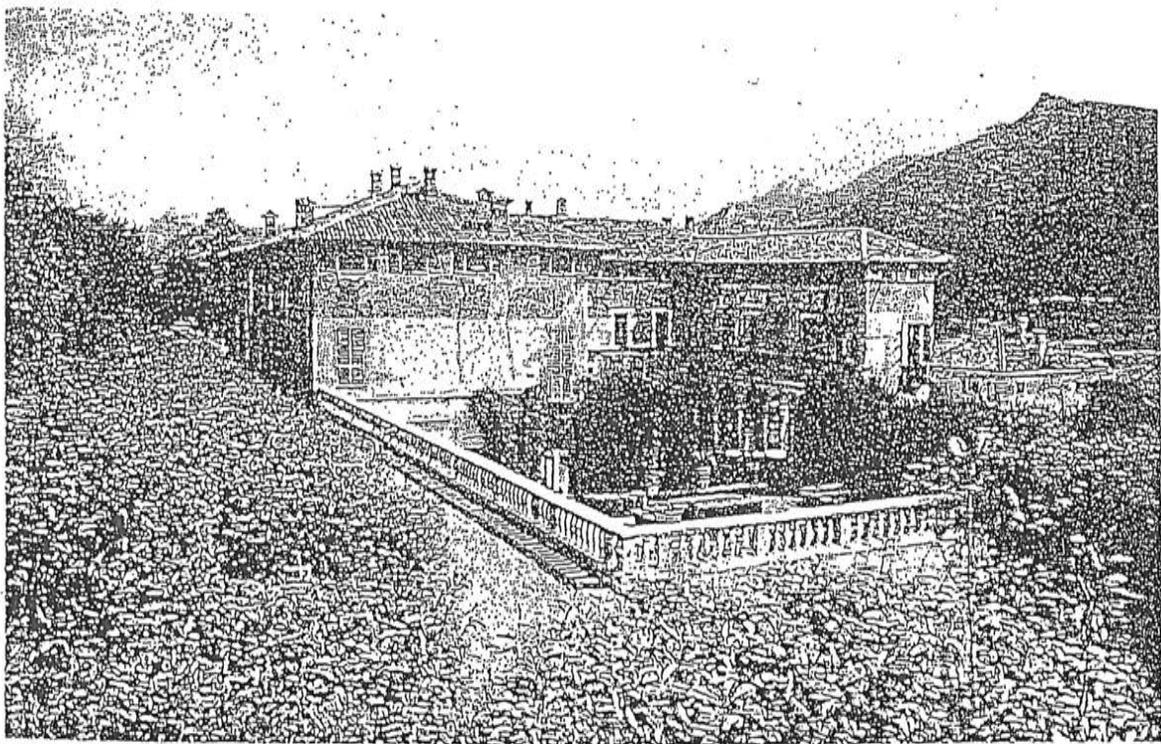


Figure 3-38 Northern Italian Architectural Character

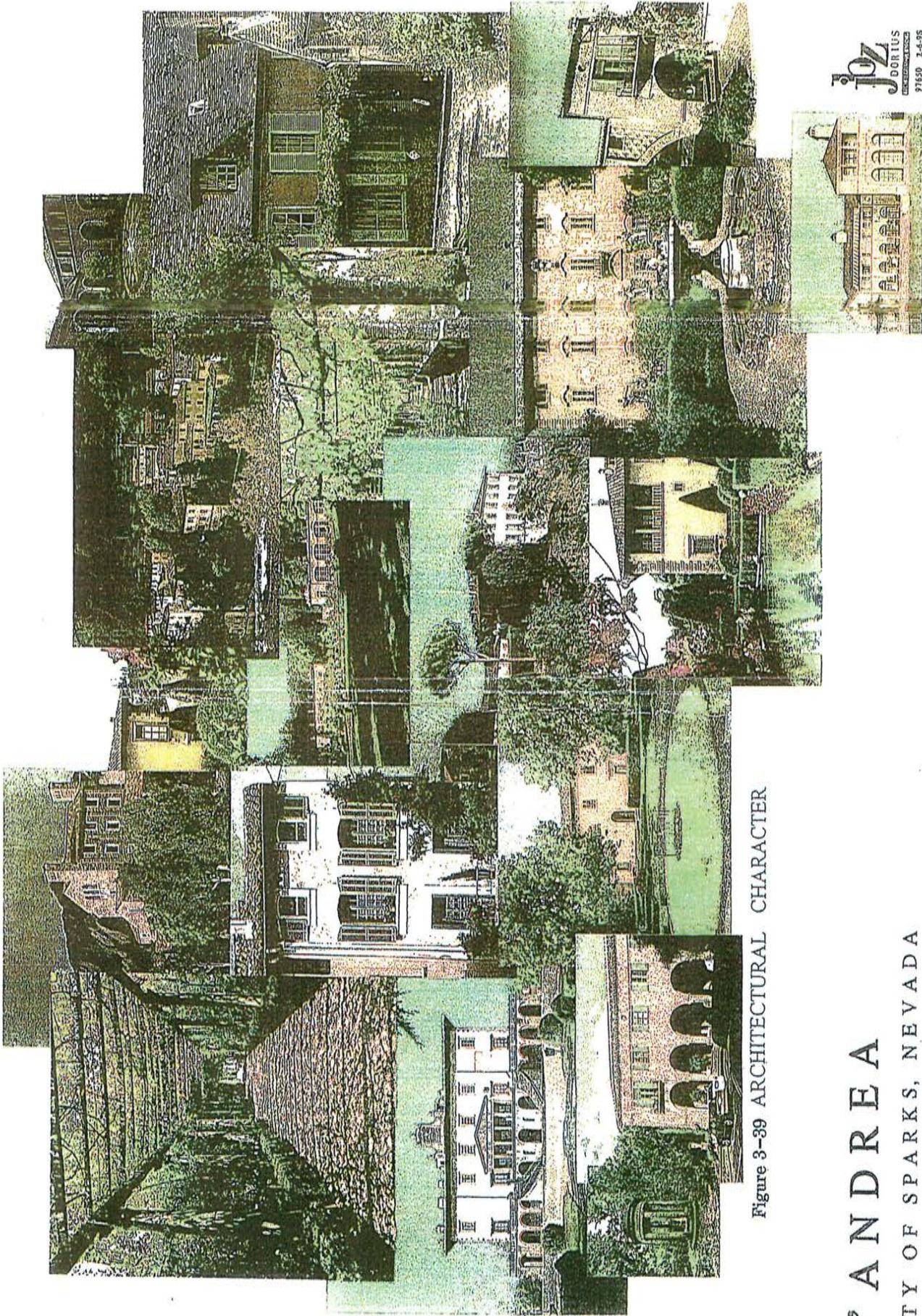


Figure 3-39 ARCHITECTURAL CHARACTER

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D'ANDREA
 CITY OF SPARKS, NEVADA

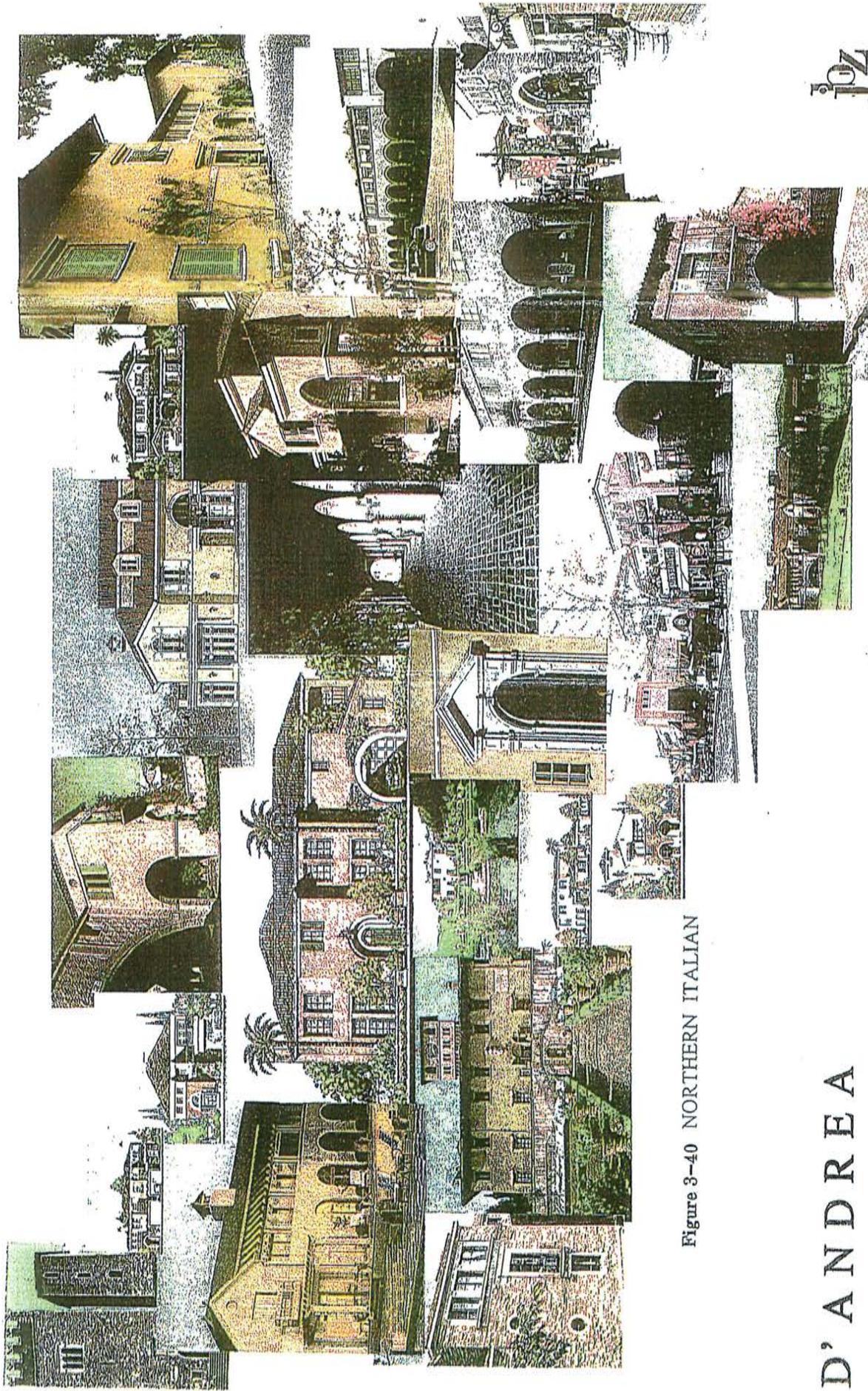


Figure 3-40 NORTHERN ITALIAN

D'ANDREA

CITY OF SPARKS, NEVADA

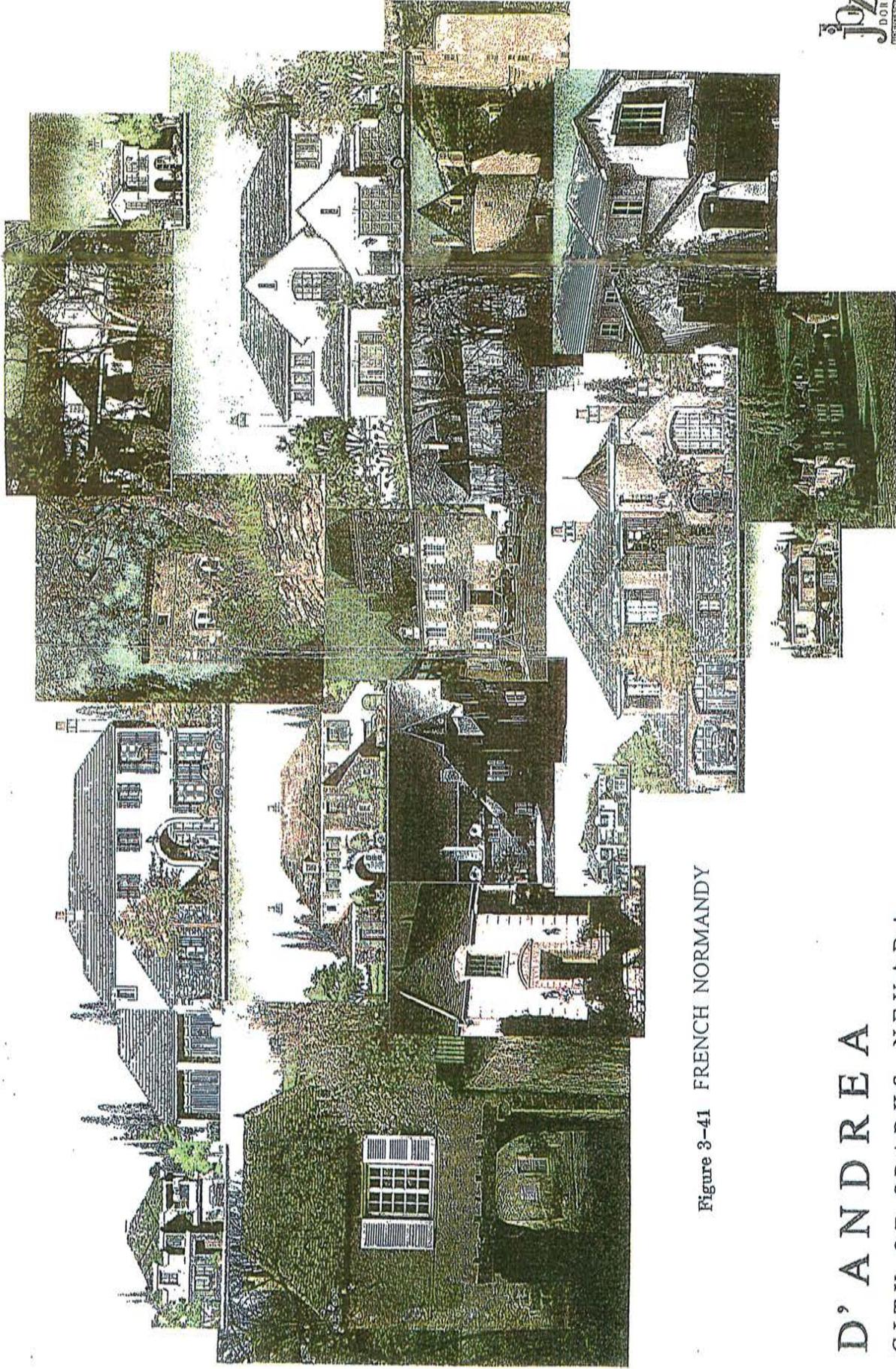


Figure 3-41 FRENCH NORMANDY

D'ANDREA

CITY OF SPARKS, NEVADA

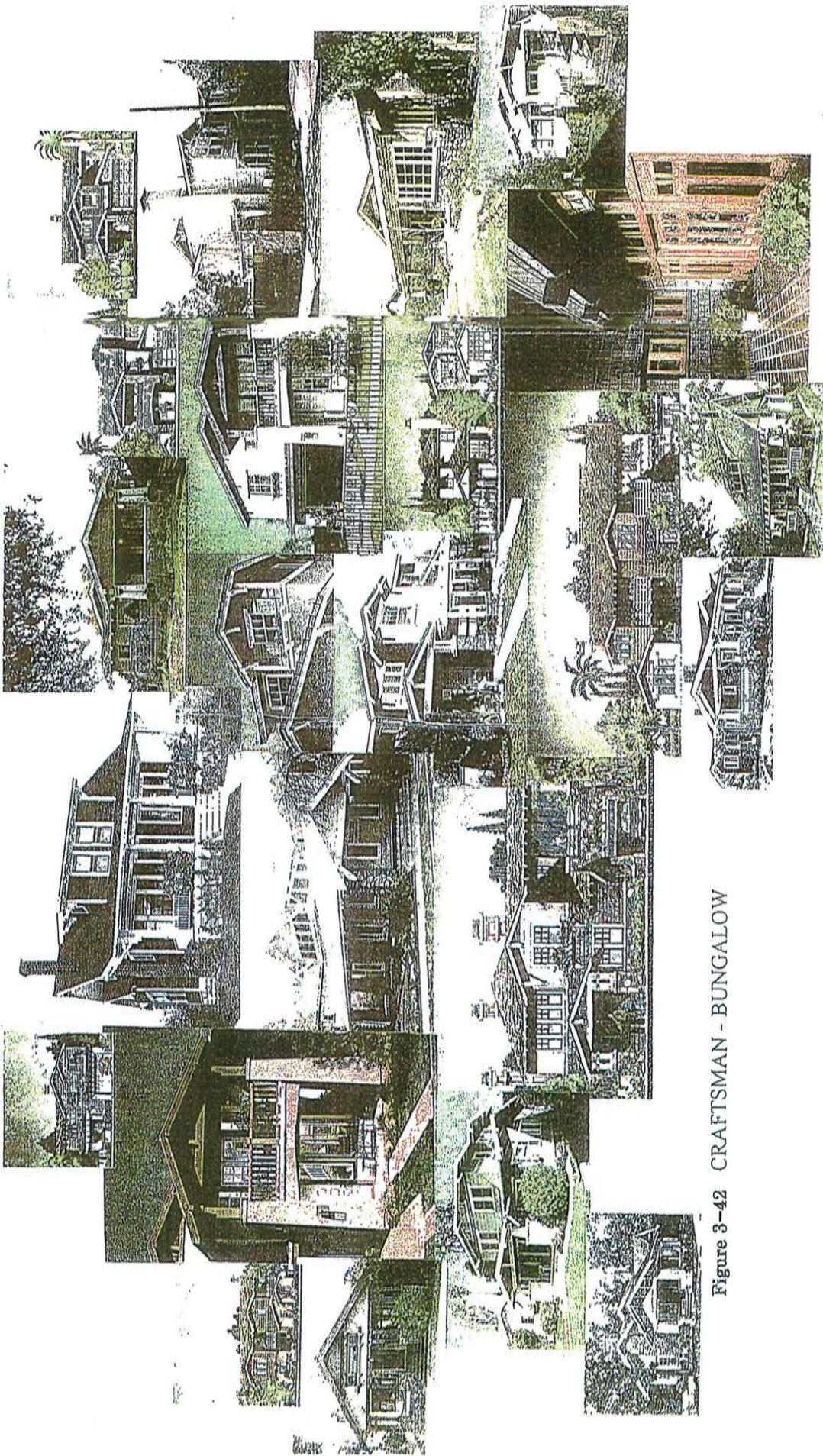


Figure 3-42 CRAFTSMAN - BUNGALOW

D'ANDREA
CITY OF SPARKS, NEVADA

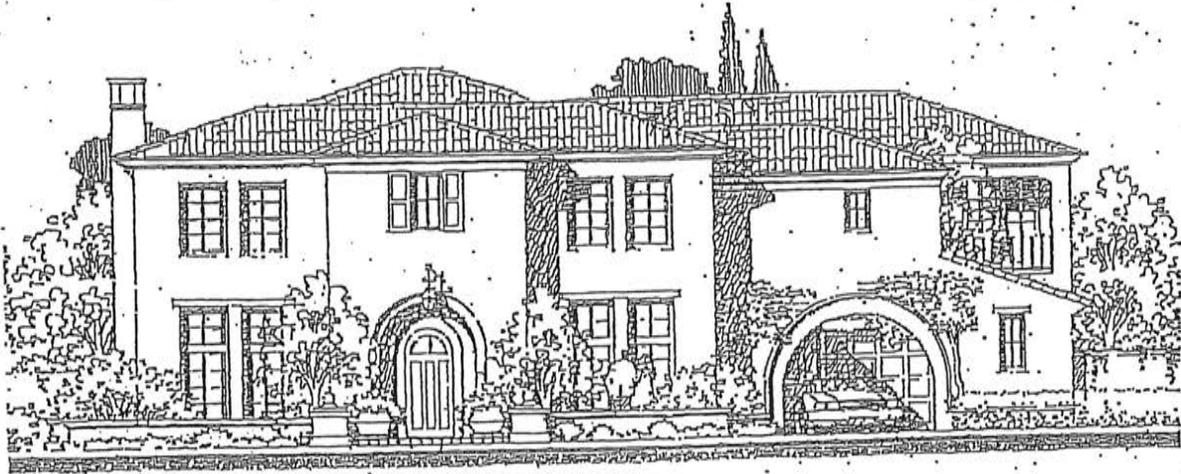


Figure 3-43 Example of Northern Italian

The Northern Italian style is characterized by the following elements:

- Hipped roof of low pitch, usually covered with tile (no or few gable roofs)
- Widely overhanging eaves
- Upper-story windows smaller and less elaborate than the windows below
- Arches above doors, windows, and porches on the first story
- Facade is usually symmetrical
- Substantial Cornices
- Arched Multipaned Doors
- Symmetrical Stone or Stuccoed Structures
- Some shutters
- Entrances - Projecting Portico or Recessed Loggia
- Light colored Mediterranean - Earth tone colors

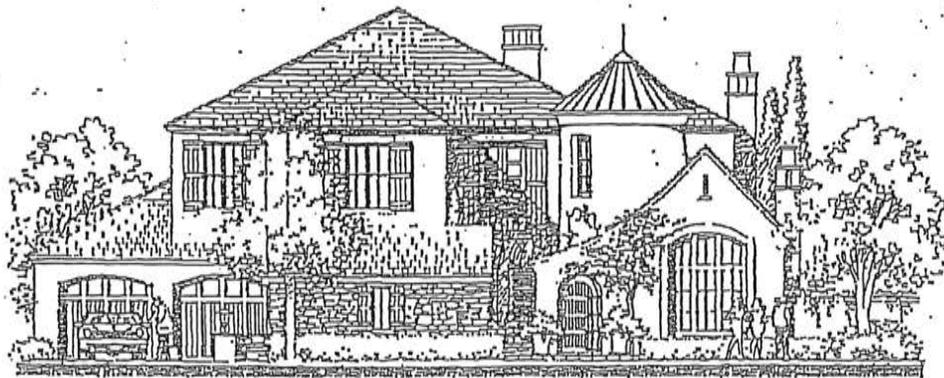


Figure 3-44 Example of French Normandy

The French Normandy style is characterized by the following elements:

- Tall, Steeply pitched (6:12 or greater), Hipped roof without dominant front-facing cross gable
- Circular stair towers
- Eaves commonly flared outward at roof - wall junction
- Substantial Rubblework Masonry
- Shutters
- Loose/Free form wrought iron
- Earthtone colors (tans & greys)

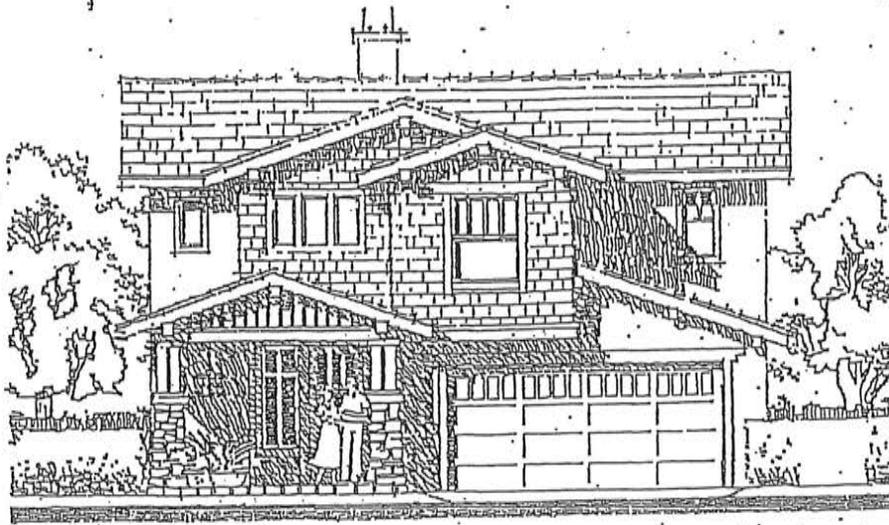


Figure 3-45 Example of Craftsman/Bungalow

The Craftsman/Bungalow style is characterized by the following elements:

- Low pitched gabled roof (occasionally hipped) with wide unenclosed eave overhang
- Roof rafters usually exposed
- Decorative Beams or Braces under gables
- Sloping (battered) foundation in random texture
- Full width or partial front porches
- Pergolas & Trellises over the porches
- Porch supports usually squared and sometimes slanting inward
- Grouped windows
- Split wood shingles on upper floors
- Cut - out brackets
- Natural Colors

The design of Bella Vista homes will be of one of the three styles defined as Northern Italian, French Normandy, and Craftsman/Bungalow and will draw their inspiration from the images previously shown in this document. An elevation of each style is shown to give a feel of the level of detail, massing, and scale that is necessary for homes. Refer to Figures 3-46, 3-47, and 3-48 for examples of architectural styles.

The following are Bella Vista Home standards.

- Bella Vista Homes shall exhibit greater variety and attention to detail in all of the styles.
- Bella Vista Homes shall be responsive to their unique characteristics of each site and should compliment any natural site features.
- Bella Vista Homes shall achieve proper proportions, scale of details, and the use of proper ornament.
- Excessive massing on either the streetscape or open space is not allowed.
- Bella Vista Homes shall be compatible with each other in terms of size, mass and scale.
- Bella Vista Homes shall avoid large expanses of uninterrupted single exterior massing and exterior materials
- Garages Doors shall be placed to limit direct view from the street using a minimum 15' setback behind the front of main facade of the house, side entry garages, rear entry garages or separate garages to be located 10' behind the rear of the main house.



Figure 3-46 Example of Northern Italian Style Home

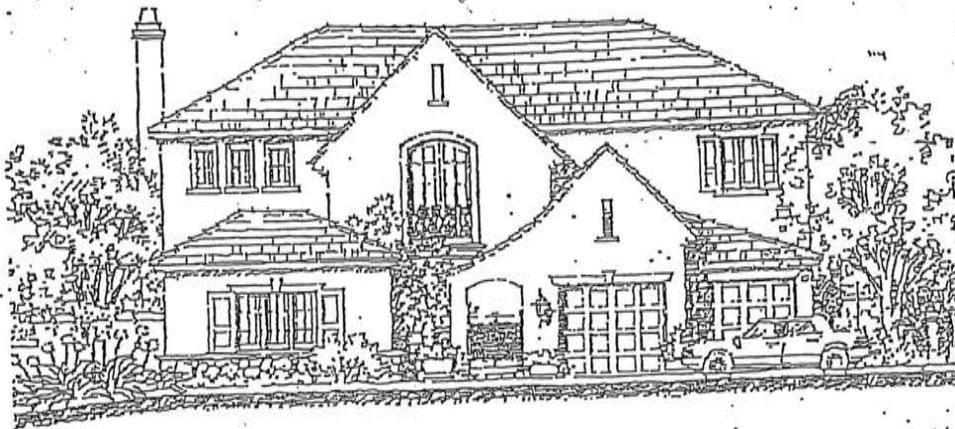


Figure 3-47 Example of French Normandy Style Home

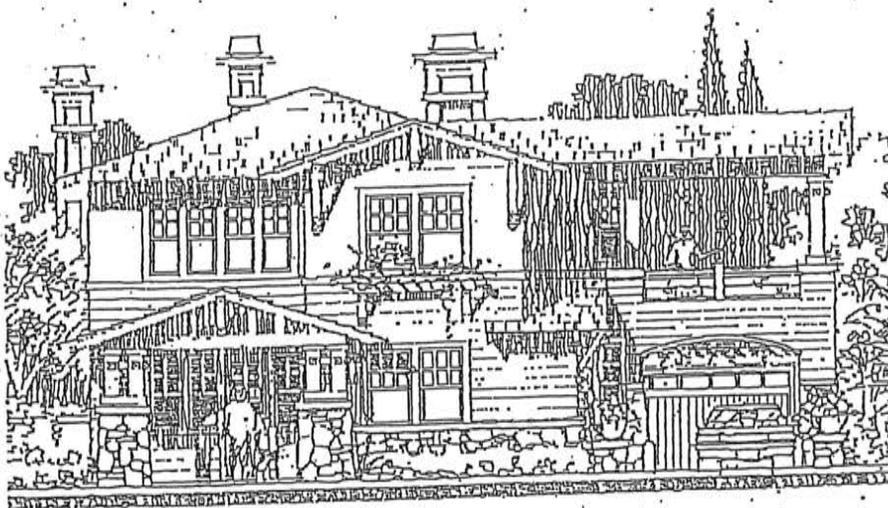


Figure 3-48 Example of Craftsman/Bungalow Style Home

Single Family Predesigned Home Design Standards

The design of predesigned homes shall reflect the three styles discussed previously:

- Appropriate Styles: Northern Italian, French Normandy, Craftsman/Bungalow
- Additional styles may be added subject to approval of the DCC, Administrator, Planning Commission and City Council.

An elevation of each style is provided to illustrate the style including details, massing and scale. Refer to Figures 3-49, 3-50, and 3-51 for examples of the three architectural styles.

The following are Predesigned Home standards that are required.

- Homes shall be compatible with one another in terms of size, mass, and scale.
- Architectural styles shall be compatible between adjacent buildings and within neighborhoods villages.
- Large expanses of uninterrupted single exterior materials shall be avoided.
- Building design will employ a combination of vertical and horizontal articulation.
- Houses shall not be designed or sited so as to fully block or shade private open space of adjacent units.
- Garages shall not dominate the primary elevation of any home. A mixture of side and recessed garages are encouraged and excessive repetition of any particular garage configuration is not allowed.
- Excessive numbers of three car garages facing the street are not allowed.
- Recessing the garage to the back of the house is encouraged as well as stepping the garage and creating a variety of roof configurations is required.
- Variation of building height and setbacks are required to create an interesting streetscape. Building facades should be designed to avoid straight blank walls. Two story wall surfaces on the Craftsman/Bungalow style shall be articulated to give scale and to break up a flat appearance.
- Rear elevations adjacent to arterial streets shall be enhanced to improve the community image. Examples of enhanced elevation treatment include architectural detailing, architectural relief of undifferentiated planes, multiple roof plans, and window trim.
- Building elevations shall include a difference in massing, building shape, roof plane orientation or composition of materials such that no more than two adjacent buildings on a block appear to be very similar to the approval of the DCC. Identical elevations and color schemes shall not be repeated to the approval of the DCC.
- Front porches are encouraged as long as they are usable.
- Downhill cantilevers and tall support poles for overhanging areas are not allowed.
- When the building site has topography, buildings shall be stepped to conform to the slope.
- Ground mounted equipment will be screened from the street.
- All mechanical equipment, including any pool equipment shall be located a minimum of 5' from any adjacent property line.
- Provide a location for trash containers out of site from the street and the neighbors.
- Building materials and color schemes shall be consistent with the chosen architectural style.
- Accent colors shall be used for shutters, trim, balcony rail, stucco recesses, or cornice bands and shall relate to the architectural character of the building.

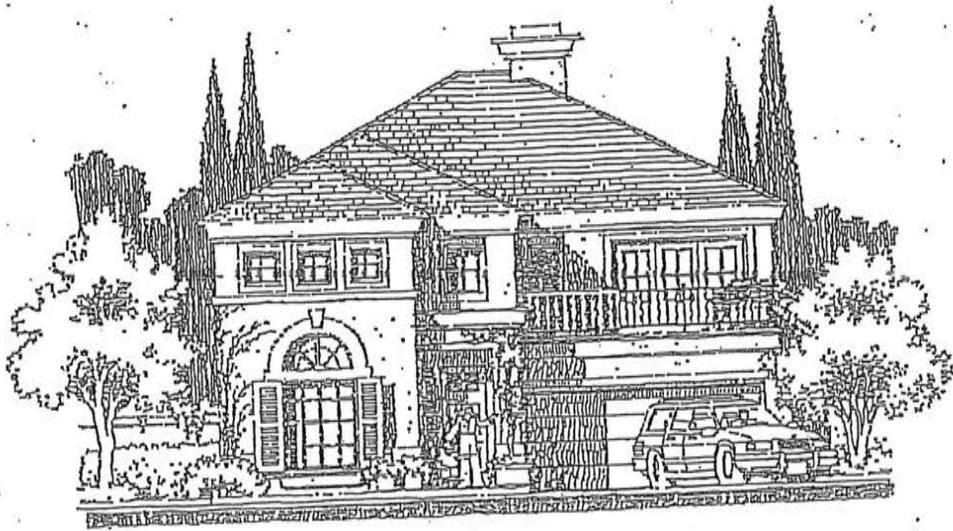


Figure 3-49 Examples of Northern Italian Style Predesigned Home

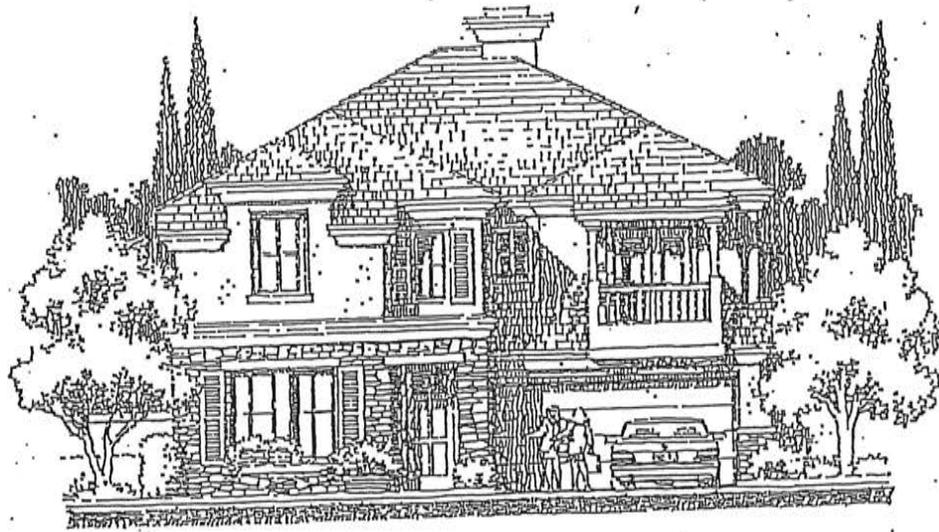


Figure 3-50 Examples of French Normandy Style Predesigned Home



Figure 3-51 Example of Craftsman/Bungalow Style Predesigned Home

Multi-Family Design Standards

The multi-family design shall incorporate one of the approved architectural styles of Northern Italian, French, Normandy, or Craftsman/Bungalow.

An elevation of each style illustrates the concept including details, massing and scale. Refer to Figures 3-52, 3-53, and 3-54 for examples of architectural styles.

The following are Multi-Family Home standards that shall be followed.

- Building heights may increase as they step back from the property line to the maximum building height.
- Rooflines shall slope to complement the surrounding topography.
- Buildings shall be clustered to reduce the scale of the development, but be designed to provide adequate air, light and open space for their occupants.
- Buildings shall be articulated with variations or interruptions of surfaces or planes through the use of staggered vertical planes, multiple rooflines, insets such as windows or doorways, balconies, projections or other similar features.
- No more than two adjacent units shall be covered by a single, unarticulated roof. Roof articulation may be achieved by changes in plane of no less than two feet, use of gables, hips, dormers, chimneys, inserts or openings.
- Building design shall ensure that windows and doorway entries are incorporated into design of front elevation in order to decrease the visual prominence of the garage.

- Each structure or composite of units shall have a different facade material or color than the immediately adjacent structure/composite, to replicate a single family style neighborhood and promote individuality.
- Patio fences shall be designed to be architecturally consistent with the residential architecture and shall provide a balance between privacy for the individual unit while allowing views of the major open space areas.
- Private entry walks shall be provided for each unit.
- Exterior stairways shall be architecturally consistent with the building they serve and shall be integrated into the building.
- Buildings shall incorporate overhangs, insets or fenestration that are consistent with its architecture style, so as to allow the low winter sun to penetrate the unit, while blocking out the high summer sun.



Figure 3-52 Examples of Northern Italian Style of Multi-Family Homes

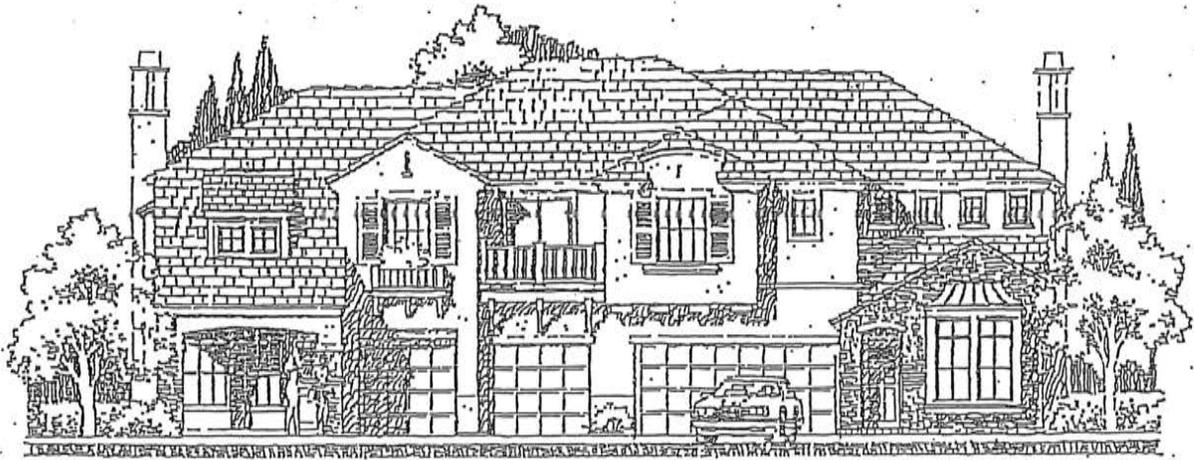


Figure 3-53 Example of French Normandy Style of Multi-Family Homes



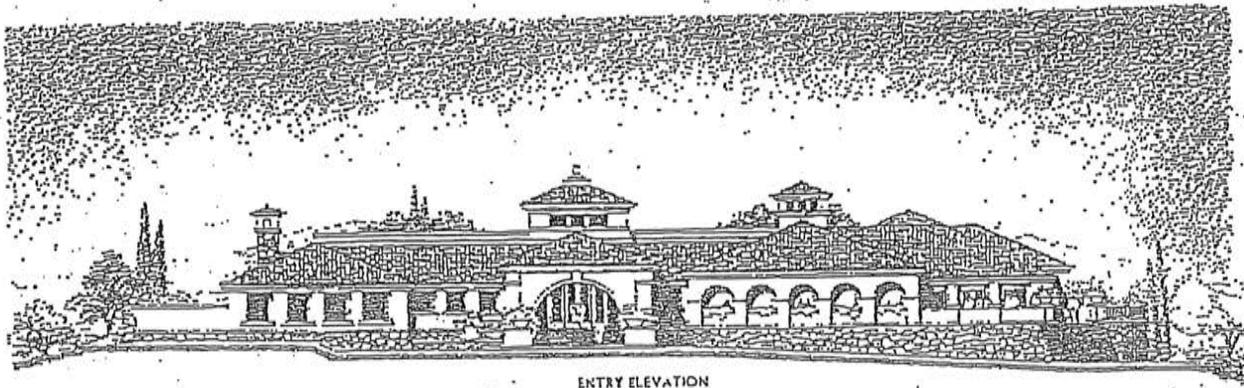
Figure 3-54 Example of Craftsman/Bungalow Style of Multi-Family Home

Commercial and Golf Course Clubhouse Standards

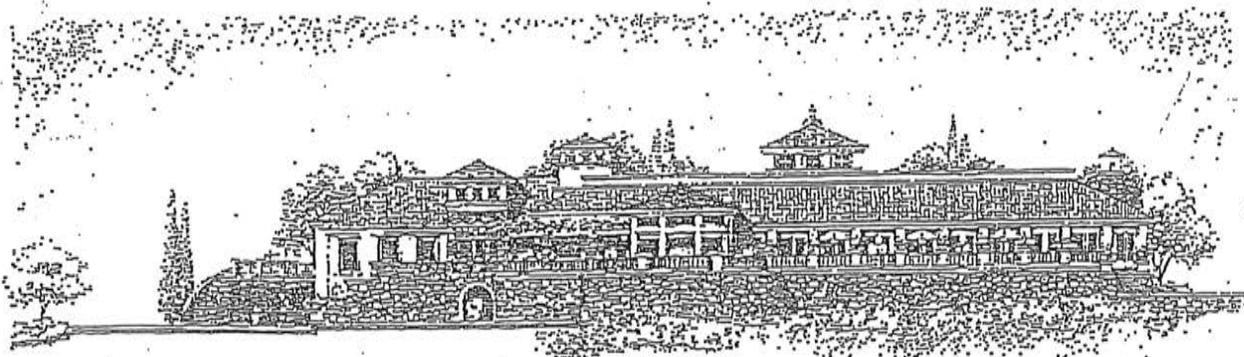
The commercial/golf course clubhouse design will establish the overall architectural theme and therefore, the appropriate style is Northern Italian. Refer to Figures 3-55, 3-56, and 3-57.

The commercial buildings and golf course clubhouse building are areas that offer an opportunity to reinforce the community character of D'Andrea. The following standards will ensure a positive impact to D'Andrea, as well as the City of Sparks. All commercial and golf course clubhouse building are subject to the following Design Standards.

- Fountains, plazas, sculptures, and other focal points shall be used as the center or confluence for several buildings.
- Long facades that face public views shall be articulated by varying building mass, form, texture, and interplay of solid and open areas.
- Columns, projections and exterior detailing shall be used to architecturally demarcate and enhance buildings. Colonnades may be used with this style of architecture.
- Ground level floors of buildings adjacent to walkways and roadways should be architecturally articulated with the use of smaller, more pedestrian scale features, such as building mass, roof lines, colonnades, signage, and planters.
- Entryways shall be recessed or covered with some kind of architectural element, such as; a trellis, a roof projection, etc.
- Exterior fenestration may have a repetitive pattern but it shall not be excessive.
- Signage shall be incorporated into the overall design.
- Building materials shall be used to create interest, focus, unity and compatibility.
- Enhanced paving shall be used at all entryways. Textured paving should also be used along all pedestrian walkways and vehicular crossings to enhance safety where vehicles and pedestrians interact.
- Low pitched roofs shall be used on all commercial and community buildings per the Northern Italian Design Standards as listed on page 3-77 of the Final Design Handbook.
- Strong vertical elements shall be used to identify important buildings within D'Andrea.



ENTRY ELEVATION



GOLF COURSE ELEVATION

Figure 3-55 Northern Italian Style Clubhouse Building



Figure 3-56 Northern Italian Style Commercial Building

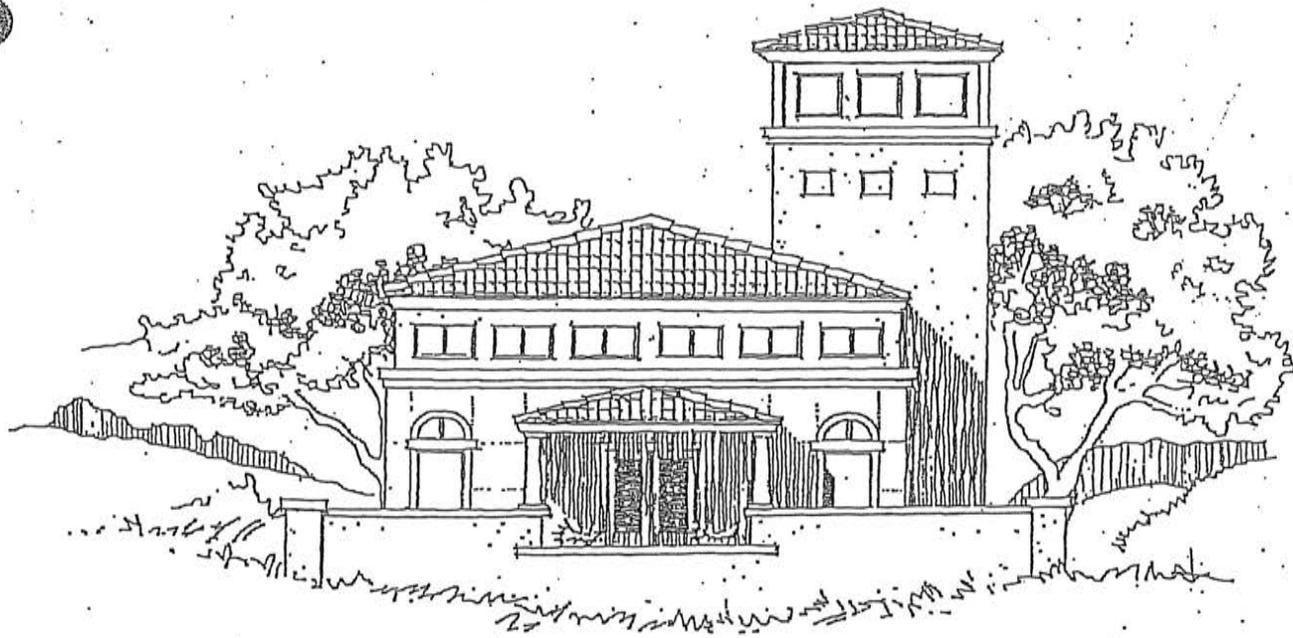


Figure 3-57 Northern Italian Style of Preview Center

Exterior Building Finish Colors

Exterior colors will blend with the surrounding environment. Acceptable exterior colors include tones and hues of beige, grey, brown, pale yellows, and any other subdued color approved by the D'Andrea Design Control Committee. Primary colors are prohibited, except as an accent color in non-hillside areas only, or when located on the building so as not to be visible from off-site or adjacent off-site areas.

Interpretation of Design

The preceding architectural styles identify a multitude of design elements and finish materials. These sketches are not intended to establish precise standards for building design, exterior finish and details of residences within D'Andrea. These elevations provide a thematic palette from which the individual designer can choose. Designers may create individualistic interpretations of the identified architectural styles, but these interpretations shall incorporate the architectural elements listed with the chosen architectural style. However, interpretations will be generally traditional in nature. Experimental or overly contemporary architectural interpretations shall be prohibited. Traditional interpretations may be achieved through the utilization of modern building materials including anodized aluminum frame windows, masonry veneers, fire resistant simulated wood roof materials. The architectural design shall be approved by D'Andrea Design Control Committee, prior to submittal to the City for entitlement and/or building permits.

Screening of Exterior Mechanical and Electrical Equipment

Any equipment, whether on the roof, side of building, or ground, shall be screened. The method of screening shall be architecturally integrated in terms of material, color, shape, and size. The screening design shall blend with the building design. Where individual equipment is provided, a continuous screen is required.

All rooftop utility and mechanical equipment shall be screened by the actual building elements rather than an open lattice enclosure. The parapet profiles shall equal the height of adjacent rooftop equipment. All mechanical and utility equipment shall be painted in colors compatible with the color of the roof. Refer to Figure 3-5B Mechanical Screening.

Mechanical screens and penthouse structures are to be integrated with the facade of the building. Screens shall be continuous and solid (no picket fences permitted). Roof "hats" are not permitted. Variations in roof height and profile shall be used to conceal mechanical equipment and define entry areas. Refer to Figure 3-59 Cross Section.

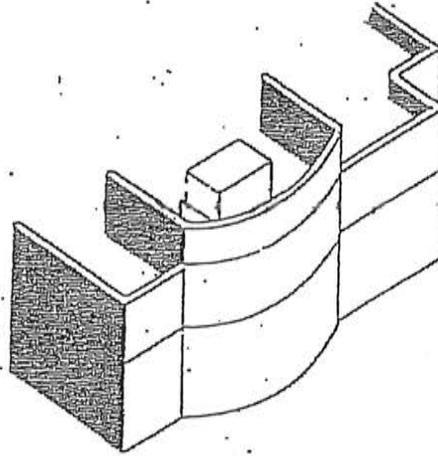


Figure 3-58 Mechanical Screening

The height of the adjacent parapet will be equal to or greater than the adjacent mechanical equipment or other component.

COMPONENTS NOT TO EXCEED HEIGHT
OF PARAPET OR PENTHOUSE

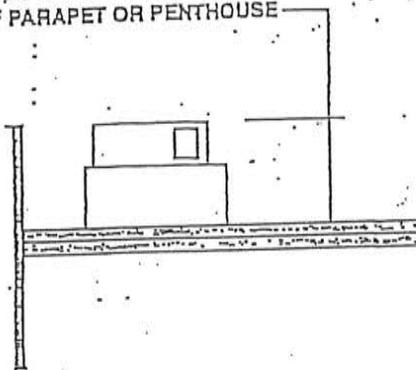


Figure 3-59 Cross Section

Screening for Service and Loading Areas

Service areas, trash enclosure, and communication devices shall be screened from public right of ways and adjoining residential properties with doors, walls or landscape buffering.

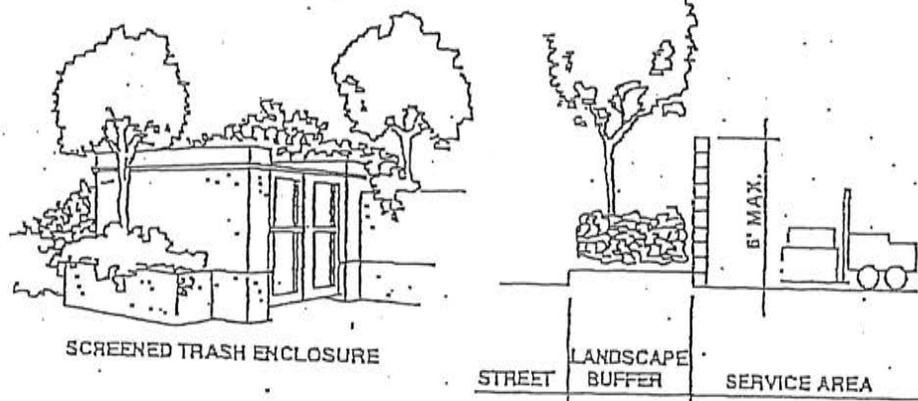


Figure 3-60 Service/Loading Areas

Service and loading areas shall be screened from Primary Visual Exposure Areas (Examples: Streets, primary image entry drives, floors of office buildings, building entries, common visual and recreational amenity areas).

Screening shall include one or a combination of the following:

- Masonry wall
- Berm
- Landscaping

Building orientation and site layout on lots adjacent to residential areas shall ensure the privacy of residential uses.



DESIGN HANDBOOK
SUPPLEMENTAL
APPENDICES

Definitions

The following defines words and terms used throughout the D'Andrea Standards Handbook. Any words or terms used in the D'Andrea Standards Handbook which are not defined in this Handbook shall have the meaning ascribed in the Sparks Municipal Code.

Accent Tree means a tree, typically deciduous, whose distinctive leaf or flower color draws attention to site features or warns of potential conflicts at intersections and village entries. The mature height is generally under 30 feet in height.

Accessory building means a detached building or structure that is subordinate to, and the use is incidental to the main building, structure or use of the same parcel/lot or development.

Administrator means City of Sparks Administrator or Public Works Director or their representative designee.

Bella Vista Homes is a term used to describe housing within D'Andrea. These lots are the largest in the D'Andrea Community and are typically located in the areas of most dramatic terrain and best views. These lots are larger, ranging from 10,000 square feet to more than an acre in size.

Canopy Tree (or Shade) means a tree, typically deciduous, whose leaf pattern and branching pattern displays characteristics of near equal height and width, providing shade. The mature height is generally over 30 feet in height.

DCA is the abbreviation for the D'Andrea Community Association which is the D'Andrea development master planned community's master homeowners association.

DCC is the abbreviation for the Design Control Committee which serves as DCA's architectural control board including one (1) members of DCA, two (2) representatives from D'Andrea Nevada LLC and the City of Sparks Administrator or his designee. The Administrator or his designee shall participate for a maximum of one (1) year from the date of the approval of the Design Handbook by City Council.

Evergreen Tree means a tree which retains its needles or leaves year round in a living condition.

Model Home means a single family dwelling used for the general public viewing within specified times on a regular basis and was constructed for the purpose of encouraging the sale of similar homes within the same development. The hours of operation is restricted to 7 a.m. to 7 p.m. seven days a week. Associated with the model home or model home complex, there needs to be a paved parking lot. When the development is sold out, the model home or model home complex shall be converted into single family homes including removal of the parking lot.

SMC is the abbreviation for the City of Sparks Municipal Code.

Structural additions means any addition to a main or accessory structure including room add-ons, porch, deck, garage, storage space, room conversion, etc.

Temporary Construction Yard is defined as an area within a village development where construction equipment and materials are stored during the construction phase of the village. The yard shall be removed when the village is sold out. As a minimum the yard shall be accessed via gravel apron to the

approval of Public Works Director.

Temporary Sales Office means a building or structure including a trailer used for the purpose of encouraging the sale of homes, land, commercial space/parcel or office space/parcel used within the same development of like use during the sales period. When the development is sold out, the temporary office shall be removed. The hours of operation is restricted to 7 a.m. to 7 p.m. seven days a week. Associated with the temporary sales office, there needs to be a paved parking lot.

Uniform Graphic System is defined as the usage of the D'Andrea emblem on the project signage, sales documents, letterhead and retail items (such as hats, shirts, jackets, etc.) to create uniformity throughout D'Andrea. Refer to Figure 3-20.

Veranda is a term used to describe attached or detached single family housing within D'Andrea. These are smaller lots, generally in the form of cluster housing. These are the smallest lots in D'Andrea with a minimum size of 3,200 square feet per unit.

Vicinato is a term used to describe an intermediate size single family housing within D'Andrea. These lots have a minimum lot size of 6,000 square feet.

Villa is term used to describe the second largest single family housing within D'Andrea. These lots have a minimum lot size of 7,000 square feet.

Villaggio is a term used to describe the second smallest single family housing within D'Andrea. These lots have a minimum lot size of 4,500 square feet.

Yard means an open space on the same lot or parcel with the building, extending from the building to the nearest lot line.

Front yard means the yard lying between the front wall of the main building and the front property line; extending the full width of the lot or parcel.

Rear yard means the yard between the main building and the rear line and extending across the full width of the lot or parcel.

Side yard means the yard lying between the side lot and the main building and extending from the front yard line to the rear yard line.

Exterior side yard means the area between the side wall of the building and side property line abutting a street right-of-way on corner lots.



WASHOE COUNTY RECORDER

OFFICE OF THE RECORDER
LAWRENCE R. BURTNES, RECORDER

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LEGIBILITY NOTICE

The Washoe County Recorder's Office has determined that the attached document may not be suitable for recording by the method used by the Recorder to preserve the Recorder's records. The customer was advised that copies reproduced from the recorded document would not be legible. However, the customer demanded that the document be recorded without delay as the parties rights may be adversely affected because of a delay in recording. Therefore, pursuant to NRS 247.120 (3), the County Recorder accepted the document conditionally, based on the undersigned's representation (1) that a suitable copy will be submitted at a later date (2) it is impossible or impracticable to submit a more suitable copy.

By my signing below, I acknowledge that I have been advised that once the document has been microfilmed it may not reproduce a legible copy.

A handwritten signature in blue ink, appearing to read "Tim Thompson", written over a horizontal line.

Signature

A handwritten date "10/10/13" in blue ink, written over a horizontal line.

Date

The name "Tim Thompson" printed in blue ink, written over a horizontal line.

Printed Name